



**CITY OF SOUTH PASADENA
PLANNING COMMISSION**

AGENDA
REGULAR MEETING
TUESDAY, OCTOBER 11, 2022 AT 6:30 P.M.

CITY COUNCIL CHAMBERS
1424 MISSION STREET, SOUTH PASADENA, CA 91030

South Pasadena Planning Commission Statement of Civility

As your appointed governing board we will treat each other, members of the public, and city employees with patience, civility and courtesy as a model of the same behavior we wish to reflect in South Pasadena for the conduct of all city business and community participation. The decisions made tonight will be for the benefit of the South Pasadena community and not for personal gain.

NOTICE ON PUBLIC PARTICIPATION & ACCESSIBILITY

The South Pasadena Planning Commission Meeting will be conducted in-person from the Council Chambers, Amedee O. "Dick" Richards, Jr., located at 1424 Mission Street, South Pasadena. Pursuant to AB 361 Government Code Section 54953, subdivision (e) (3), the Planning Commission may conduct its meetings remotely and may be held via video conference.

The Meeting will be available:

- In Person Hybrid – City Council Chambers, 1424 Mission Street, South Pasadena
- Via Zoom: <https://us02web.zoom.us/j/83530439651> **Meeting ID: 8353 043 9651**

To maximize public safety while still maintaining transparency and public access, members of the public can observe the meeting via Zoom in the following methods below.

- Go to the Zoom website, <https://Zoom.us/join> and enter the Zoom meeting information; or
- Click on the following unique Zoom meeting link:
<https://us02web.zoom.us/j/83530439651>

CALL TO ORDER: Chair John Lesak

ROLL CALL: Chair John Lesak
Vice-Chair Laura Dahl
Commissioner Amitabh Barthakur
Commissioner Janet Braun
Commissioner Lisa Padilla

COUNCIL LIAISON: Councilmember Diana Mahmud

APPROVAL OF AGENDA
Majority vote of the Commission to proceed with Commission business.

DISCLOSURE OF SITE VISITS AND EX-PARTE CONTACTS
Disclosure by Commissioners of site visits and ex-parte contact for items on the agenda.

PUBLIC COMMENT GUIDELINES (Public Comments are limited to 3 minutes)

The Planning Commission welcomes public input. If you would like to comment on an agenda item, members of the public may participate by one of the following options:

Option 1:
Participate in-person at the City Council Chambers, 1424 Mission Street, South Pasadena.

Option 2:
Participants will be able to “raise their hand” using the Zoom icon during the meeting, and they will have their microphone un-muted during comment portions of the agenda to speak for up to 3 minutes per item.

Option 3:
Email public comment(s) to PlanningComments@southpasadenaca.gov. Public Comments received in writing will not be read aloud at the meeting, but will be part of the meeting record. Written public comments will be uploaded online for public viewing under Additional Documents. There is no word limit on emailed Public Comment(s). Please make sure to indicate:

- 1) Name (optional), and
- 2) Agenda item you are submitting public comment on, and
- 3) Submit by no later than 12:00 p.m., on the day of the Planning Commission meeting.

NOTE: Pursuant to State law, the Planning Commission may not discuss or take action on issues not on the meeting agenda, except that members of the Planning Commission or staff may briefly respond to statements made or questions posed by persons exercising public testimony rights (Government Code Section 54954.2). Staff may be asked to follow up on such items.

PUBLIC COMMENT

1. Public Comment – General (Non-Agenda Items)

PUBLIC HEARING

- 2. PROJECT NO. 2391 CUP** – A request for a Conditional Use Permit to allow the establishment and operation of a pre-school in the Button Hall building (1,816 sq. ft.) and an afterschool program serving students K-6th grade on the second floor of Evelyn Howell Crook Memorial building (1,886 sq. ft.) at an existing Oneonta Congregational Church campus located at 1507 Garfield Avenue (APN 5321-005-032).

Recommendation:

Approve the project subject to the recommended conditions of approval.

- 3. PROJECT NO. 2456 CUP-DRX-TRP** – A request for a Conditional Use Permit and Design Review for a 1,957 square-foot (sq. ft.) addition to an existing 3,216 sq. ft. veterinary hospital (Vet Villa Animal Hospital) in the Commercial General zone. The project includes the demolition of a 354 sq. ft. portion of the existing building, expansion of the veterinary hospital into the entire building, and the removal of one olive tree that is located in the private yard adjacent to Garfield Avenue. The project also includes a request for extended hours of operation from 11:00 p.m. to 6:00 a.m. which would allow the veterinary hospital to operate 24 hours a day, seven days a week.

Recommendation:

Approve the project subject to the recommended conditions of approval.

ADMINISTRATION

- 4. Comments from City Council Liaison**
- 5. Comments from Planning Commissioners**
- 6. Comments from Staff**

ADJOURNMENT

7. Adjourn to the Regular Planning Commission meeting scheduled for November 8, 2022.

PUBLIC ACCESS TO AGENDA DOCUMENTS AND BROADCASTING OF MEETINGS

Planning Commission meeting agenda packets are available online at the City website: <https://www.southpasadenaca.gov/government/boards-commissions/planning-commission/test-planning-commission-agendas-minutes-copy>

AGENDA NOTIFICATION SUBSCRIPTION

Individuals can be placed on an email notification list to receive forthcoming agendas by emailing CityClerk@southpasadenaca.gov or calling the City Clerk's Division at (626) 403-7230.

ACCOMMODATIONS



The City of South Pasadena wishes to make all of its public meetings accessible to the public. If special assistance is needed to participate in this meeting, please contact the City Clerk's Division at (626) 403-7230. Upon request, this agenda will be made available in appropriate alternative formats to persons with disabilities. Notification at least 48 hours prior to the meeting will assist staff in assuring that reasonable arrangements can be made to provide accessibility to the meeting (28 CFR 35.102-35.104 ADA Title II).

I declare under penalty of perjury that I posted this notice of agenda on the bulletin board in the courtyard of City Hall at 1414 Mission Street, South Pasadena, CA 91030, and on the City's website as required by law.

10/7/2022

Date

matt chang

Matt Chang, Planning Manager



Planning Commission Agenda Report

ITEM NO. 2

DATE: October 11, 2022

TO: Planning Commission

FROM: Angelica Frausto-Lupo, Community Development Director
Matt Chang, Planning Manager

PREPARED BY: Braulio M. Madrid, Associate Planner

SUBJECT: **Project No. 2391-CUP – A Conditional Use Permit to allow the establishment and operation of a pre-school in the Button Hall building (1,816 sq. ft.) and an after-school program serving students K-6th grade on the second floor of Evelyn Howell Crook Memorial building (1,886 sq. ft.) at the existing Oneonta Congregational Church campus located at 1507 Garfield Avenue. (APN No: 5321-005-032)**

Recommendation

Staff recommends that the Planning Commission adopt a Resolution approving Project No. 2391-CUP, subject to the conditions of approval (**Attachment 1**).

Background

The Oneonta Congregational Church is located at 1507-1515 Garfield Avenue, sitting on 4.37 acres of property at the northwest corner of Garfield Avenue and Oak Street. The property's existing zoning is Community Facility (C-F), which is a Special Purpose Zoning District for public and quasi-public land uses, including churches. There are several buildings on the existing site, including a main sanctuary, church offices, living quarters for the minister, and various halls and meeting rooms for its religious activities.

Aside from the applicant, My Kkids, there are two other child day care programs on site: Co-Op and Kids Klub. The Co-Op child care, is an independent 501 (c)(3) that is renting its facility from the Oneonta Congregational Church. In its current form they are a part time, non-religious, parent participation-based pre-school with a toddler component. Co-Op served an average of 45 students over the last 5 years with a maximum number of 7 employees. The Kids Klub runs a before and after school program only and has a maximum number of 15 employees and serves up to 70 students during school sessions and up to 120 students in camps during school breaks. Both existing daycares are independent and unassociated with the proposed business.

My Kkids, the applicant, serves pre-school and school-age students, and is operational on a weekday basis all throughout the year. The program serves children from two and a half years to twelve years old. The applicant located the operation of the facility within two existing rooms on-site: the Button Hall, a 1,816- square-foot room interior to the main Oneonta Congregational

Church building, and 1,886 square-feet of the second floor of the Evelyn Howell Crook Memorial Building.

History

On January 27, 2014, the Planning Commission adopted a Resolution approving Project No. 1657-CUP (Conditional Use Permit) to allow the operation of a before and after-school program at Oneonta Congregational Church campus in the Whitmore building and an existing small modular building at the north end for Kids Klub with an address of 1515 Garfield Avenue. The program serves up to 70 students during school months and up to 120 students in camps during school breaks and operates Monday-Friday from 7:30 a.m. to 6:00 p.m.

On August 7, 2014, the Planning Commission adopted a Resolution approving to Modify Project No. 1657-CUP at the request of the applicant to relocate the operation of Kids Klub from the existing modular building to the Oakview Room on campus. The number of students served, days and time of operation, and number of staff remained the same.

On July 1, 2020, the director of My Kkids, a separate pre-school and afterschool facility, filed a business license with the City requesting clearance to occupy the existing Button Hall building and a portion of the second floor of the Evelyn Howell Crook Memorial Building at Oneonta Congregational Church campus with an address of 1507 Garfield Avenue.

On August 31, 2020, My Kkids obtained City business license clearance to occupy the buildings at Oneonta Congregational Church campus.

On November 5, 2020, the City Fire Department conducted an inspection and the My Kkids facilities passed inspection.

On November 30, 2020, My Kkids received a license from the California Department of Social Services.

On December 4, 2020, My Kkids facility relocated from The United Methodist Church located at 699 Monterey Road to Oneonta Congregational Church campus with an address of 1507 Garfield Avenue.

On December 11, 2020, the director of My Kkids received an email from the City regarding a zoning violation related to occupation and operation of the day care program at the church campus. Child day care facilities are not listed as a permitted or a conditionally permitted land use for the C-F Zone. However, the activities of a child care facility are similar to those of a private school, which is a conditionally permitted use in the C-F Zone. My Kkids serves up to 25 pre-school children and up to 52 school age children, a discretionary approval (Administrative Use Permit) is required. In addition, since the Kids Klub, a separate daycare facility, was required to obtain a CUP in 2014, staff at that time felt a CUP process is more appropriate for My Kkids. My Kkids has a similar name to another licensed child-care facility on site Kids Klub (which does have a Conditional Use Permit). The City mistakenly issued My Kkids a business license, believing it was a renewal of an existing license for Kids Klub. However, a business license does not confer a right to operate this business without a CUP, thus triggering this process.

On January 20, 2021, applicant, My Kkids filed a CUP application with the City.

After submittal of the application, Planning Division and Public Works staff worked with the applicant and the traffic engineer to provide comments and guidance regarding the application submittal including traffic analysis report.

On September 29, 2022, mailing notices were sent to residents and owners within a 300-foot radius of the subject site notifying them of the Planning Commission hearing on October 11, 2022. A Notice of Public Hearing was published in the South Pasadena Review newspaper on September 30, 2022.

Project Description

The applicant is requesting that the Planning Commission approve a Conditional Use Permit allowing for the operation of My Kkids to remain at the church campus. My Kkids is licensed and is in compliance with requirements of the California Department of Social Services. My Kkids would operate year-around, Monday through Friday, from 7:00 a.m. to 7:00 p.m. (**Attachment 2**, Project Narrative). The proposed My Kkids day care program would serve pre-school and school-age students. The proposed day care center is expected to accommodate up to 77 students and 7 staff members during the school year. The program would serve children from two and a half years to twelve years old. During the summer, when the school is not in session, the maximum number of students is the same as school year (in session).

The “During COVID” and “After COVID” programing that My Kkids is observing is based on Community Care licensing guidelines put out by the California Department of Social Services. The applicant has voluntarily operated with a reduced capacity to ensure students have ample space to social distance in the classroom. The Community Care licensing guidelines suggests that pre-school learning centers should consider allocating six (6) by six (6) foot areas for individual play. As such, the operations of My Kkids has been following the guidelines. However, My Kkids has cautiously begun transitioning to a “post-COVID” model. The children are still at 6 feet apart with dividers between them, but they have ceased the use of individual play areas. My Kkids is monitoring the licensing guidelines to ascertain the appropriate increase in enrollment. However, the total number of students shall not exceed 77 children without first applying for a modification to the CUP, to be reviewed and approved by the Planning Commission.

“During COVID”, the pre-school program serves up to 15 children with two (2) staff members from 8:30 a.m. to 5:30 p.m. “After COVID” programing will allow up to 25 children in the preschool program with three (3) staff members from 8:30 a.m. to 6:30 p.m. The pre-school would remain in the Button Hall building located on the south side of campus near Oak Street (Attachment 4, Project Plans).

The kinder school program will serve kindergarten school age children up to children of 12 years. During COVID, the program serves up 25 students from 8:30 a.m. to 5:30 p.m. with three (3) fulltime staff members. After COVID programing is in place, the school age program will serve up to 52 students from 7:00 a.m. to 7 p.m. with four (4) staff members as listed in the bulletin points below. This program is currently conducted and would remain on the second floor of the

Evelyn Howell Crook Memorial Building, which is located near the center of the campus (**Attachment 4**, Project Plans).

“Post-COVID”, the program is expected to operate with a fully operation after-school program and pre-school program. The details are provided below:

Kinder-School Program (Monday through Friday)

- Maximum of 52 Students and 4 Staff
- AM Group (7:00 a.m. – 11:15 a.m.): Breakfast, Academics, and Extracurricular Activities
- PM Group (11:30 a.m. – 7:00 p.m.): Lunch, Academics, and Extracurricular Activities

Pre-School Program (Monday through Friday, 8:30 a.m. – 6:30 p.m.)

- Maximum of 25 Children and 3 Staff
- Supervised Learning, Outside Play Learning, Naps, and Free Play
- No interior construction or exterior changes to the buildings are not being proposed. Additionally, the applicant does not plan to install a sign on the campus for My Kkids. Therefore, no exterior changes will occur, except for placement of a permanent 4’ high fence to boarder the outdoor play areas of My Kkids. The proposed wrought iron fence measures 4’ high as required by the California Department of Social Serviced per Title 22 Section CCR 101238.2.
- Presently, there are two other day care programs at the church campus (**Table 1**). Refer to site plan (**Attachment 4**) for building locations on site – Project Plans.

Table 1: Breakdown of Day Care programs on Church Campus

Child care	Co-Op (501 c(3))	Kids Klub	My Kkids (this CUP)
Type	Pre-school	School Age, Before and After School Program	Pre-School and School-Age, Before and After School Program
Building Occupied	Evelyn Howell Crook Memorial Building (1st Floor)	Whitmore Building and Oakview Room	Button Hall and Evelyn Howell Crook Memorial Building (2nd Floor)
Maximum Number of Children	60	70 (school year), 120 (summer)	77
Maximum Number of Staff	7		7

The project site is located at the northwest corner of Oak Street and Garfield Avenue. Garfield Avenue serves as the city limit line between the City of South Pasadena and City of San Marino. As shown in **Figure 1**, the site is bounded by Oak Street to the south and Garfield Avenue to the east. The property abuts single-family homes to the north, south, east, and west. Across Oak Street there is also the South Pasadena YMCA (see **Figure 1**, Aerial View of Project Site).

Figure 1: Aerial View of Project Site



Project Analysis

General Plan Consistency

The General Plan land use designation of the site is Community Facilities, which allows institutional, academic, governmental, and community services uses. These include library buildings, community rooms, senior center, the historical museum, city hall, police, and fire stations, private and parochial schools, churches, the War Memorial Building, and the Eddy House.

The General Plan's Open Space and Conservation Element acknowledge the trend of private schools and local churches to increasingly provide a variety of programs that maximize the city's recreational and educational opportunities. The General Plan states that it is an important goal that the children of the community are offered enough quality programming to keep them interested in positive activities as described in Section 7.2B Public - Quasi-public Recreation Programs.

The General Plan's Economic Development and Revitalization Element notes that there is interdependence among economic development and the overall quality of life in South Pasadena. High standards for community facilities and services attract businesses and residents. Population growth, employment growth and new development in turn generate revenues that make high standards possible.

Thus, the child care facility is consistent with the General Plan's goals and policies that aim to offer quality programming that keeps the City's youth interested in positive activities. The use also contributes to the overall quality of life in the City by offering before and after-school programs that bring working parents comfort in knowing their children are safe and engaged in positive activities.

The establishment of an additional child care service for pre-school and school age children at the site is consistent with the General Plan designation, because the proposed use is similar to a school where children receive care, instruction, and supervision. Additionally, it is compatible with the existing functions of the church and the proposed use has already been established by the other service providers.

Zoning Code Compliance

The zoning for the subject site is Community Facilities (C-F) and is considered a Special Purpose Zoning District pursuant to Section 36.240. The C-F zoning district is applied to areas appropriate for public and quasi-public land uses. Typical land uses include government and institutional offices, schools, libraries, churches, and other related municipal uses. The C-F zoning district is consistent with the Community Facilities land use designation of the General Plan.

Child day care facilities are not listed as a permitted or a conditionally permitted land use for the C-F Zone (Table 2-6 of SPMC Section 36.240.030). However, the activities of a child care facility are similar to those of a private school, which is a conditionally permitted use in the C-F Zone.

Additionally, there are other existing religious facilities in South Pasadena that already provide child day care services to the public. In granting the original CUP, the Planning Commission determined the use to be appropriate for this property.

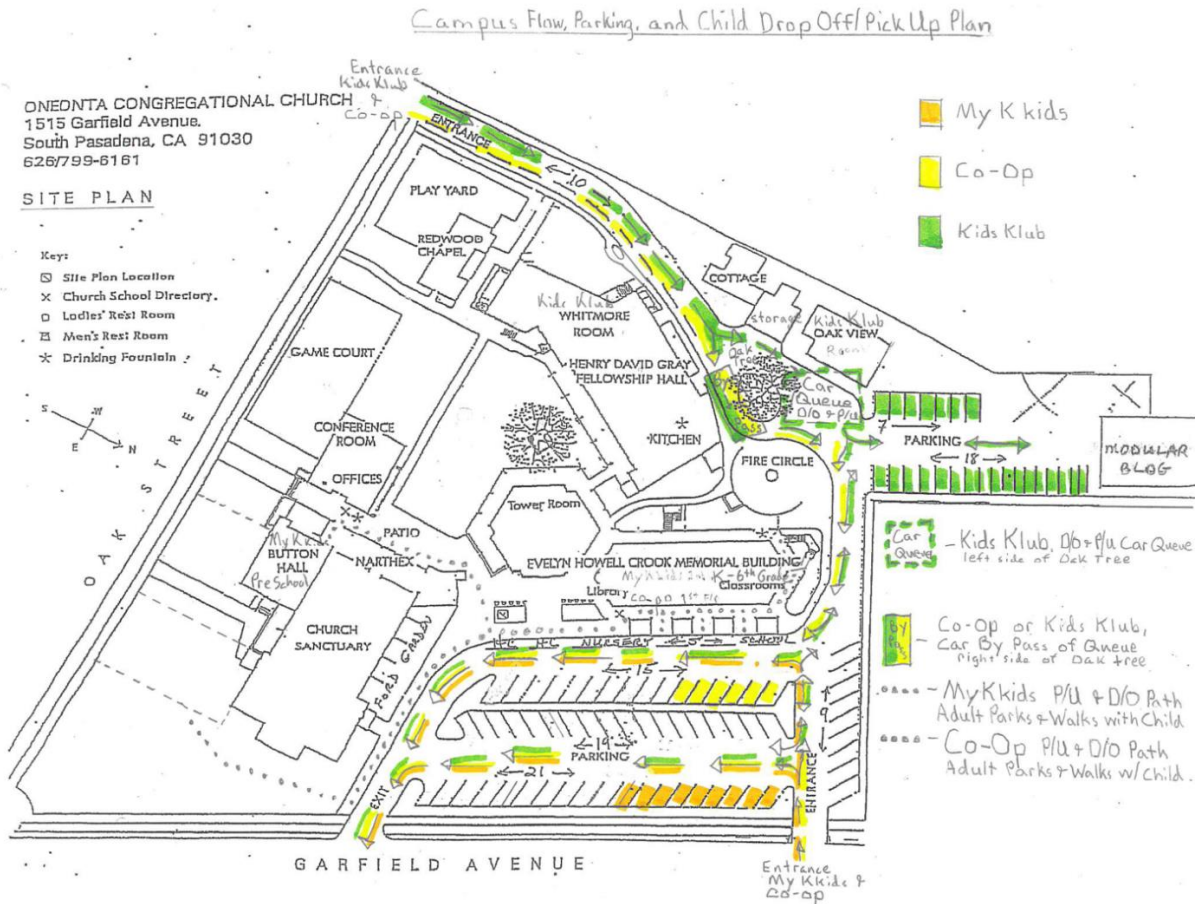
The boundaries of the site that abut residential lots (west and north) are primarily occupied by driveways or parking lots, which provide buffers from the play areas and buildings where potential noise from activities may occur. The campus also has well established landscaping on the boundaries with the residential neighbors that provides an additional buffer for the abutting residential properties. The proposed CUP does not include any remodels of any of the buildings. As conditioned, the applicant will be required to replace the existing temporary fencing used for outdoor activities to be in compliance with local and state regulations. The overall design of the church campus will maintain an attractive, harmonious, and orderly development as contemplated by this Section, and the General Plan.

Accessibility and Drop-off

The pick-up and drop-off areas for the programs on campus are illustrated on Figure 2. The safe area for picking up and dropping off children will all take place on the subject site; there will be

no pick-up or drop-off areas near local streets. Parents or guardians can walk their children to the center from a parking area at the north end of the site.

Figure 2: Campus Flow, Parking and Child Drop-off/Pick-up Plan



The proposed day care facility will continue to operate within the 4.3-acre site that encompasses several buildings and pockets of open space. Potential noise sources are likely to originate from the outdoor activity areas. Pursuant to the CUP for the other day cares on site and regulations by the state, the applicant will use two outdoor activity areas that are located at the south end of the site surrounded by buildings, adjacent to Oak Street. The outdoor activities are not expected to exceed ambient noise levels in the area as restricted by the Health and Safety Element of the General Plan.

Staff recommends that the CUP limit the operational hours to 7:00 a.m. to 7:00 p.m., Monday through Friday, as conditioned. Another condition of approval limits the number of children served by My Kkids to 77 children, as this was the maximum number used for the analysis of the traffic study.

Parking

The subject site provides a total of 96 parking spaces for church services, Co-Op, and Kids Klub programming. Kids Klub uses the 25 parking spaces located in the northerly area of the campus and a portion of spaces in main parking lot fronting Garfield Avenue. My Kkids would be using

the main parking lot area as well. **Table 2** provides a breakdown of operation days and times for each existing use at the campus, the proposed use, and parking required for each.

Kumon is an additional business that operates as an after school program for tutoring. However, Kumon utilizes one 441 square-foot office space in the second floor of the existing Evelyn Howell Crook Memorial Building (EHC) which is also shared with the My Kkids and Co-Op day care facilities. The subject building, EHC, was originally developed as classrooms for Sunday school. The intended use of this building has not changed as proposed by My Kkids.

The church offers worship services on Sundays at 9:30 am however, this event does not conflict with any of daycare facilities since the event takes place on Sundays. The church has no weekday activities between 7 a.m. and 7 p.m. during weekdays except the Bible Study and Boy Scouts, which would start after 7 p.m.

As proposed, and outlined in **Table 2**, sufficient parking would be provided with the additional child care program of My Kkids. The Oneonta Congregational Church has a total of 96 parking spaces. There is very low demand for parking on the property during the weekdays since church services are on Sundays.

Table 2: Parking Requirements for Project Site

Type of Service on Campus	Operation Days	Operation Hours	Staff Parking Required 1 space for each person	Staff Parking Proposed	Parent Parking Required 1 space for every 5 children	Parent Parking Proposed	Sum of Parking Required
<i>My Kkids Pre-school (Button Hall Bldg) Max. Children: 25 Max. Staff: 3</i>	<i>M-F</i>	<i>7am-7pm</i>	<i>3 spaces</i>	<i>3 spaces</i>	<i>5 spaces</i>	<i>5 spaces</i>	<i>8 spaces</i>
<i>My Kkids Afterschool (2nd floor of E.H.C. Memorial Bldg.) Max. Children: 52 Max Staff: 4</i>	<i>M-F</i>	<i>7am-7pm</i>	<i>4 spaces</i>	<i>4 spaces</i>	<i>11 spaces</i>	<i>11 spaces</i>	<i>15 spaces</i>
<i>Oneonta Co-op (1st floor of E.H.C Memorial Bldg.) Max. Children: 60 Max. Staff: 7</i>	<i>M-F</i>	<i>8:30am-12:30pm</i>	<i>7 spaces</i>	<i>7 spaces</i>	<i>12 spaces</i>	<i>12 spaces</i>	<i>19 spaces</i>
<i>Kids Klub Afterschool (Whitmore Rm.)</i>	<i>M-F</i>	<i>7am-6pm</i>	<i>30 spaces</i>	<i>30 spaces</i>	<i>24 spaces</i>	<i>24 spaces</i>	

& Oak View Room) Max.Children:70 school year, 120 summer 8 wks Max. Staff: 30							54 spaces
Church Services/Events During the Week	Currently none						
			Total: 44 Staff Parking Spaces			Total Required	96 spaces

Traffic Study

To assess the traffic impacts to the neighborhood from the additional proposed use on site, a focused traffic impact assessment was prepared by Minagar & Associates, Inc. (**Attachment 5 Traffic Study**).

Access to the project site would be provided two ways: 1) by Oak Street via an existing 20-foot wide, private one-way alley which traverses the site in a clockwise direction, and 2) the existing 20-foot-wide northerly driveway on Garfield Avenue (**Figure 1**). All day care traffic will exit on the east side of the project site onto Garfield Avenue (**Attachment 8 Parking and Access**).

Parents/guardians for the proposed My Kkids will park and walk their children to the center (**Figure 2**).

A traffic analysis was performed based on the existing morning (a.m.) and afternoon (p.m.) peak hour traffic volumes and Levels of Service (LOS) of four un-signalized intersections, which included three project driveways and one two-way stop-controlled intersection within city limits. An analysis of the Opening Year (2021) with Project conditions revealed that each of the four studied intersections is anticipated to continue operating at its pre-project LOS.

The overall analysis result concludes that the generated traffic of the proposed MyKkids child care center project in addition to the existing traffic of Co-Op and Kids Klub would not have any potential impact on traffic, it would be compatible with the surrounding transportation environment in the City of South Pasadena and would not be required to provide any traffic mitigation measures upon the adjacent roadway system.

As a condition of approval, it is recommended that post-operational traffic study be prepared at the applicant’s expense one year after the issuance of the Conditional Use Permit. The study shall be in compliance with the City’s adopted Transportation Impact Analysis methodology, and shall include, but not limited to, new turning movement counts and a traffic signal warrant analysis at the intersection of Garfield Avenue and Oak Street. Counts shall be collected when schools and colleges are in session, and the scheduling of the counts shall be approved by the Director of Public Works. Should additional mitigation measures be required, the project

applicant shall contribute their fair share of the cost towards the implementation (PW Condition #2).

Neighborhood Compatibility

The existing character of the surrounding neighborhood consists of a variety of single-family residential buildings of various architectural styles. The Church is bounded by Garfield Avenue to the east, Oak Street to the south, and is surrounded by single-family homes to the north and west. There is also an existing YMCA facility is located across the street on Oak Street. Along the east side of Garfield Avenue, and across the street from the Church, the adjacent single-family properties are within the limits of the City of San Marino (**Attachment 3** Photographs with Key).

The My Kkids child care facility will operate between the hours to 7:00 a.m. to 7:00p.m., Monday through Friday when the church grounds are underutilized. The applicant has plans to implement procedures for safe pick-up and drop-off of children on the property; there will be no pick-up or drop-off areas near local streets. Outdoor activities will be confined to open spaces areas with the proposed fencing to buffer the outdoor activities from impacting the neighborhood. (**Attachment 7** Fence Plan)

Conditional Use Permit Findings

In order to approve a CUP, the Planning Commission must make certain findings listed in SPMC section 36.410.060. The required findings are listed below.

- 1. The proposed use is allowed with Conditional Use Permit approval within the applicable zoning district and complies with all applicable provisions of this Zoning Code;**

The project applicant seeks approval of a Conditional Use Permit (CUP) to operate a child care facility within two existing rooms located on-site: the Button Hall, a 1,816 square foot room interior to the main Oneonta Congregational Church building, and the second floor of the 1,886 square foot Evelyn Howell Crook Memorial Building located on the northern side of the site. The project site is located within a Community Facilities (CF) zoning district. Child day care facilities are not listed as a permitted or a conditionally permitted land use for the CF Zone. However, the activities of the proposed child care facility are similar to those of a private school, which is a conditionally permitted use in the CF Zone. The proposal complies with all applicable provisions of the Zoning Code with respect to specific land use standards for child day care centers and parking requirements.

- 2. The proposed use is consistent with the General Plan and any applicable specific plan;**

The proposed My Kkids day care use is consistent with the provisions of the General Plan's General Plan's Open Space and Conservation Element, which acknowledges

the need for a variety of programs that maximize the City's recreational and educational opportunities. An important goal of the General Plan is to offer children in the community with enough quality programming to keep them interested in positive activities. The subject property is designated for Community Facilities. The 4.3-acre site offers an opportunity for an efficient joint-use of the site that allows multiple day care services (My Kkids, Co-op, and Kids Klub) during the week and religious services on the weekends. The General Plan's Economic Development and Revitalization Element calls for high standards with regard to the City's community facilities, noting that there is an interdependence among economic development and the overall quality of life in South Pasadena.

- 3. The establishment, maintenance, or operation of the use would not, under the circumstances of the particular case, be detrimental to the health, safety, or general welfare of persons residing or working in the neighborhood of the proposed use;**

The applicant will use two outdoor activity areas that are located at the south end of the site surrounded by buildings, adjacent to Oak Street (Attachment 4, Project Plans). The placement of the outdoor areas in this location reduces the potential for noise for neighboring residential properties.

There are 96 parking spaces and roadways that provide adequate vehicular circulation. As proposed the CUP limits the operational hours from 7:00 a.m. to 7:00 p.m., Monday through Friday when the church grounds are underutilized. The approved CUP limits the number of children to 77 children total. The applicant has plans to implement procedures for safe pick-up and drop-off of children; there will be no pick-up or drop-off areas near major streets.

The project has been conditioned to require that the child care facility shall comply with all appropriate licenses from County and State agencies charged with regulating the operation of child care centers such as the need of an outdoor play area. And any proposed changes to the child care facility or use shall be reviewed and approved by the City of South Pasadena and the California State Department of Social Services.

As such, the establishment, maintenance and operation of the proposed use would not be detrimental to the health, safety or general welfare of persons working or residing in the neighborhood.

- 4. The use, as described and conditionally approved, would not be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the City;**

The My Kkids child care facility will operate between the hours to 7:00 a.m. to 7:00 p.m., Monday through Friday when the church grounds are underutilized. The applicant has plans to implement procedures for safe pick-up and drop-off of children; there will be no pick-up or drop-off areas near local streets. Outdoor

activities will be confined to open spaces where there are walls and buildings to reduce the effects of noise in the neighborhood.

Based on the results of a traffic analysis, the generated traffic of the Proposed MyKkids child care center project in addition to the existing traffic of Co-Op and Kids Klub would not have any potential impact on traffic, it would be compatible with the surrounding transportation environment in the City of South Pasadena and would not be required to provide any traffic mitigation measures upon the adjacent roadway system. (See PW Condition #2).

- 5. The subject site is adequate in terms of size, shape, topography, and circumstances and has sufficient access to streets and highways which are adequate in width and pavement type to carry the quantity and quality of traffic expected to be generated by the proposed use; and**

The subject site is 4.37 acres of gently sloping land that is developed with church buildings, roadways, and parking lots. The site is adequate in size, shape, and circumstances for the approved child care facility and its necessary pick-up/drop-off area and parking. The site is located at the northeast corner of Garfield Avenue and Oak Street. Vehicular traffic for the existing child care facilities (Kids Klub and Co-op) will enter from Oak Street onto a private one-way road through the church site. As proposed by the applicant, vehicular traffic for the new child care facility (My Kkids) will have access from Garfield Avenue into a parking lot on site. All vehicles exit the site on to Garfield Avenue to the east. The subject site is adequate in terms of size, topography and circumstances and has sufficient access to streets and highways adequate to accommodate the traffic generated by the proposed use. The proposed CUP makes no changes to the existing site.

- 6. The design, location, operating characteristics, and size of the proposed use would be compatible with the existing and future land uses in the vicinity, in terms of aesthetics, character, scale, impacts on neighboring properties.**

This particular case involves the addition of a day care use on a 4.3-acre church campus within existing buildings and open spaces in a park-like setting. The project does not involve any construction or interior improvements to the buildings on site. The subject building E.H.C, was designed with the intent to be used as classrooms for Sunday school for the church. The use has been previously approved for other tenants on the property. No new signage is proposed. Thus the proposed design, location, operations, characteristics and size of the proposed use is compatible with the existing neighborhood properties.

Environmental Analysis

This project is exempt from California Environmental Quality Act (CEQA) analysis based on State CEQA Guidelines Section 15031, Class 1 – Existing Facilities. A Class 1 Categorical Exemption includes additions to existing structures provided the addition will not result in an increase of more than 10,000 square feet, provided the project site is in an area where all public services and facilities are available and is not located in an environmentally sensitive area.

Staff Recommendation

Based on the above analysis, staff recommends that the Planning Commission adopt the attached resolution for approval of the CUP (Project No. 2391-CUP), subject to the attached conditions of approval.

Alternatives to Consider

If the Planning Commission does not agree with staff's recommendation, the following options are available:

1. The Planning Commission can Approve the project as is or with modified condition(s) added or removed and provide findings; or
2. The Planning Commission can Continue the project, providing the applicant with clear recommendations to revise the proposal; or
3. The Planning Commission can Deny the project if it finds that the project does not meet the City's CUP requirements.

Public Notification of Agenda Item

A Public Hearing Notice was published on September 30, 2022, in the *South Pasadena Review*. Hearing notices were sent to all properties within a 300-foot radius on September 29, 2022. In addition, the public was made aware that this item was to be considered at a public hearing by virtue of its inclusion on the legally publicly noticed agenda, and the posting of the same agenda and reports on the City's website

Public Comment

At the time of writing this report 12 letters of support were submitted.

Attachments

1. Planning Commission Resolution with Exhibit "A" - Conditions of Approval
2. Project Narrative
3. Photographs with Key
4. Project Plans
5. Traffic Study
6. Public Comment
7. Fence Plan
8. Parking and Access

ATTACHMENT 1

Resolution Planning Commission Resolution with Exhibit "A" -
Conditions of Approval

PLANNING COMMISSION RESOLUTION NO. 22-_____

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF SOUTH PASADENA APPROVING PROJECT NO. 2391-CUP CONSISTING OF A CONDITIONAL USE PERMIT TO ALLOW THE ESTABLISHMENT AND OPERATION OF A PRE-SCHOOL IN THE BUTTON HALL BUILDING (1,816 SQ. FT.) AND AFTERSCHOOL PROGRAM SERVING STUDENTS K-6TH GRADE ON THE SECOND FLOOR OF EVELYN HOWELL CROOK MEMORIAL BUILDING (1,886 SQ. FT.) AT THE EXISTING ONEONTA CONGREGATIONAL CHURCH CAMPUS LOCATED AT 1507 GARFIELD AVENUE (APN 5321-005-032)

WHEREAS, On January 20, 2021 the applicant, Tom Webb, of the lot addressed 1507 Garfield Avenue, submitted an application for a Conditional Use Permit (Project No. 2391-CUP) to the Community Development Department to allow the establishment and operation of a pre-school in the Button Hall building (1,816 sq. ft.) and an afterschool program serving students K-6th grade on the second floor of Evelyn Howell Crook Memorial building (1,886 sq. ft.) at an existing church campus; and

WHEREAS, The proposed project is categorically exempt from the California Environmental Quality Act (CEQA) pursuant to 15301, Class 1 – Existing Facilities; and

WHEREAS, the Planning Division evaluated the project for consistency with the City's General Plan, City of South Pasadena Municipal Code, and all other applicable state and local regulations; and

WHEREAS, On September 30, 2022, the City of South Pasadena Planning Division, published a legal notice in the *South Pasadena Review*, a local newspaper of general circulation, indicating the date, time, and location of the public hearing in compliance with state law concerning Project No. 2391-CUP for this Planning Commission hearing and a public hearing postcard notice was mailed out to all property owners within 300-foot radius from the project site; and

WHEREAS, On October 11, 2022 the City of South Planning Commission held a duly noticed public hearing at which time the Commission considered the staff report, oral report, testimony, and written evidence submitted by the Applicant and by members of the public concerning Project No. 2391-CUP and considered the proposed Conditional Use Permit to allow the establishment and operation of a pre-school in the Button Hall building (1,816 sq. ft.) and afterschool program serving students K-6th grade on the second floor of Evelyn Howell Crook Memorial building (1,886 sq. ft.) at the existing Oneonta Congregational Church campus, subject to conditions of approval, at 1507 Garfield Avenue.

NOW, THEREFORE, THE PLANNING COMMISSION OF THE CITY OF SOUTH PASADENA DOES HEREBY FIND, DETERMINE, AND RESOLVE AS FOLLOWS:

SECTION 1: ENVIRONMENTAL FINDINGS

The Planning Commission determines that the proposed project is categorically exempt from the California Environmental Quality Act (CEQA) pursuant to Section 15301, Class 1 – Existing Facilities. The Class 1 exemption includes the operation and permitting, of existing public or private structures and facilities, involving negligible or no expansion of existing or former use.

SECTION 2: CONDITIONAL USE PERMIT FINDINGS

The Planning Commission finds that the proposed project is consistent with all applicable findings for approval of the Conditional Use Permit pursuant to South Pasadena Municipal Code (SPMC) Section 36.410.060(F), as follows:

1. *The proposed use is allowed with Conditional Use Permit or Administrative Use Permit approval within the applicable zoning district and complies with all applicable provisions of this Zoning Code.*

The project applicant facility within two existing rooms located on-site: the Button Hall, a 1,816 square foot room interior to the main Oneonta Congregational Church building, and the second floor of the 1,886 square foot Evelyn Howell Crook Memorial Building located on the northern side of the site. The project site is located within a Community Facilities (CF) zoning district. Child day care facilities are not listed as a permitted or a conditionally permitted land use for the CF Zone. However, the activities of the proposed child care facility are similar to those of a private school, which is a conditionally permitted use in the CF Zone. The proposal complies with all applicable provisions of the Zoning Code with respect to specific land use standards for child day care centers and parking requirements.

2. *The proposed use is consistent with the General Plan and any applicable specific plan.*

The proposed My Kkids day care use is consistent with the provisions of the General Plan's General Plan's Open Space and Conservation Element, which acknowledges the need for a variety of programs that maximize the City's recreational and educational opportunities. An important goal of the General Plan is to offer children in the community with enough quality programming to keep them interested in positive activities. The subject property is designated for Community Facilities. The 4.3-acre site offers an opportunity for an efficient joint-use of the site that allows multiple day care services (My Kkids, Co-op, and Kids Klub) during the week and religious services on the weekends. The General Plan's Economic Development and Revitalization Element calls for high standards with regard to the City's community facilities, noting that there is an interdependence among economic development and the overall quality of life in South Pasadena.

3. *The establishment, maintenance, or operation of the use would not, under the circumstances of the particular case, be detrimental to the health, safety, or general welfare of persons residing or working in the neighborhood of the proposed use.*

The applicant will use two outdoor activity areas that are located at the south end of the site surrounded by buildings, adjacent to Oak Street (Attachment 4, Project Plans). The placement of the outdoor areas in this location reduces the potential for noise for neighboring residential properties.

There are 96 parking spaces and roadways that provide adequate vehicular circulation. As proposed the CUP limits the operational hours from 7:00 a.m. to 7:00 p.m., Monday through Friday when the church grounds are underutilized. The approved CUP limits the number of children to 77 children total. The applicant has plans to implement procedures for safe pick-up and drop-off of children; there will be no pick-up or drop-off areas near major streets.

The project has been conditioned to require that the child care facility shall comply with all appropriate licenses from County and State agencies charged with regulating the operation of child care centers such as the need of an outdoor play area. And any proposed changes to the child care facility or use shall be reviewed and approved by the City of South Pasadena and the California State Department of Social Services.

As such, the establishment, maintenance and operation of the proposed use would not be detrimental to the health, safety or general welfare of persons working or residing neighborhoods.

4. *The use, as described and conditionally approved, would not be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the City.*

The My Kkids child care facility will operate between the hours to 7:00 a.m. to 7:00 p.m., Monday through Friday when the church grounds are underutilized. The applicant has plans to implement procedures for safe pick-up and drop-off of children; there will be no pick-up or drop-off areas near local streets. Outdoor activities will be confined to open spaces where there are walls and buildings to reduce the effects of noise in the neighborhood.

Based on the results of a traffic analysis, the generated traffic of the Proposed MyKkids child care center project in addition to the existing traffic of Co-Op and Kids Klub would not have any potential impact on traffic, it would be compatible with the surrounding transportation environment in the City of South Pasadena and would not be required to provide any traffic mitigation measures upon the adjacent

roadway system (See PW Condition #2).

- 5. The subject site is adequate in terms of size, shape, topography, and circumstances and has sufficient access to streets and highways which are adequate in width and pavement type to carry the quantity and quality of traffic expected to be generated by the proposed use.*

The subject site is 4.37 acres of gently sloping land that is developed with church buildings, roadways, and parking lots. The site is adequate in size, shape, and circumstances for the approved child care facility and its necessary pick-up/drop-off area and parking. The site is located at the northeast corner of Garfield Avenue and Oak Street. Vehicular traffic for the existing child care facilities (Kids Klub and Co-op) will enter from Oak Street onto a private one-way road through the church site. As proposed by the applicant, vehicular traffic for the new child care facility (My Kkids) will have access from Garfield Avenue into a parking lot on site. All vehicles exit the site on to Garfield Avenue to the east. The subject site is adequate in terms of size, topography and circumstances and has sufficient access to streets and highways adequate to accommodate the traffic generated by the proposed use. The proposed CUP makes no changes to the existing site.

- 6. The design, location, operating characteristics, and size of the proposed use would be compatible with the existing and future land uses in the vicinity, in terms of aesthetics, character, scale, impacts on neighboring properties.*

This particular case involves the addition of a day care use on a 4.3-acre church campus within existing buildings and open spaces in a park-like setting. The project does not involve any construction or interior improvements to the buildings on site. The subject building E.H.C, was designed with the intent to be used as classrooms for Sunday school for the church. The use has been previously approved for other tenants on the property. No new signage is proposed. Thus the proposed design, location, operations, characteristics and size of the proposed use is compatible with the existing neighborhood properties.

SECTION 3: RECORD OF PROCEEDING

The documents and other materials that constitute the record of the proceedings upon which the Planning Commission's decision is based, which include, but are not limited to, the staff reports, as well as all materials that support the staff reports for the proposed project, and are located in the Community Development Department of the City of South Pasadena at 1414 Mission Street, South Pasadena, CA 91030. The custodian of these documents is the City Clerk of the City of South Pasadena.

SECTION 4. DETERMINATION

Based upon the findings and guidelines outlined in Sections 1 through 2 above and provided during the public hearing, the Planning Commission of the City of South Pasadena hereby:

- a. Determines that the proposed project complies with the City's General Plan, City of South Pasadena Municipal Code, City's Design Guidelines, and all other applicable state and local regulations; and
- b. Approves the application for a Conditional Use Permit to allow the establishment and operation of a pre-school in the Button Hall building (1,816 sq. ft.) and afterschool program serving students K-6th grade on the second floor of Evelyn Howell Crook Memorial building (1,886 sq. ft.) at the existing Oneonta Congregational Church campus at 1507 Garfield Avenue (Project No. 2391-CUP) (APN: 5321-005-032), subject to the Conditions of Approval that are attached hereto as Exhibit "A."

SECTION 5: APPEALS

Any interested person may appeal this decision or any portion of this decision to the City Council. Pursuant to the South Pasadena Municipal Code, any such appeal must be filed with the City, in writing, and with appropriate appeal fee, no later than fifteen (15) days, following the date of the Planning Commission's final action.

SECTION 6: CERTIFICATION OF THE RESOLUTION

The Secretary shall certify that the foregoing Resolution was adopted by the Planning Commission of the City of South Pasadena at a duly noticed regular meeting held on the 11th day of October, 2022.

PASSED, APPROVED, AND ADOPTED this 11th day of October, 2022 by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

John Lesak, Chair

ATTEST:

Amitabh Barthakur, Secretary to the Planning Commission

Exhibit "A"

Conditions of Approval

CONDITIONS OF APPROVAL
Conditional Use Permit
PROJECT NO. 2391-CUP
1507 Garfield Avenue (APN: 5321-005-032)

The following approvals are granted as described below and as shown on the development plans submitted to and approved by the Planning Commission on October 11, 2022:

Conditional Use Permit: to consider a Conditional Use Permit to allow the establishment and operation of a preschool in the Button Hall building (1,816 sq. ft.) and an afterschool program serving students K-6th grade on the second floor of Evelyn Howell Crook Memorial building (1,886 sq. ft.) at an existing Oneonta Congregational Church campus located at 1507 Garfield Avenue (Assessor's Parcel Number 5321-005-032).

Note: As a convenience to the applicant, the development requirements from applicable Departments/Agencies are listed herein. These requirements list what the applicant will be required to comply with in order to receive a Building Permit, a Certificate of Occupancy, or other Department-issued entitlement.

PLANNING DIVISION:

- P1. This approval and all the approval and all rights hereunder shall terminate within 12 months of the effective date of their approval by the Planning Commission unless otherwise conditioned and/or unless action is taken to secure Building Permits and maintain active Building Permits with the Building Division beginning with the submittal of the plans for Plan Check review.
- P2. Approval by the Planning Commission does not constitute a building permit or authorization to begin any construction. An appropriate permit issued by the South Pasadena Building Division must be obtained prior to construction, enlargement, relocation, conversion or demolition of any building or structure on any of the properties involved with this approval.
- P3. All other requirements of any law, ordinance, or regulation of the State of California, City of South Pasadena, and any other government entity shall be complied with.
- P4. Compliance with and execution of all conditions listed herein shall be necessary prior to obtaining any occupancy inspection clearance and/or prior to obtaining any occupancy clearance.
- P5. The applicant and each successor in interest to the property which is the subject of this project approval, shall defend, indemnify and hold harmless the City of South Pasadena and its agents, officers and employees from any claim, action or proceeding against the City or its agents, officers or employees to attack, set aside, void or annul any approval of the City, City Council or Planning Commission concerning this approval. In the event of any claim or lawsuit, the applicant and/or successor shall submit a deposit in such amount as the City reasonably determines necessary to protect the City from exposure to fees, costs or liability with respect to such claim or lawsuit.
- P6. The Preschool program is limited to accommodating a maximum of 25 children.
- P7. The Kinder-school program limited to accommodating a maximum of 52 children
- P8. The child care facility hours of operation are limited to the hours of 7:00 AM to 7:00 PM, Monday through Friday. No child care services may be offered outside those time frames without first obtaining approval from the Planning Commission.

- P9. The childcare facility shall comply with all appropriate licenses from County and State agencies charged with regulating the operation of child care centers.
- P10. Any proposed changes to the child care facility or use shall be reviewed and approved by the City of South Pasadena and the California State Department of Social Services.
- P11. Permanent fencing shall be proposed to enclose the existing outdoor play area used by the “My Kkids” to replace the existing temporary vinyl fencing. New fencing shall be in compliance with the California Department of Social Services, Department of Education, and local South Pasadena development standards. If standards or requirements are in conflict, state requirement will supersede local standards.

BUILDING AND SAFETY DIVISION:

- B1. The use for educational purposes in the existing building, “Button Hall”, which is accessory to the place of worship (“Sanctuary”), is allowed per Section 305.1.1 of the California Building Code, provided the occupant load in this space or room is less than 100. No condition of approval is required assuming no construction is proposed.
- B2. The continuing use for day care or educational purposes on the second floor of the existing building, “Evelyn Howell Crook Memorial Building”, which was classified as E Occupancy Group, is allowed per Section 452.1.4 of the California Building Code, provided the building is equipped with an automatic sprinkler throughout and the existing means of egress systems are for exclusive use of the 2nd floor occupants. No condition of approval is required assuming no construction is proposed.
- B3. No work or construction materials will be permitted to encroach into adjacent property without written approval from the affected property owner.

PUBLIC WORKS DEPARTMENT

- PW1. The applicant shall obtain City approval for any modifications or revisions to the approval of this project. Deviations not identified on the plans may not be approved by the City, potentially resulting in the need for the project to be redesigned.
- PW2. A post-operational traffic study shall be prepared at the applicant’s expense one year after the issuance of the Conditional Use Permit. The study shall be in compliance with the City of South Pasadena’s adopted Transportation Impact Analysis methodology, and shall include, but not limited to, new turning movement counts and a traffic signal warrant analysis at the intersection of Garfield Avenue and Oak Street. Counts shall be collected when schools and colleges are in session, and the scheduling of the counts shall be approved by the Director of Public Works. Should additional mitigation measures be required, the project applicant shall contribute their fair share of the cost towards the implementation.

FIRE DEPARTMENT:

- FD1. The project shall comply with all current adopted California Building, Fire Codes, NFPA and SPFD standards requirements based on occupancy classification.

ATTACHMENT 2

Project Narrative

Narrative of the MyKkids Proposed Project

MyKkids seeks to operate a Non-Profit Christian daycare consisting of a pre-school and after school program, to be located at the existing Oneonta Congregational Church campus located at 1515 Garfield Ave South Pasadena. MyKkids will use the campus address of 1507 Garfield Ave. South Pasadena, CA. The program will serve children from two and a half years through twelve years of age and be operational on a weekday only basis throughout the year.

The child care program will be conducted in two (2) areas on the campus: "Button Hall" for the pre-school, which is a 1,816-SF room located on the south side of the main sanctuary building, and a 1,886-SF area on the second floor of the Evelyn Howell Crook Memorial Building. The programs will run year-round.

During the **Covid** school year, the program will operate according to our State mandated Covid guidelines, the After-School program will involve approximately 15 children and include supervised online learning, outside recess and extracurricular activities from 8:30am – 5:30pm, Monday through Friday, and have a staff of three, one being a floater. The Pre-School school program will also be following mandated guidelines and would have approximately 10 children and will involve supervised learning, outside play learning, Christian story time and activities centers from 8:30am – 5:30pm, Monday through Friday, and have a staff of two. Both programs together serve approximately twenty-five children, some of which would be Covid "Essential" workers children, and there would be total staff of 5.

During the **Post Covid** school year, we anticipate the After-School program for Kindergarteners, to continue, as in the past, to be a half day school program, broken into AM and PM groups. The PM Kinder school group program will involve a supervised program of breakfast, academics, Chapel time and extracurricular activities in the morning from 7:00am – 11:15am then be taken to school, and the AM Kinder school group program would then be picked up from school when the PM school group is dropped off. The AM kinder school group then would return to MyKkids, have lunch, academics, Chapel time, extracurricular activities and play in the afternoon from 11:30am to 7:00pm. The older grades in the After-School program and the PM Kinder group would get picked up from school between 2:25pm to 3:10pm. The total After-School program will have a maximum of 52 students, with a full and part-time staff of four people, one of which would be a floater. The Pre-School school program would have a maximum of 25 children and will involve supervised; learning centers, outside play learning, Christian story time, naps and free play from 8:30am – 6:30pm, Monday through Friday, and have a staff of three.

ATTACHMENT 3

Photographs and Key

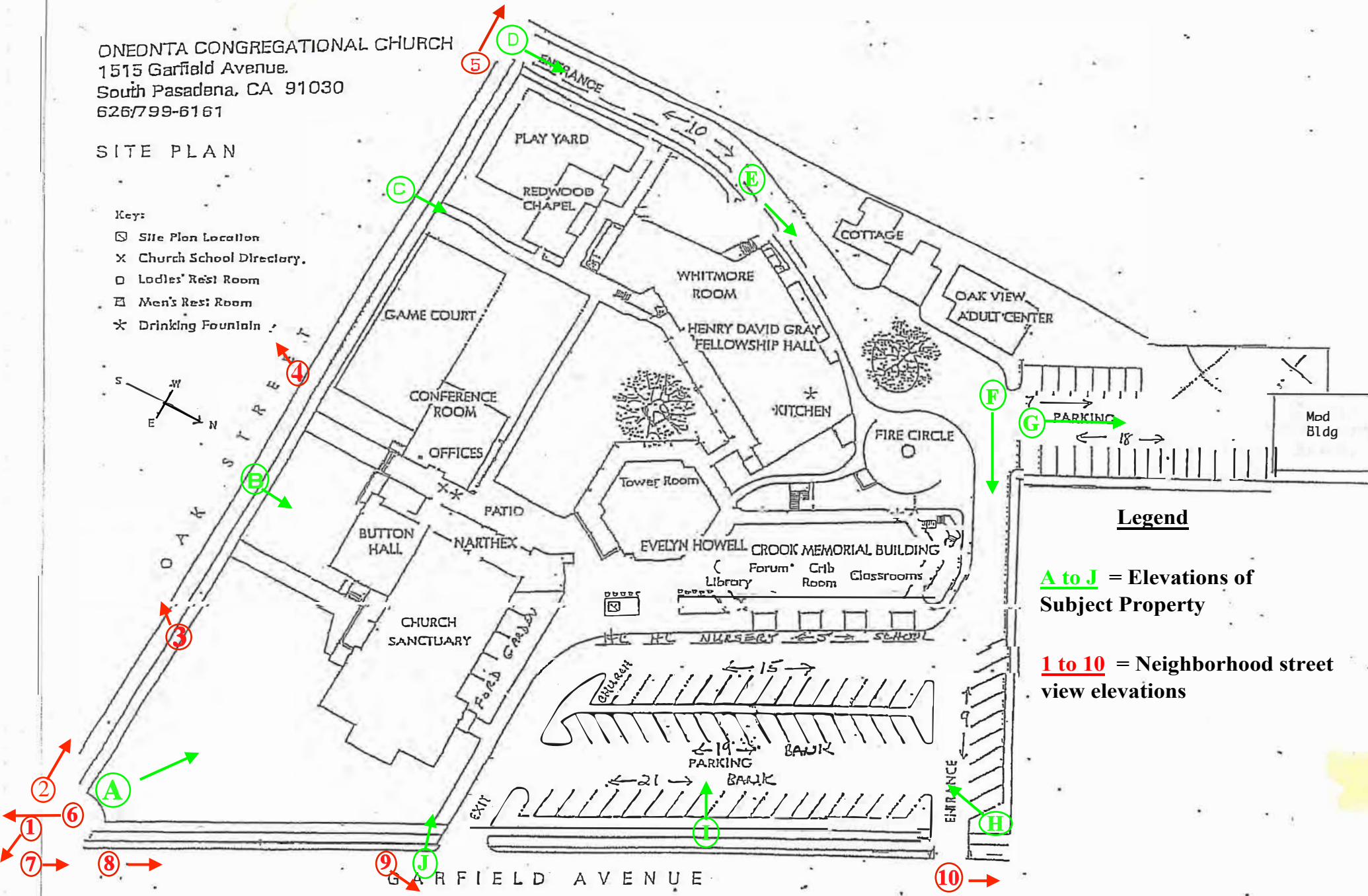
Photo Key - MyKkids

ONEONTA CONGREGATIONAL CHURCH
 1515 Garfield Avenue.
 South Pasadena, CA 91030
 626/799-6161

SITE PLAN

Key:

- Site Plan Location
- X Church School Directory.
- Ladies' Rest Room
- ⊞ Men's Rest Room
- * Drinking Fountain



Legend

A to J = Elevations of Subject Property

1 to 10 = Neighborhood street view elevations

Subject Property Elevations



A



B



C



D



E



F



G



H



I



J

End of Subject Property Elevations

Start of neighborhood street views



305 Oak St. San Marino

1600 Garfield Ave. SM

YMCA

1515 Garfield Ave. SP
Subject Property



YMCA

oneonra.org
1515 Garfield Ave SP
(subject property)

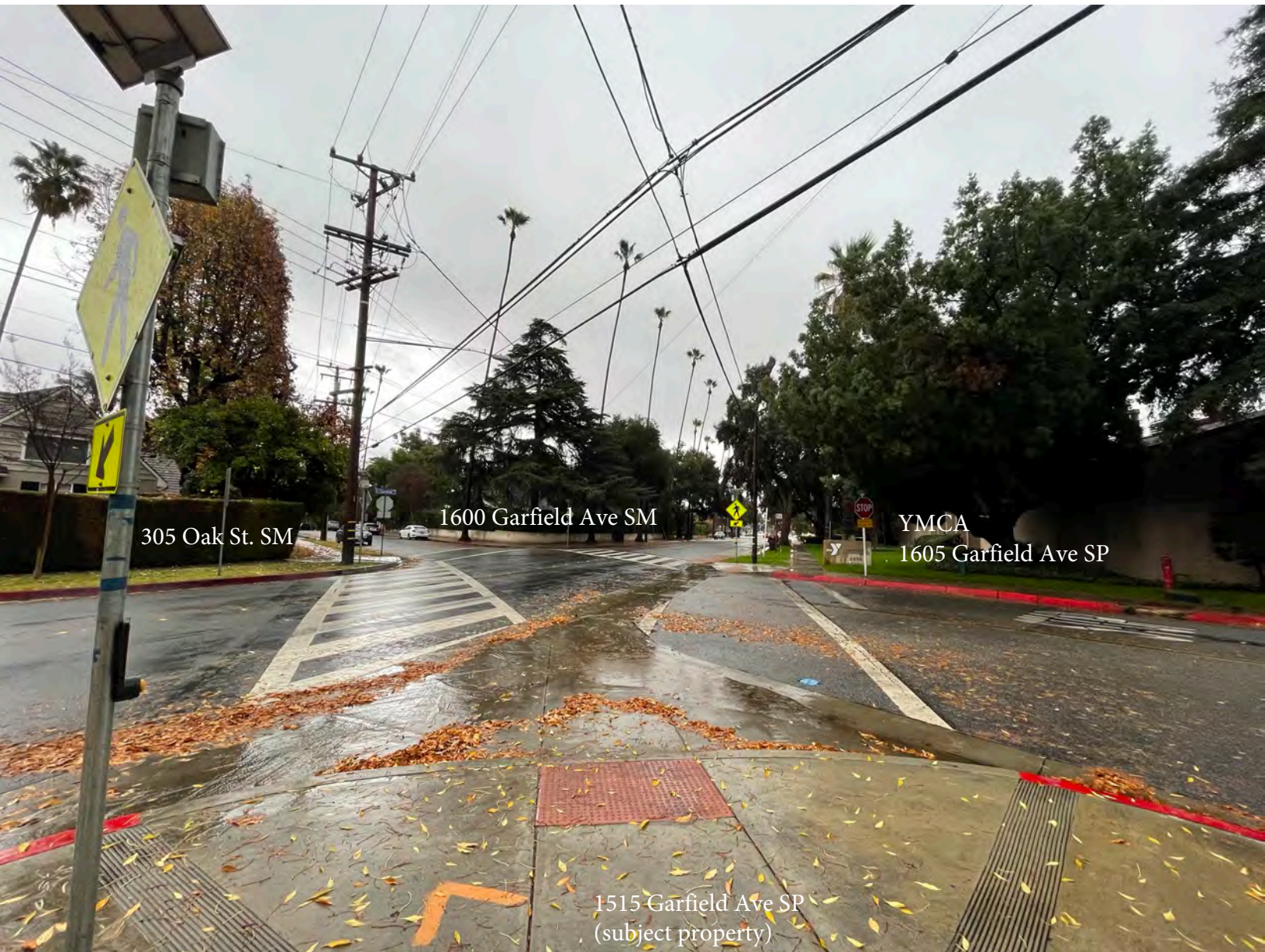
2







5

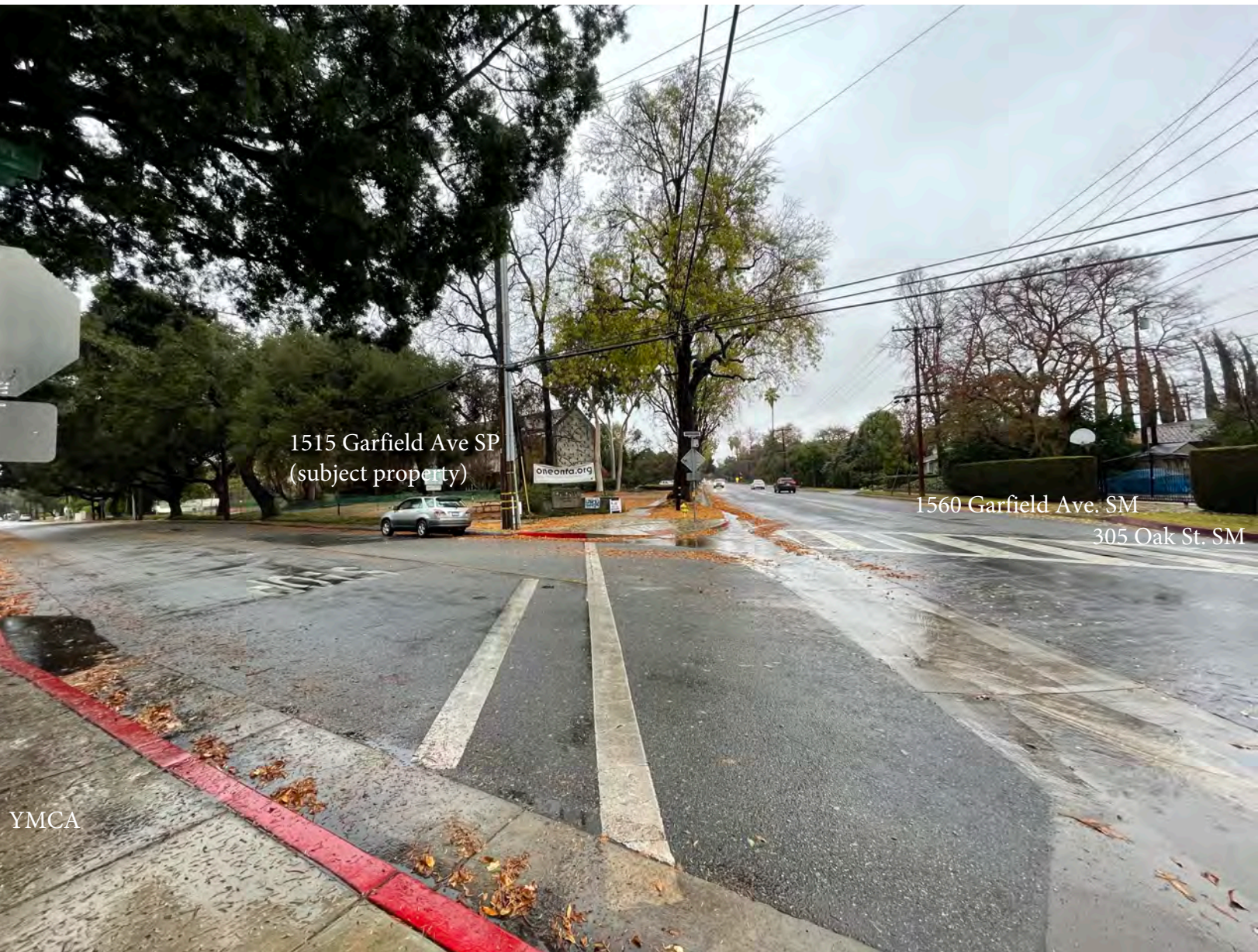


305 Oak St. SM

1600 Garfield Ave SM

YMCA
1605 Garfield Ave SP

1515 Garfield Ave SP
(subject property)



1515 Garfield Ave SP
(subject property)

oneonta.org

1560 Garfield Ave. SM
305 Oak St. SM

YMCA

7



1515 Garfield Ave SP
(subject property)

1520 Garfield SM

1540 Garfield SM

1560 Garfield SM



1460 Garfield SM

1480 Garfield SM

1500 Garfield SM

1520 Garfield SM

1515 Garfield Ave SP
(subject property)



1358 Garfield SM

1380 Garfield SM

1400 Garfield SM

1420 Garfield SM

1440 Garfield Ave. SM

1515 Garfield Ave SP
(subject property)

10

ATTACHMENT 4

Project Plans

Campus Flow, Parking, and Child Drop Off/Pick Up Plan

ONEONTA CONGREGATIONAL CHURCH
 1515 Garfield Avenue.
 South Pasadena, CA 91030
 626/799-6161

SITE PLAN

Key:

- Site Plan Location
- × Church School Directory
- Ladies' Rest Room
- ⊞ Men's Rest Room
- * Drinking Fountain

- My K kids
- Co-Op
- Kids Klub



- Car Queue - Kids Klub, D/O + P/U Car Queue left side of Oak Tree
- Car By Pass - Co-Op or Kids Klub, Car By Pass of Queue right side of Oak tree
- My K kids P/U + D/O Path Adult Parks + Walks with Child
- Co-Op P/U + D/O Path Adult Parks + Walks w/child.

Entrance My K kids & Co-op

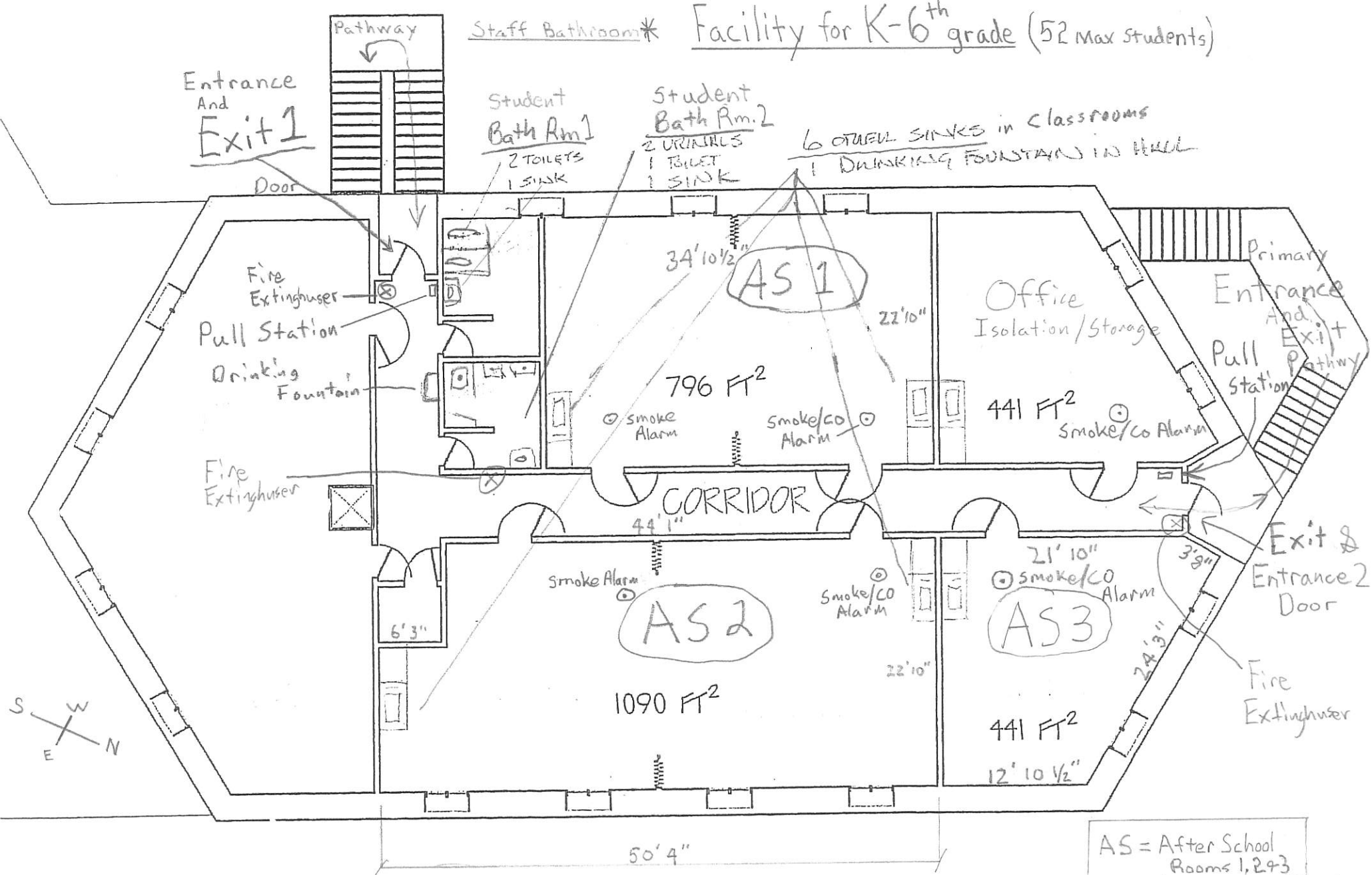
Staff Bathroom* Facility for K-6th grade (52 Max Students)

Entrance
And
Exit 1
Door

Student
Bath Rm 1
2 TOILETS
1 SINK

Student
Bath Rm. 2
2 URINALS
1 TOILET
1 SINK

6 OTHER SINKS in classrooms
1 DRINKING FOUNTAIN IN HALL

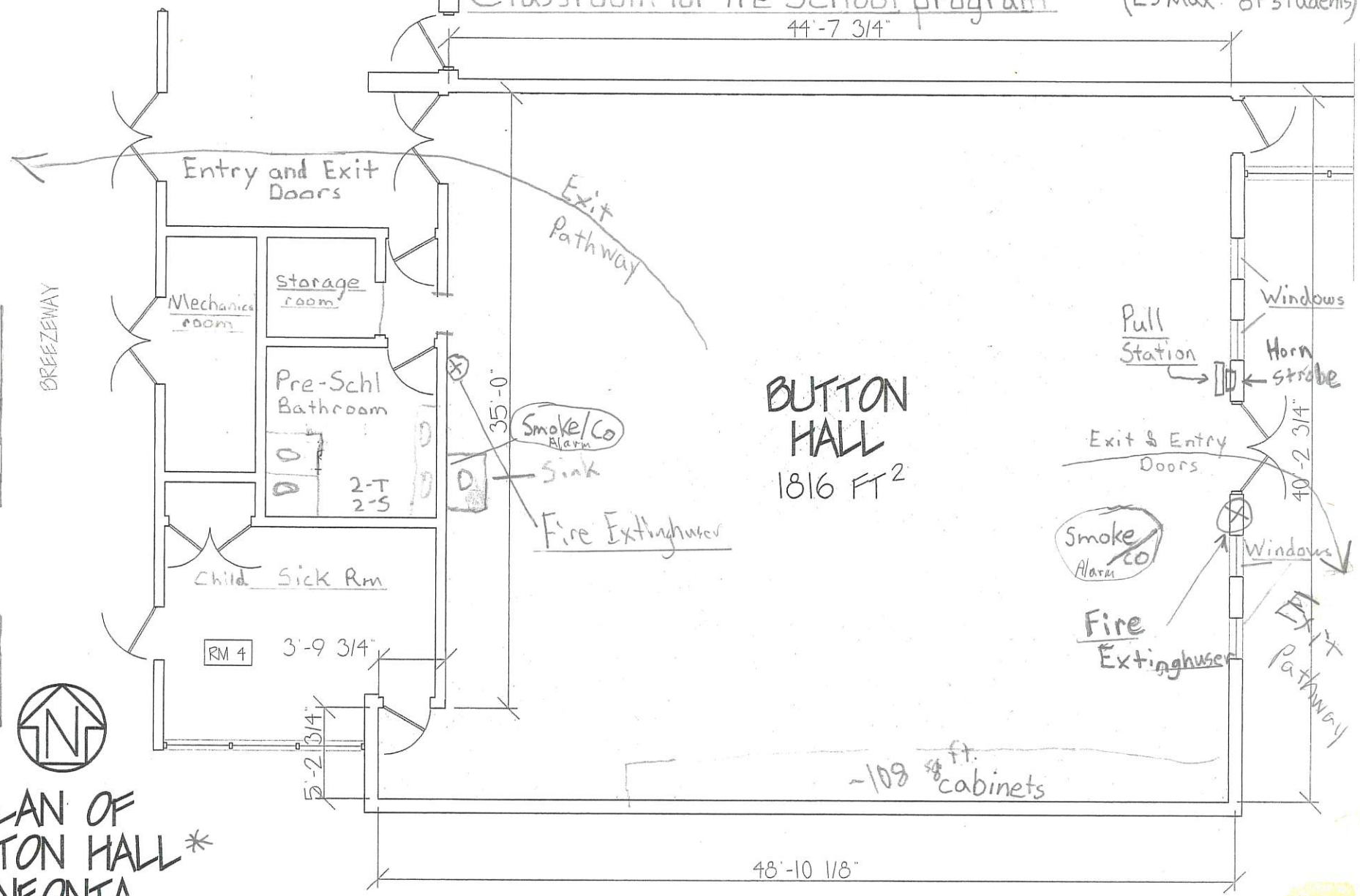


2nd Flr of Christian Education Bldg. for K-6th grade
Oneonta Congregational Church

AS = After School
Rooms 1, 2 & 3
have 52 max #
of students,
during school
and summer
sessions.
* = Staff Bathroom
see Button Hall
map.

Classroom for Pre-School program (25 max # of students)

44'-7 3/4"



Staff Bathroom in Church Office

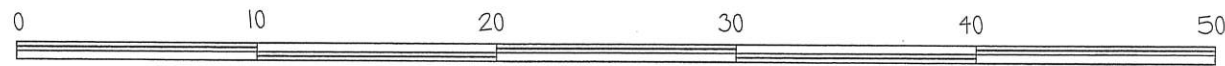
Adult Bthrm

2 Urinals
2 Sinks
2 Toilets



PLAN OF
BUTTON HALL*
ONEONTA
CONGREGATIONAL
CHURCH

* - One Story Bldg



SCALE 1/8" = 1'-0"

P.A.F. 11/14/07

Proposed Rooms, Play Yards, Lunch Areas, Bus. Days/Hrs.

ONEONTA CONGREGATIONAL CHURCH
 1515 Garfield Avenue.
 South Pasadena, CA 91030
 626/799-6161

SITE PLAN

Key:

- Site Plan Location
- x Church School Directory
- Ladies' Rest Room
- ⊞ Men's Rest Room
- * Drinking Fountain



■ - My K kids, M-F, 7a-7p

1507 Garfield Ave
 South Pasadena, CA

■ - Co-Op, M-F, 8:30a-12:30p

■ - Kids Klub, M-F, 7a-6p

▨ - Button Hall
 (Max. 25 Pre-School)

■ - Memorial Building
 (Max. 52 School Age)
 (2nd Floor Only)

■ - Memorial Building
 (1st and 2nd Floor)

■ - Whitmore & Oak View

--- Fence, semi permanent, M-F
 Pool style removable Fence

--- Fence, proposed permanent
 Decorative metal/Water Capture Sys

ATTACHMENT 5

Traffic Study



FOCUSED TRAFFIC IMPACT ASSESSMENT MYKKIDS CHILD CARE FACILITY

Located at
ONEONTA CONGREGATIONAL CHURCH
1515 GARFIELD AVENUE, SOUTH PASADENA CA 91030



PREPARED FOR:

CITY OF SOUTH PASADENA

Public Works Department
1414 Mission Street
South Pasadena, CA 91030-3298



PREPARED BY:

MINAGAR & ASSOCIATES, INC.

Traffic Engineering – ITS – Transportation Planning – CEM
23282 Mill Creek Dr, Suite 120
Laguna Hills, CA 92653
Tel: (949) 707-1199
Web: www.minagarinc.com



Revised June 10, 2021

EXECUTIVE SUMMARY

This report presents the results of a focused traffic impact assessment conducted for the proposed MyKKids Child Care Facility at 1515 Garfield Avenue in South Pasadena, California during December of 2020. The overall analysis result concludes that the traffic generated by MyKKids (Proposed Project), Co-Op (Existing), and Kids Klub (Existing) would not have any significant impact on the overall traffic. The proposed day care center is expected to accommodate up to 77 students and 7 staff members at maximum capacity for the school year. The project will occupy the existing Button Hall and second floor of the Evelyn Howell Crook Memorial Building on the existing 4.37-acre Oneonta Congregational Church campus, which is located at the northwest corner of Garfield Avenue and Oak Street.

Access to the project site would be provided by Oak Street via an existing 20' wide, private one-way alley which traverses the site in a clockwise direction as well as the existing 20' wide northerly driveway on Garfield Avenue. Day care traffic will exit on the east side of the project site onto Garfield Avenue. The potential traffic impacts related to the proposed project were evaluated following the City of South Pasadena's *Transportation Impact Analysis (TIA) Methodology, May 6, 2020* and the latest *Highway Capacity Manual (HCM)* analysis method.

A traffic analysis was performed based on the existing morning (AM) and afternoon (PM) peak-hour traffic volumes and Levels of Service (LOS) of four unsignalized intersections, which included three project driveways and one two-way stop-controlled intersection within city limits. An analysis of the *Post COVID (2021) Plus Project* conditions revealed that each of the four studied intersections is anticipated to continue operating at its pre-project LOS, and that the project as proposed would not generate any significant traffic impacts to adjacent intersections during the weekday AM and PM peak hours. Based on the results of the analysis, ***the generated traffic of the Proposed MyKKids Child Care Center project in addition to the Existing traffic of Co-Op and Kids Klub would not have any potential impact on traffic, it would be compatible with the surrounding transportation environment in the City of South Pasadena and would not be required to provide any traffic mitigation measures upon the adjacent roadway system.***

Additionally, the Project Land Use is classified as a "community serving project" per the City's *TIA Methodology*, and Governor's Office of Planning and Research's (OPR's) *Technical Advisory on Evaluation Transportation Impacts in CEQA*, published April 2018. As the Project qualifies as one of the City's Land Use Development Screening Thresholds, it can be presumed to have a Less Than Significant impact on transportation and ***is not required to perform further VMT analysis.***

Introduction

Minagar & Associates, Inc. (MAI) conducted a traffic impact analysis to identify potential traffic impacts related to the proposed MyKKids day care center in the City of South Pasadena. **Figure ES-1** shows the vicinity of the project site in relation to the citywide transportation network.

During July of 2014, Minagar & Associates, Inc. performed a similar Traffic Impact Analysis (TIA) for the Kids Klub Child Care Facility at the same location of Oneonta Congregational Church. The traffic counts for this TIA were taken from the previous study and projected to 2020 utilizing an average growth factor from the Southern California Association of Government's (SCAG's) ConnectSoCal Forecast.

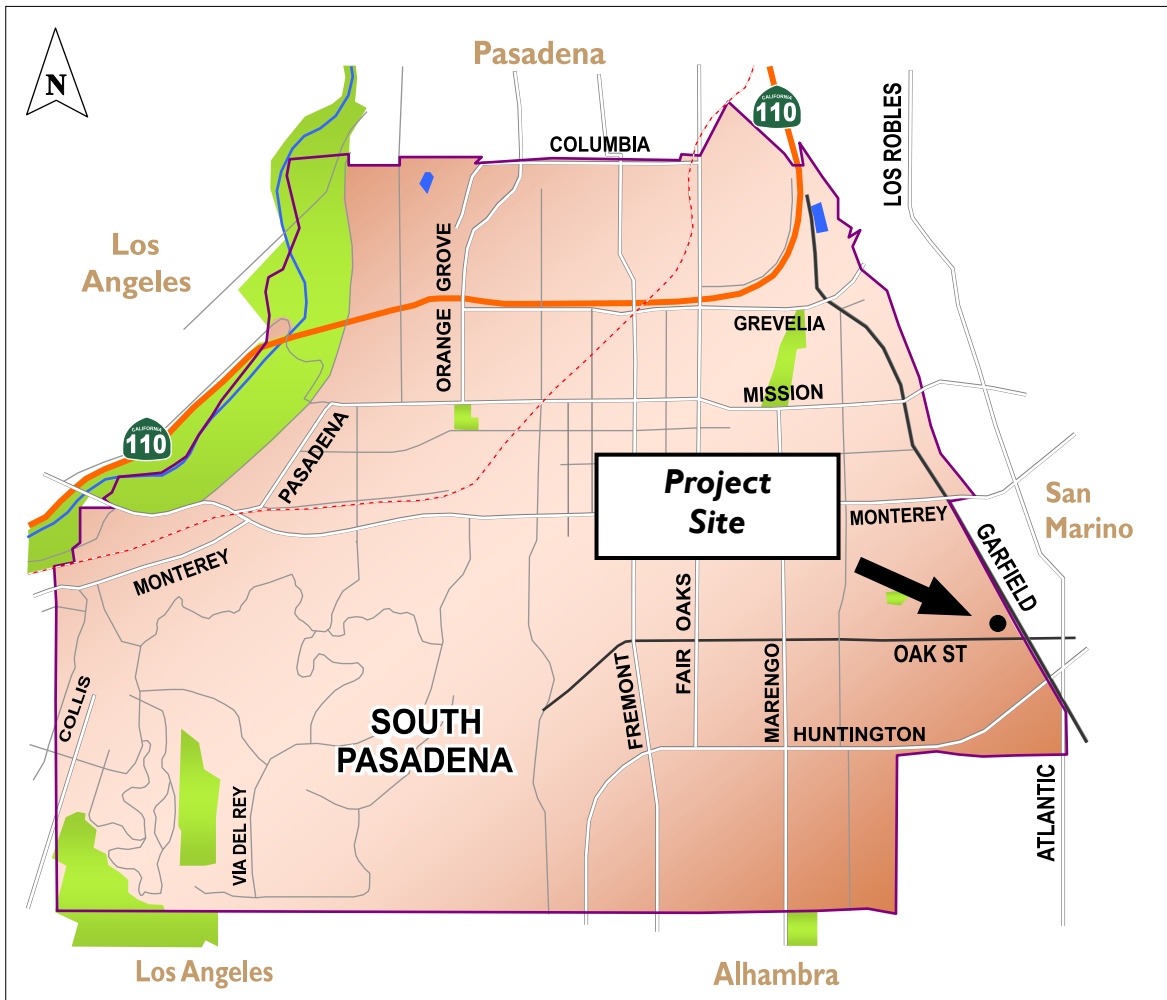


Figure ES-1. Vicinity Map



Existing Site and Proposed Project Operations

The Existing Oneonta Congregational Church is located at 1515 Garfield Avenue, on 4.37 acres of property at the northwest corner of Garfield Avenue and Oak Street. The property's existing zoning is C-F, which is a Special Purpose Zoning District for public and quasi-public land uses, including churches. There are several buildings on the existing site, including a main sanctuary, church offices, living quarters for the minister, and various halls and meeting rooms for its religious activities. The Oneonta Congregational Church is bounded by Garfield Avenue to the east, Oak Street to the south, and is surrounded by single-family homes to the north and west. There is also an existing YMCA facility is located across the street on Oak Street. Along the east side of Garfield Avenue, and across the street from the Oneonta Congregational Church, the adjacent properties are within the limits of the City of San Marino.

The Oneonta Congregational Church does not operate any daycare programs nor does it have a pre-school program. The church offers worship services on Sundays at 9:30 am; however, this event does not conflict with any of the daycare facility activities as it takes place on Sundays.

The Existing Co-Op child care is an independent 501 c(3) renting its facility from the Oneonta Congregational Church. It served 45 students as the highest average number of students over the last 5 years with a maximum number of 7 employees.

The Existing Kids Klub runs an after-school program only and has a maximum number of 15 employees, the highest average is unknown; however, the maximum number of students allowed per state licensing is 70.

The Proposed MyKkids Project will consist of a Pre-school and School Age Program at the Existing Oneonta Congregational Church Complex, located at 1515 Garfield Avenue in the City of South Pasadena. The day care program would serve Pre-School and School-Age students, and be operational on a weekday basis all throughout the year. The program would serve children from two and a half years to twelve years old. In response to the City's review comments, during the summer, when the school is not in session, the maximum number of students is the same as school year (in session). The project applicant seeks approval of a Conditional Use Permit (CUP) Amendment to operate the facility within two existing rooms located on-site: the "Button Hall," a 1,816-SF room interior to the main Oneonta Congregational Church building, and the second floor of the 1,886-SF "Evelyn Howell Crook Memorial Building" located on the northern side of the site.

During the **COVID** school year, the program will operate in accordance with California State COVID daycare guidelines. The programs are to include the following:



- School Age Program (Monday through Friday, 8:30am – 5:30pm)
 - Maximum of 25 Children and 3 Staff
 - Supervised Online Learning, Outside Recess, and Extracurricular Activities

- Pre-School Program (Monday through Friday, 8:30am – 5:30pm)
 - Maximum of 15 Children and 2 Staff
 - Supervised Learning, Outside Play Learning, Activity Centers

During the **Post Covid** school year, the program is expected to operate with a fully operation After-School Program and Pre-School Program. The details of which are provided below.

- After-School Program (Monday through Friday)
 - Maximum of 52 Students and 4 Staff
 - AM Group (7:00am – 11:15am): Breakfast, Academics, and Extracurricular Activities
 - PM Group (11:30am – 7:00pm): Lunch, Academics, and Extracurricular Activities

- Pre-School Program (Monday through Friday, 8:30am – 6:30pm)
 - Maximum of 25 Children and 3 Staff
 - Supervised Learning, Outside Play Learning, Naps, and Free Play

Traffic, Circulation & Parking

Garfield Avenue is designated in the City's General Plan Circulation and Accessibility Element as a Minor Arterial, with two undivided travel lanes (one per direction) and on-street parking. The existing posted speed limit on Garfield Avenue is 35 miles per hour. Oak Street is designated as a Collector in the City's General Plan, and similarly provides two undivided lanes of travel with on-street parking. The existing posted speed limit on Oak Street is 25 miles per hour. The intersection of Garfield Avenue and Oak Street is currently unsignalized, with traffic moving freely in the north/south directions on Garfield Avenue and the east/west directions stop-controlled. There are marked pedestrian crosswalks across the east and west legs of the intersection, as well as on the uncontrolled north and south approaches which require motorists to yield to pedestrians when approaching the intersection.

Currently, there are three access driveways serving the project site: one (1) entrance-only driveway on Garfield Avenue, one (1) exit-only driveway on Garfield Avenue, and one (1) entrance-only driveway on Oak Street. Vehicular traffic for the proposed childcare center will enter from the Oak Street driveway or Garfield Avenue driveway, and continue through on a 20-foot wide, private one-way road. Vehicles will circulate in a clockwise direction through the

church site. Vehicles would then exit onto Garfield from the available driveway on the east side of the project site.

Figure ES-2 illustrates the study area developed on the *Synchro-10.0* traffic modeling and analysis software program, which includes functional lane geometries and traffic controls associated with each of the four unsignalized study intersections. **Figure ES-3** shows the existing AM and PM peak hour traffic volumes collected at each study location by Minagar & Associates, Inc. during June 3, 4, and 5, 2014 and projected to 2020 utilizing a 0.21% growth factor.

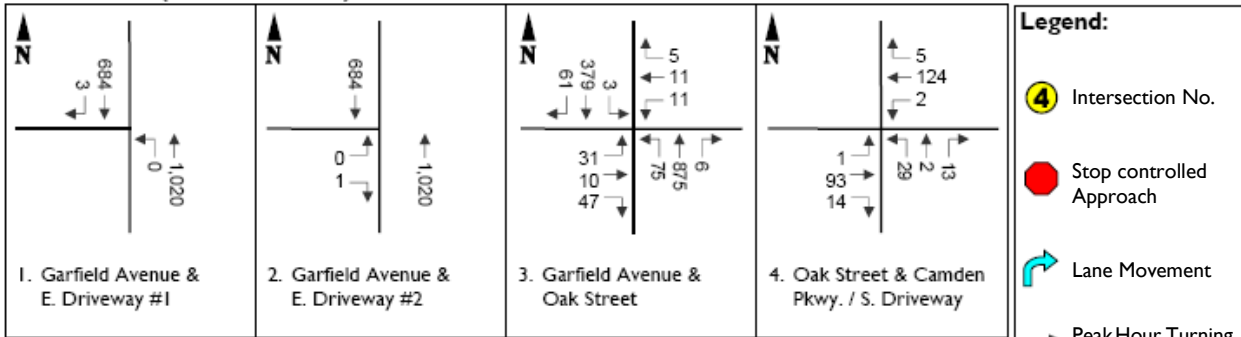
Table ES-1 below shows the calculation of the average growth factor from the Southern California Association of Government’s (SCAG’s) ConnectSoCal Forecast.

Table ES-1. Average Growth Factor Tabulation

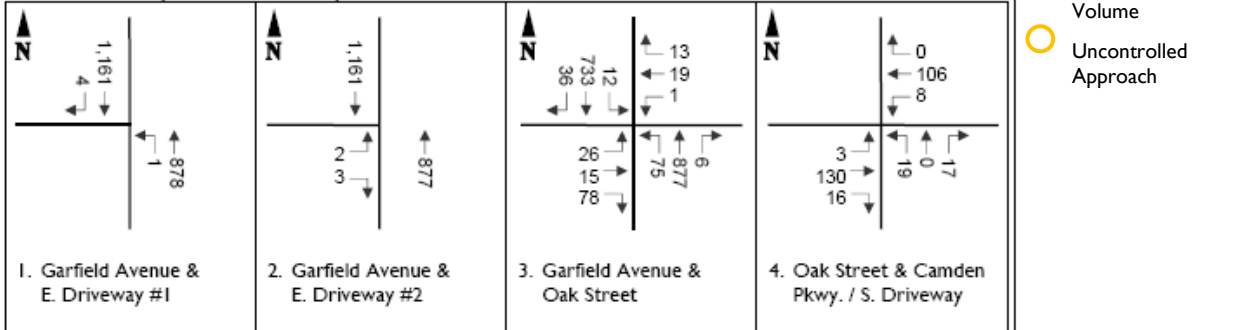
Population						Average Growth Factor (%)
Units	2014	2020	2030	2035	2045	
	25992	26088	26533	26767	27240	
Growth Factor (%)	2014→2020	2020→2030	2030→2035	2035→2045		
	0.092	0.169	0.176	0.175		
Housing						0.21
Units	2014	2020	2030	2035	2045	
	10431	10517	10831	10973	11245	
Growth Factor (%)	2014→2020	2020→2030	2030→2035	2035→2045		
	0.21	0.295	0.261	0.245		
Employment						
Units	2014	2020	2030	2035	2045	
	11411	11528	11730	11832	12136	
Growth Factor (%)	2014→2020	2020→2030	2030→2035	2035→2045		
	0.255	0.174	0.173	0.254		



AM Peak Hour (7:30AM – 8:30AM)



PM Peak Hour (5:00PM – 6:00PM)



Project Trip Forecast

Trip Generation

Project trip generation was estimated by applying the size and proposed use of the project to the appropriate trip generation rates published by the Institute of Transportation Engineers (ITE) in *Trip Generation*, 10th Edition. Based on ITE's trip generation rates for "Day Care Center" (Land Use #565), the proposed project is expected to generate between 31 and 61 weekday peak hour trips, depending on the assumed unit of trip generation (student- or employee-based). For the purposes of this analysis, the more conservative student-based assumption was applied, resulting in an estimated generation of 60 AM peak hour project trips and 61 PM peak hour project trips. **Table ES-2** includes a summary of trip generation estimates determined for the project using the range of criteria described above and includes the AM and PM peak hour trip generation estimates selected for use in the traffic analysis.

Trip Distribution

The trip distribution for the proposed MyKKids Child Care Center project is shown in **Figure ES-4**. Inbound and outbound project distribution patterns were modeled on the basis of the location and density of surrounding residential neighborhoods in the City of South Pasadena, the location of the site with respect to nearby grade schools, the peak hour weekday traffic count volumes collected by Minagar & Associates, Inc. during the school season in early June 2014, and an analysis of the logical inbound/outbound routes to the project site from the surrounding transportation system.

Trip Assignment

Based upon the MyKKids Child Care Center trip distribution percentages, developed as described above, the project's peak hour trips were assigned to the surrounding local and regional roadway system in the traffic model. **Figure ES-5** shows the proposed AM and PM peak hour trip assignments at each study intersection.

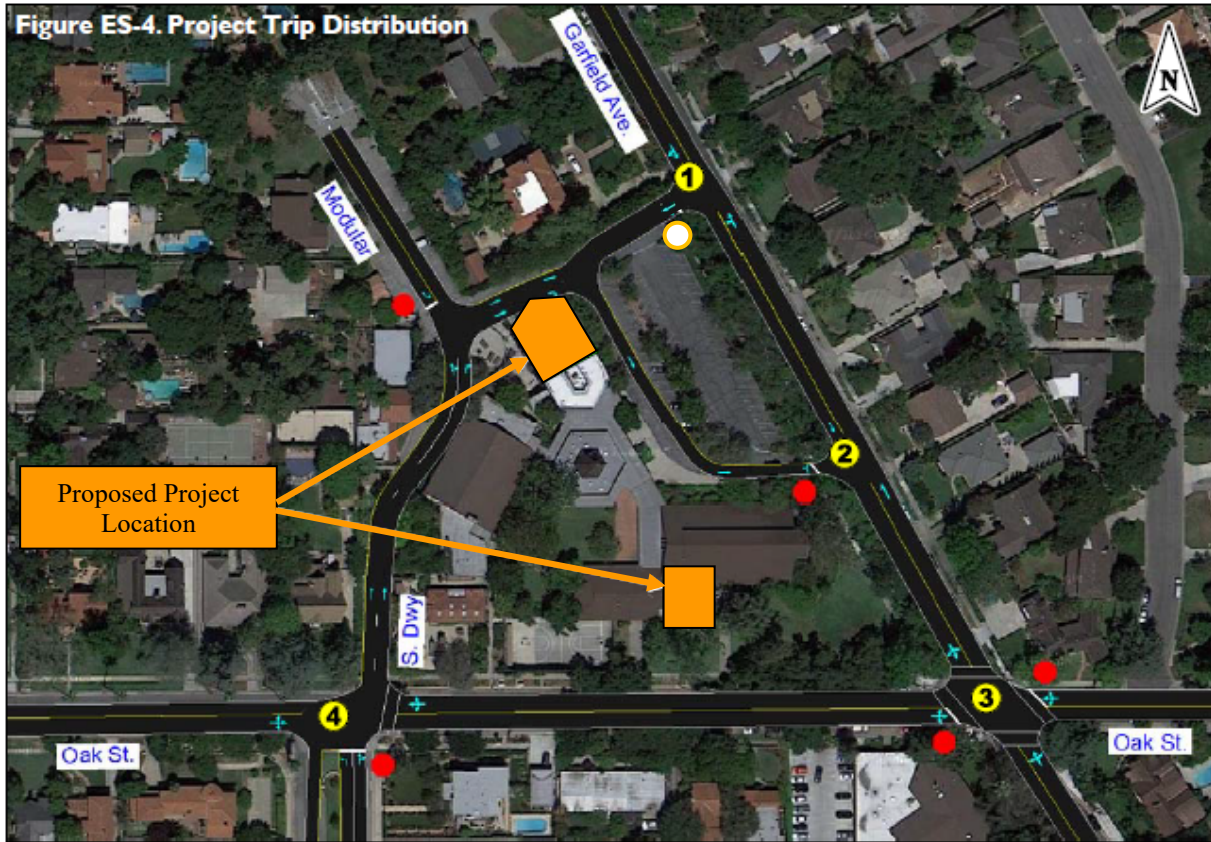
TABLE ES-2
Project Trip Generation [1]

Trip Rates (Employee-Based)				
ITE Land Use Code	Peak Hour	Weekday Trip Rate	In	Out
#565: Day Care Center	AM	4.49 trips / employee	53%	47%
	PM	4.36 trips per employee	47%	53%
Trip Rates (Student-Based)				
ITE Land Use Code	Peak Hour	Trip Rate	In	Out
#565: Day Care Center	AM	0.78 trips / student	53%	47%
	PM	0.79 trips / student	47%	53%

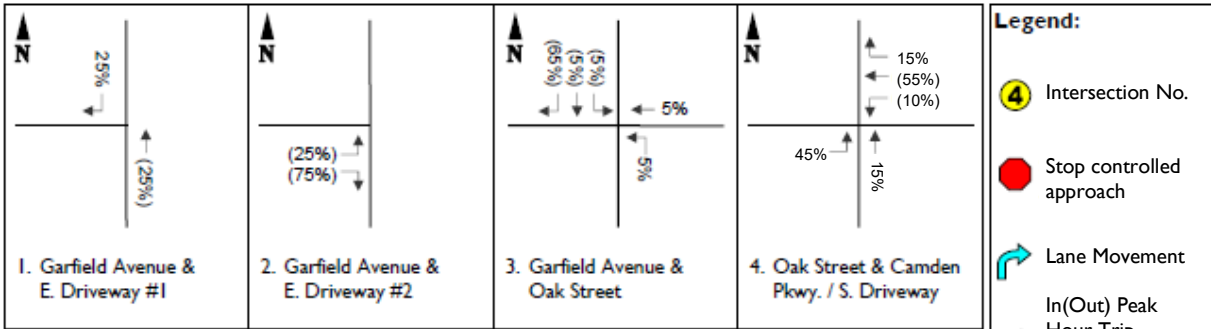
Project Trip Generation (Employee-Based)					
Proposed Land Use	Peak Hour	Size	Weekday Trip Generation		
			Total	In	Out
MyKKids Child Care Center	AM	7 employees	31	17	15
	PM		31	14	16
Project Trip Generation (Student-Based)					
Proposed Land Use	Peak Hour	Size	Weekday Trip Generation		
			Total	In	Out
MyKKids Child Care Center	AM	77 students	60	32	28
	PM		61	29	32

[1] Source: Institute of Transportation Engineers (ITE) *Trip Generation*, 10th Ed.

Note: Highlighted cells indicate the trip generation estimates used in the traffic analysis



AM Peak Hour



PM Peak Hour

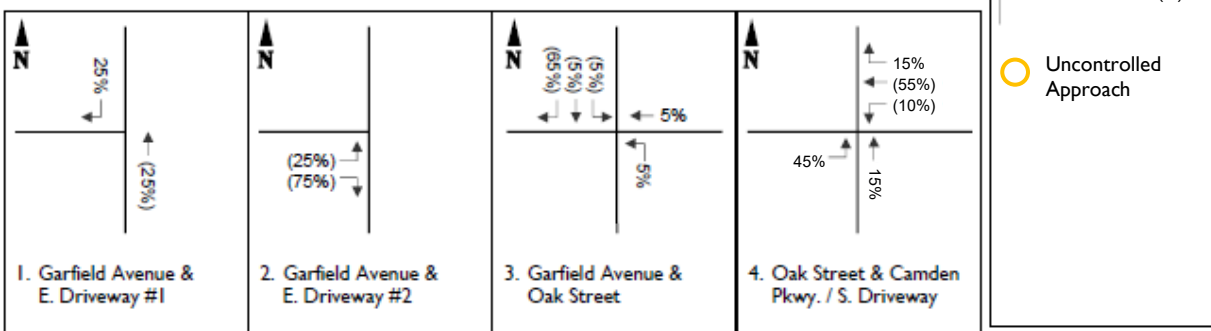
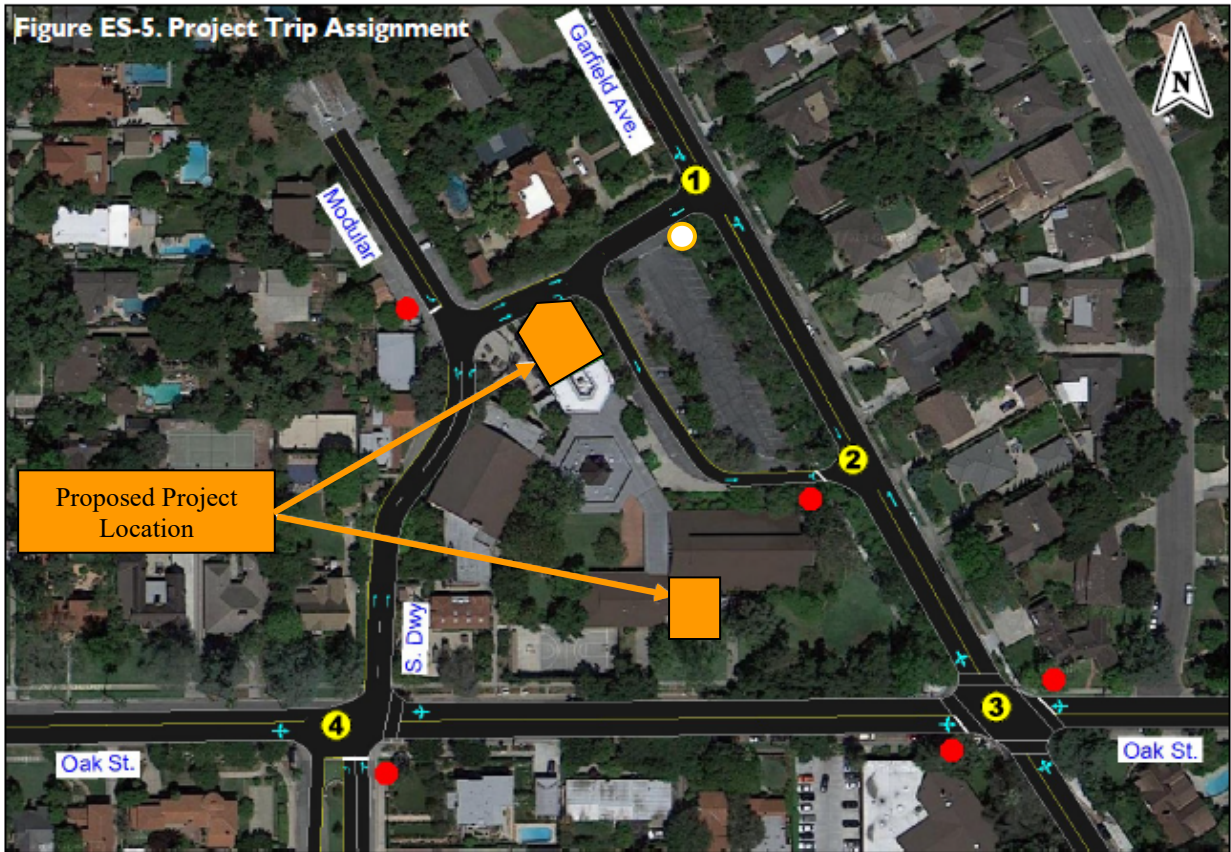
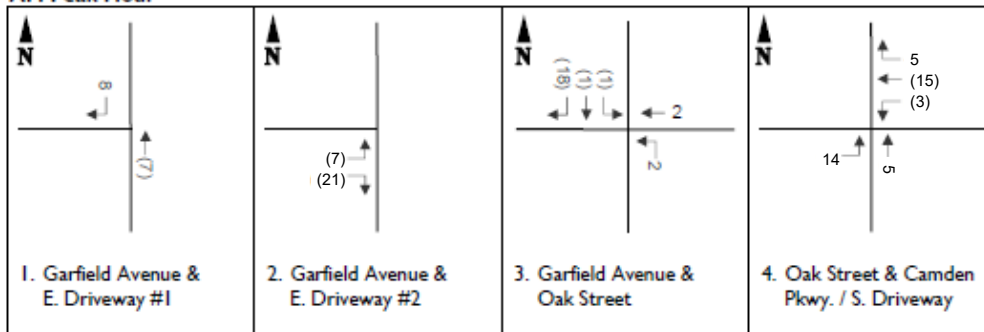


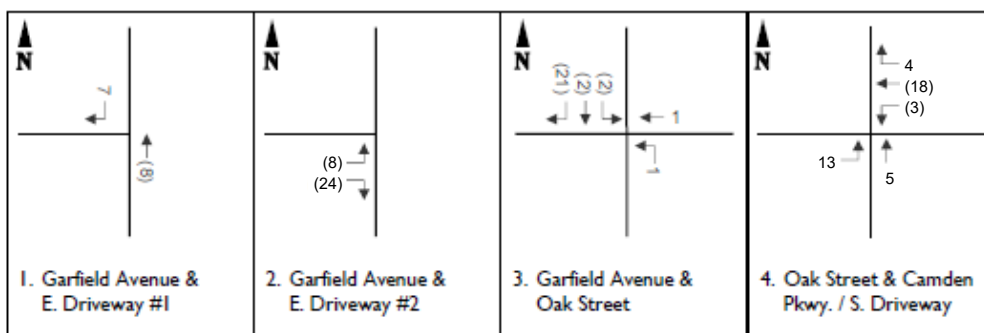
Figure ES-5. Project Trip Assignment



AM Peak Hour



PM Peak Hour



Legend:

- 4 Intersection No.
- Stop controlled approach
- Lane Movement
- ↔ In/Out Peak Hour Trip Assignment
- Uncontrolled Approach

LOS Analysis & Impact Significance Methodologies

To determine the peak-hour intersection operations at project driveways and at the intersection of Garfield Avenue and Oak Street, the City of South Pasadena’s *Traffic Impact Analysis (TIA) Methodology*, adopted May 6, 2020, was referred to in accordance with the CMP and standard industry practice. The average stop-controlled intersection delay per vehicle would be calculated in seconds using the latest HCM intersection analysis methodology, and output an overall Level of Service (LOS) which ranges from LOS “A”, describing generally free flow conditions, to LOS “F”, representing overloaded conditions and heavy delays. The LOS criteria and range of HCM average intersection delay values associated with each LOS grade are summarized in **Table ES-3** below.

TABLE ES-3
Level of Service Definitions for Unsignalized Intersections*

LOS	Description	Control Delay (seconds)
A	<ul style="list-style-type: none"> Nearly all motorists find freedom of operation. Instances with more than one vehicle in queue very seldom. 	≤ 10
B	<ul style="list-style-type: none"> Some motorists begin to consider the delay as an inconvenience. Occasionally more than one vehicle in queue. 	> 10-15
C	<ul style="list-style-type: none"> Majority of motorists feel restricted, but not objectionably so. Several instances with more than one vehicle in queue. 	> 15-25
D	<ul style="list-style-type: none"> Motorists feel objectionably restricted in vehicular movement. Many instances with more than one vehicle in queue. 	> 25-35
E	<ul style="list-style-type: none"> Condition of demand being near or equal to the probable maximum capacity of vehicles than can be accommodated by the movement. Almost always more than one vehicle in queue. Motorists find the delays approaching intolerable levels. 	> 35-50
F	<ul style="list-style-type: none"> Forced Flow. Represents an intersection failure. Caused by geometric and/or operational constraints external to the intersection. 	> 50

* Includes two-way and all-way stop controlled intersections.
 LOS thresholds source: City of South Pasadena *Traffic Impact Analysis Methodology*, May 2020

For unsignalized intersections, the CMP guidelines do not define a standard for significance, and local agencies generally adopt their own method which ranges from analyzing added vehicle delay, to the triggering of traffic signal warrant criteria. However, most cities rely on a consistent scale that is based on the incremental increase of total stop-controlled delay experienced at the intersection, due to the added project trips. The thresholds stated in the City of South Pasadena’s *TIA Methodology* were used as a guideline for the evaluation of unsignalized intersection impacts. This includes an identified traffic impact where the project results in a causing or worsening LOS "F" conditions.

Gap Analysis

Per the City’s most recent request, an analysis of Vehicle Critical Gap was conducted for any applicable project site’s ingress and egress intersections per AASHTO & HCM. This technique has been described as gap acceptance, gaps are measured in time. At un-signalized intersections, drivers must also respect the priority of other drivers that are travelling through the traffic stream. The gap acceptance theory that is used in the analysis of un-signalized intersections is based on the concept of defining the extent that the drivers will be able to utilize a gap of a size of duration. Table ES-4 shows the base critical gap and follow-up times for two-way stop-controlled intersection.

Table ES-4: Base Critical Gaps and Follow-Up Times for TWSC Intersections

Vehicle Movement	Base Critical Gap, $t_{c,base}$ (s)		Base Follow-up Time, $t_{f,base}$ (s)
	Two-Lane Major Street	Four-Lane Major Street	
Left turn from major	4.1	4.1	2.2
Right turn from minor	6.2	6.9	3.3
Through traffic on minor	6.5	6.5	4.0
Left turn from minor	7.1	7.5	3.5

Source: Highway Capacity Manual (2000)

The following findings apply to the worst peak hour (PM). For the intersection of Garfield Avenue & E. Driveway #1, the available gap is 5.8 seconds for vehicles maneuvering left. The available gap for the intersection of Garfield Avenue & E. Driveway #2 is 7.4 seconds for vehicles maneuvering left and 8.3 seconds for vehicles maneuvering right. For the intersection of Oak St & E. Driveway #3, the available gap is 18.4 seconds for vehicles maneuvering left. Upon completing the gap analysis it was therefore concluded that the intersections meet the criteria that assures the safety of maneuvering vehicles entering and exiting the project’s site.

Intersection Levels of Service and Project Impact Significance

Minagar & Associates, Inc. performed a traffic analysis to evaluate the level of service of the surrounding transportation system with and without the project, and to estimate the potential for traffic-related project impacts on adjacent streets and site driveways. The evaluation is based primarily on analyzing the existing weekday traffic volume data of the adjacent roadway system collected by Minagar & Associates, Inc. during the peak morning and afternoon hours. The morning and afternoon peak hours were found to be consistent and within the same 60-minute period for each intersection and survey day, which occurred between 7:30-8:30AM and 5:00-6:00PM.

Intersection level of service (LOS) and project traffic impacts were then analyzed for the following scenarios:





- Existing Year 2020 Conditions. Represents the current weekday morning (AM) and afternoon (PM) peak hour traffic conditions based on intersection turning movement counts

collected by Minagar & Associates, Inc. in June 2014 and projected to 2020 using the 0.21% Growth Factor.

- **Post COVID (2021) without Project Conditions.** Represents the projected weekday morning (AM) and afternoon (PM) peak hour traffic conditions projected from the existing year (2020) conditions.
- **Post COVID (2021) with Project Conditions.** Represents the addition of project-generated AM and PM peak hour traffic volumes on the surrounding transportation system for operations post COVID (2021).

Table ES-5 summarizes the results of the intersection LOS analysis for the *Existing Year 2020*, *Post COVID 2021 without Project*, and *Post COVID Plus Project* conditions. The findings of the LOS analyses indicate that each intersection, including access driveways, is currently operating at an acceptable LOS “A” throughout the day, with the exception of the PM peak hour at Garfield Avenue and Oak Street which is currently operating at deficient LOS “E”. With the addition of project traffic unsignalized vehicle delays would increase at each intersection by varying amounts. The most critical intersection identified in the analysis was Garfield Avenue at Oak Street, which would experience an increase in PM peak hour traffic delays by 6.2 seconds. The addition of project traffic would not cause any intersection’s LOS to be reduced to LOS E or F. Thus, based on the significance criteria of the City’s TIA Methodology, none of the four study intersections would be significantly impacted by the addition of project trips on Garfield Avenue and Oak Street.

Table ES-5
Summary of Intersection Level of Service (LOS) and Project Impacts

Study Intersection			Level of Service and Delay (sec/veh)			
			Existing Year 2020	Post COVID 2021		
Location	Control	Peak Hour	Existing Conditions	Post COVID w/o Project	Post COVID Plus Project	Project Impact
(1) Garfield Avenue at E. Driveway #1	 Two-way	AM PM	A (0.0) A (0.0)	A (0.0) A (0.0)	A (0.0) A (0.0)	+0.0 +0.0 No Impact
(2) Garfield Avenue at E. Driveway #2	 Two-way	AM PM	A (0.0) A (0.1)	A (0.0) A (0.1)	A (0.4) A (1.1)	+0.4 +1.0 No Impact
(3) Garfield Avenue at Oak Street	 Two-way	AM PM	A (7.6) E (41.2)	A (7.7) E (41.1)	A (8.6) E (47.3)	+0.9 +6.2 No Impact
(4) Oak Street at Camden Parkway / S. Driveway	 Two-way	AM PM	A (1.7) A (1.6)	A (1.7) A (1.6)	A (2.1) A (1.9)	+0.4 +0.3 No Impact

An analysis of all Project Scenarios for 2020 and 2021 shows that all of the four (4) studied intersections would continue to operate at their pre-project Levels of Service, and that per the requirements of the California Environmental Quality Act (CEQA), the project would not cause any significant traffic impacts at any the four studied adjacent unsignalized intersections. Therefore, as proposed, the MyKKids Child Care Center project would be compatible with the surrounding transportation environment in the City of South Pasadena and would serve the goals, policies and strategies of the General Plan by implementing a shuttling program to transport students during the peak pick-up and drop-off hours of local schools. **Based on the results of the traffic impact analysis, the project would not be required to provide any traffic mitigation measures upon the adjacent roadway system.**

Vehicle Miles Traveled (VMT) Analysis & Screening Criteria

California Senate Bill 743 (SB 743) directs the State Office of Planning and Research (OPR) to amend the California Environmental Quality Act (CEQA) Guidelines for evaluating transportation impacts to provide alternatives to Level of Service that “promote the reduction of greenhouse gas emissions, the development of multimodal transportation networks, and a diversity of land uses.”

In December 2018, the California Natural Resources Agency certified and adopted the updated CEQA Guidelines package. The amended CEQA Guidelines, specifically Section 15064.3, recommend the use of Vehicle Miles Travelled (VMT) as the primary metric for the evaluation of transportation impacts associated with land use and transportation projects. In general terms, VMT quantifies the amount and distance of automobile travel attributable to a project or region. Agencies may currently opt-in to applying the updated CEQA guidelines for VMT analysis and implementation is required State-wide by July 1, 2020.

The updated CEQA Guidelines allow for lead agency discretion in establishing methodologies and thresholds provided there is substantial evidence to demonstrate that the established procedures promote the intended goals of the legislation. Where quantitative models or methods are unavailable, Section 15064.3 allows agencies to assess VMT qualitatively using factors such as availability of transit and proximity to other destinations.

The Technical Advisory on Evaluating Transportation Impacts in CEQA (State of California, December 2018) [“Technical Advisory”] provides technical considerations regarding methodologies and thresholds with a focus on office, residential, and retail developments as these projects tend to have the greatest influence on VMT. At publishing of this report, many jurisdictions are currently in the process of developing updated procedures for VMT analysis.

Using these guidelines alongside the ones set by the City’s *TIA Methodology*, published May 2020. The primary screening criteria for this Project is the project type, and if the Project qualifies, then **further VMT analysis is not required**.

Table ES-6. Project Screening for VMT Analysis

VMT Screening Evaluation		
Project Type	Land Use Type	Traffic Impact Analysis Required?
Day Care	Community Serving	NO

As shown in **Table ES-6**, the Project’s land use type **is not** required for further analysis per the City of South Pasadena *TIA Methodology* and OPR, **therefore further VMT and Traffic Impact Analysis for the Project is not required**.

Conclusions

The overall analysis result concludes that the traffic generated by MyKKids (Proposed Project), Co-Op (Existing), and Kids Klub (Existing) would not have any significant impact on the overall traffic. Moreover, the analysis shows that all of the four (4) studied intersections would continue to operate at their pre-project Levels of Service, and that per the requirements of the California Environmental Quality Act (CEQA), the project would not cause any significant traffic impacts at any the four studied adjacent unsignalized intersections. Therefore, as proposed, the MyKKids Child Care Center project would be compatible with the surrounding transportation environment in the City of South Pasadena and would serve the goals, policies and strategies of the General Plan by implementing a shuttling program to transport students during the peak pick-up and drop-off hours of local schools. Based on the results of the analysis, **the generated traffic of the Proposed MyKKids Child Care Center project in addition to the Existing traffic of Co-Op and Kids Klub would not have any potential impact on traffic, it would be compatible with the surrounding transportation environment in the City of South Pasadena and would not be required to provide any traffic mitigation measures upon the adjacent roadway system.**

Additionally, as per the City of South Pasadena’s *TIA Methodology* as well as the Governor’s Office of Planning and Research, **this Project passes the screening criteria for Vehicle Miles Traveled (VMT) and thereby is not required to perform additional VMT analysis or Traffic Impact Studies.**



Appendix A: Turning Movement Count (TMC) Data



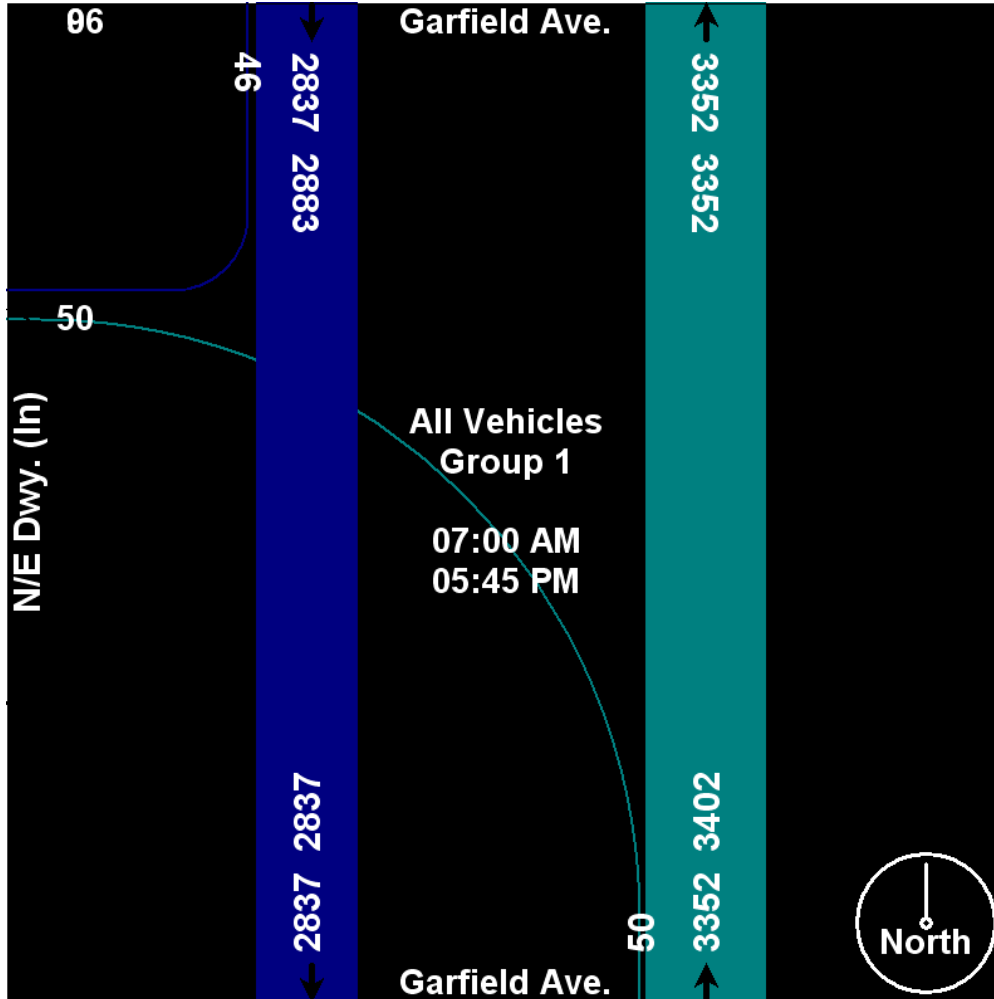


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File Name : 01-aft_GarfieldAv @ NEDwy(In)
Site Code : 01
Start Date : 11/18/2014
Page No : 2





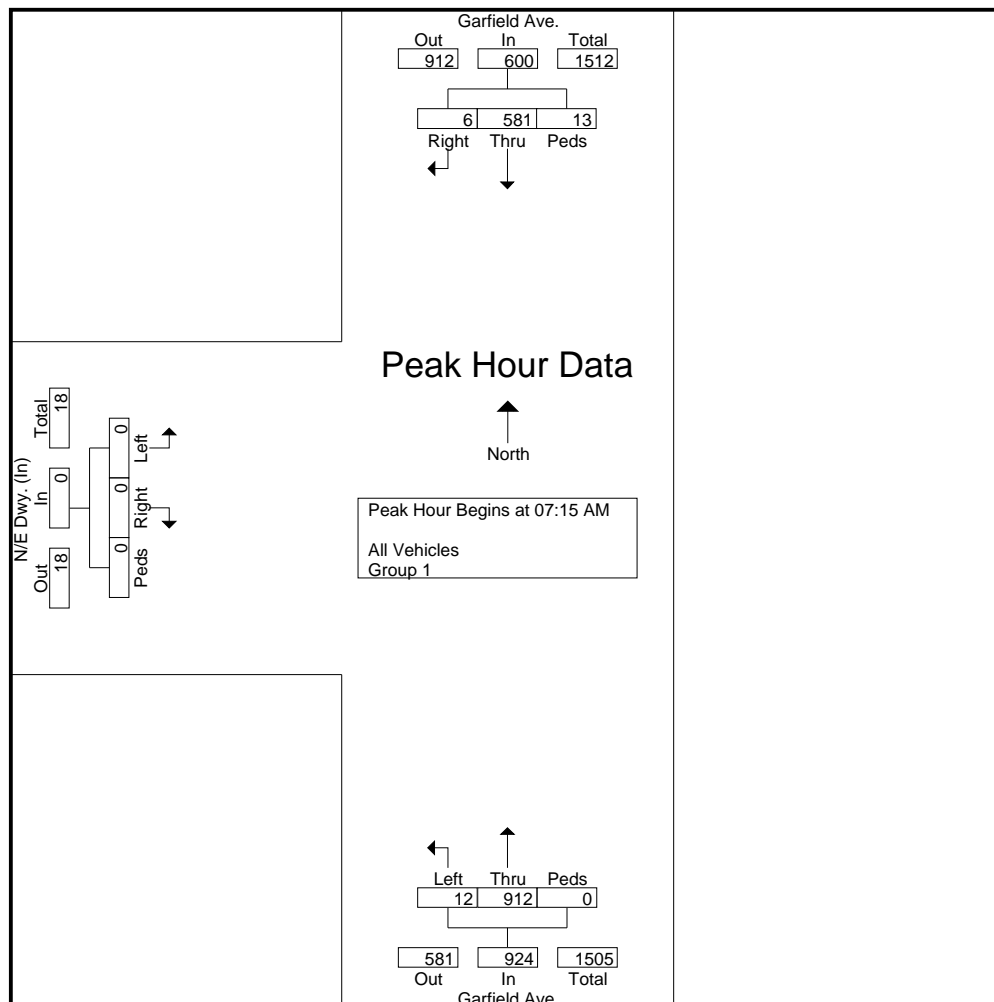
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Start Time	N/E Dwy. (In) Eastbound				Garfield Ave. Northbound				Garfield Ave. Southbound				Int. Total
	Left	Right	Peds	App. Total	Left	Thru	Peds	App. Total	Thru	Right	Peds	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:15 AM													
07:15 AM	0	0	0	0	0	231	0	231	133	1	4	138	369
07:30 AM	0	0	0	0	1	260	0	261	149	0	3	152	413
07:45 AM	0	0	0	0	10	201	0	211	159	0	4	163	374
08:00 AM	0	0	0	0	1	220	0	221	140	5	2	147	368
Total Volume	0	0	0	0	12	912	0	924	581	6	13	600	1524
% App. Total	0	0	0	0	1.3	98.7	0		96.8	1	2.2		
PHF	.000	.000	.000	.000	.300	.877	.000	.885	.914	.300	.813	.920	.923





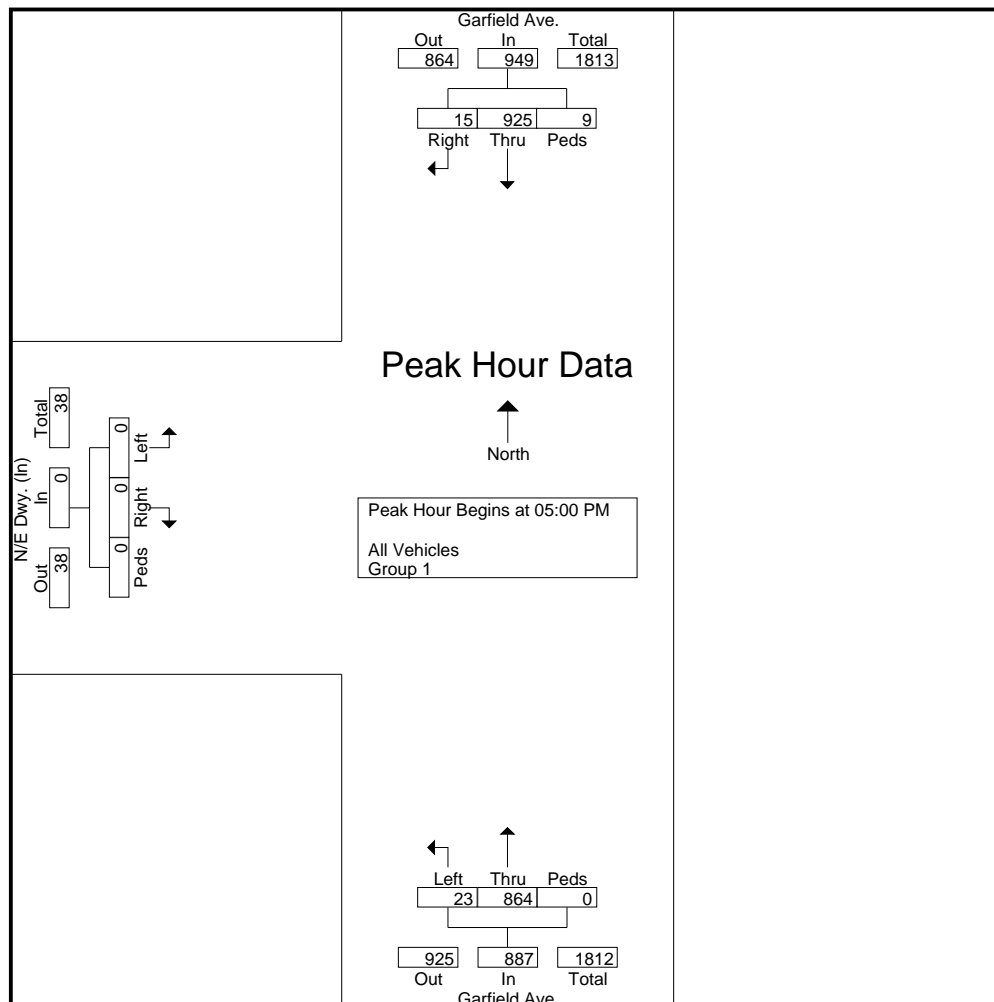
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Start Time	N/E Dwy. (In) Eastbound				Garfield Ave. Northbound				Garfield Ave. Southbound				Int. Total
	Left	Right	Peds	App. Total	Left	Thru	Peds	App. Total	Thru	Right	Peds	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 05:00 PM													
05:00 PM	0	0	0	0	6	191	0	197	182	3	2	187	384
05:15 PM	0	0	0	0	12	215	0	227	221	4	1	226	453
05:30 PM	0	0	0	0	1	221	0	222	254	4	0	258	480
05:45 PM	0	0	0	0	4	237	0	241	268	4	6	278	519
Total Volume	0	0	0	0	23	864	0	887	925	15	9	949	1836
% App. Total	0	0	0	0	2.6	97.4	0		97.5	1.6	0.9		
PHF	.000	.000	.000	.000	.479	.911	.000	.920	.863	.938	.375	.853	.884



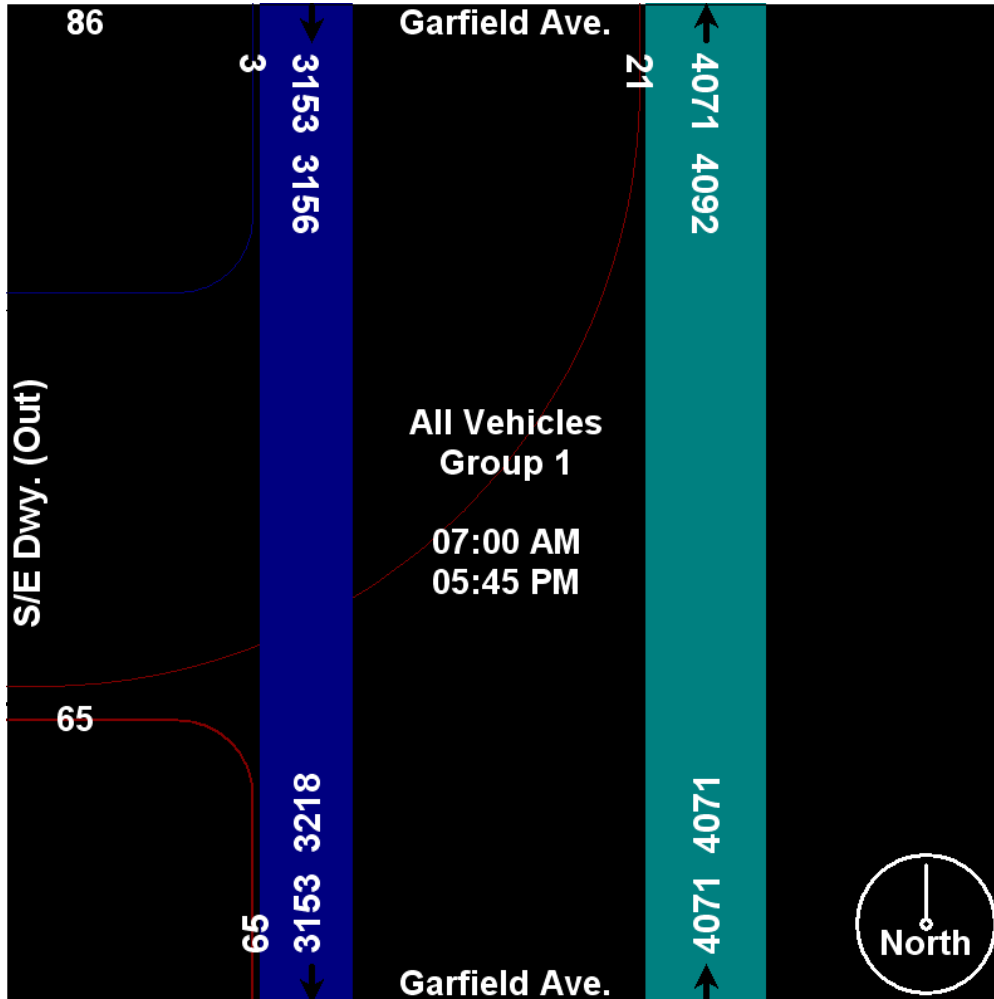


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File Name : 02-aft_GarfieldAv @ SEDwy(Out)
Site Code : 02
Start Date : 11/18/2014
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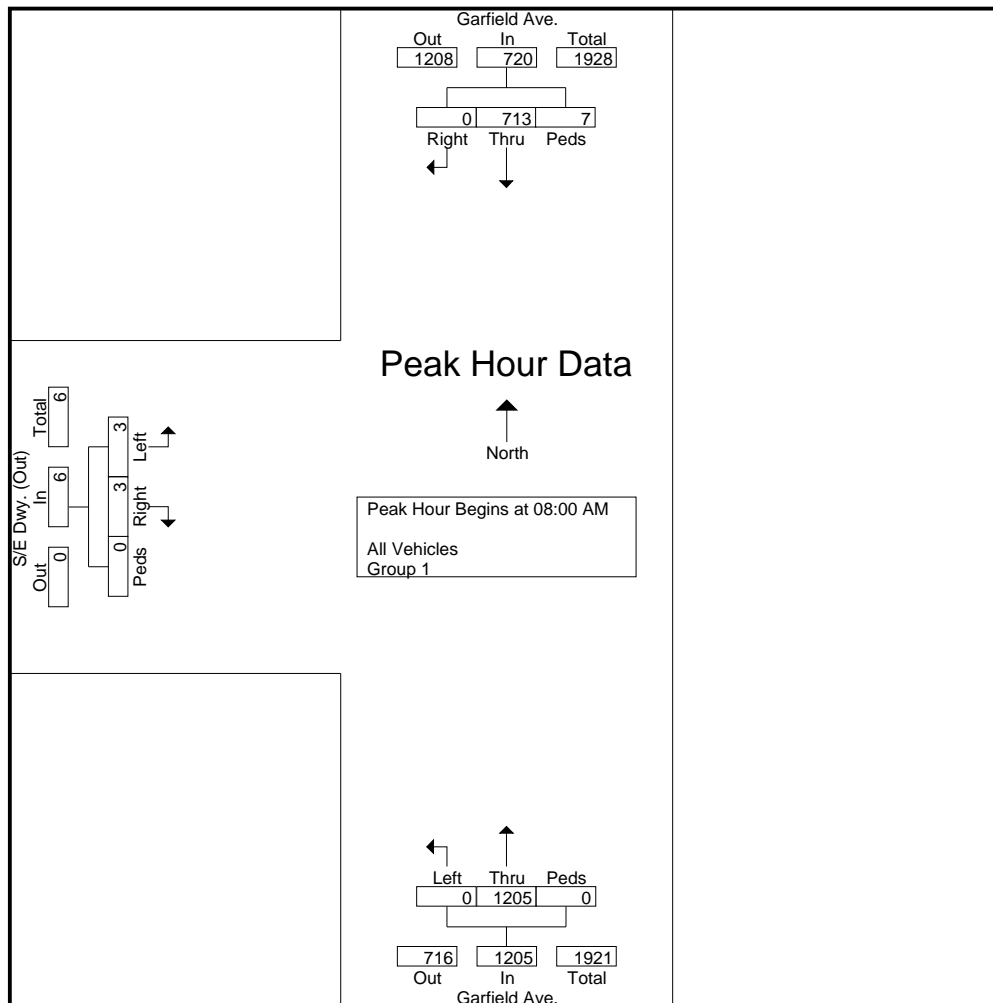
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File Name : 02-aft_GarfieldAv @ SEDwy(Out)
Site Code : 02
Start Date : 11/18/2014
Page No : 3

Start Time	S/E Dwy. (Out) Eastbound				Garfield Ave. Northbound				Garfield Ave. Southbound				Int. Total
	Left	Right	Peds	App. Total	Left	Thru	Peds	App. Total	Thru	Right	Peds	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 08:00 AM													
08:00 AM	1	2	0	3	0	278	0	278	184	0	1	185	466
08:15 AM	1	1	0	2	0	272	0	272	153	0	1	154	428
08:30 AM	1	0	0	1	0	317	0	317	196	0	2	198	516
08:45 AM	0	0	0	0	0	338	0	338	180	0	3	183	521
Total Volume	3	3	0	6	0	1205	0	1205	713	0	7	720	1931
% App. Total	50	50	0		0	100	0		99	0	1		
PHF	.750	.375	.000	.500	.000	.891	.000	.891	.909	.000	.583	.909	.927





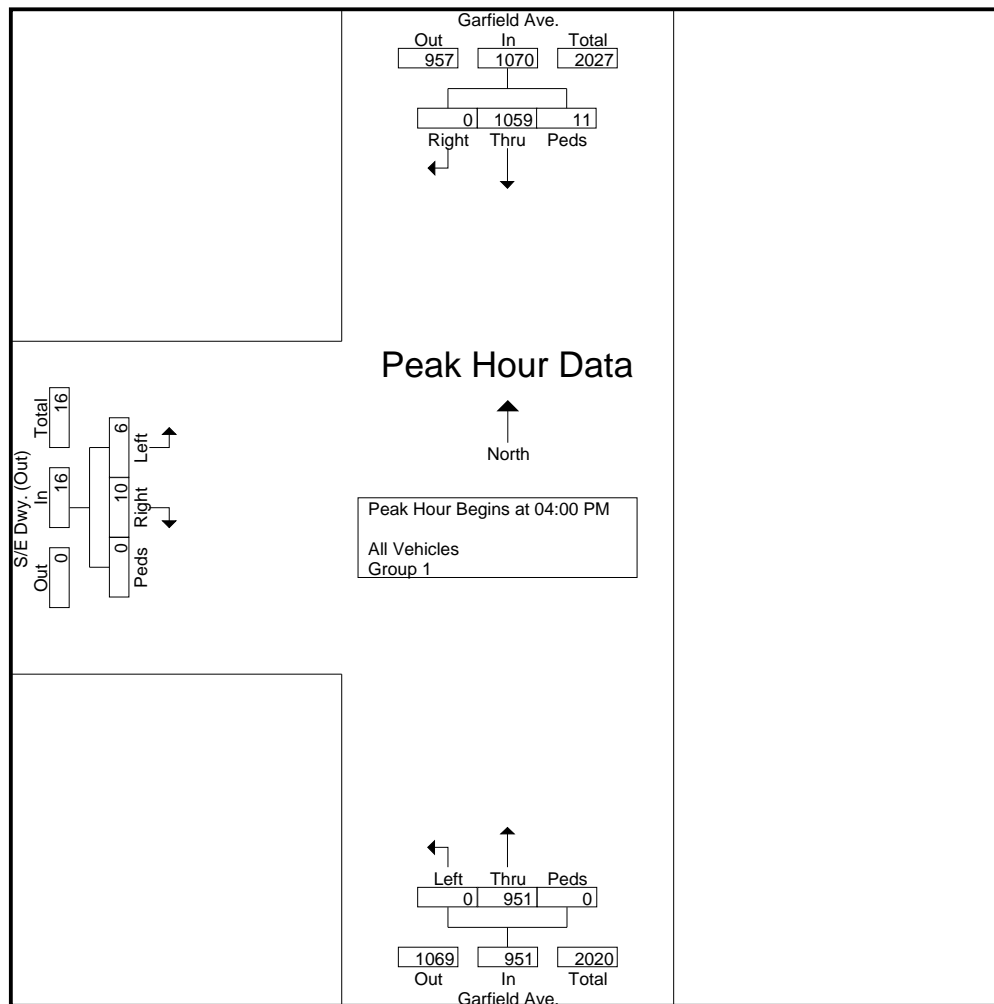
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File Name : 02-aft_GarfieldAv @ SEDwy(Out)
Site Code : 02
Start Date : 11/18/2014
Page No : 4

Start Time	S/E Dwy. (Out) Eastbound				Garfield Ave. Northbound				Garfield Ave. Southbound				Int. Total
	Left	Right	Peds	App. Total	Left	Thru	Peds	App. Total	Thru	Right	Peds	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 04:00 PM													
04:00 PM	1	1	0	2	0	246	0	246	215	0	1	216	464
04:15 PM	2	3	0	5	0	203	0	203	254	0	5	259	467
04:30 PM	2	2	0	4	0	282	0	282	333	0	5	338	624
04:45 PM	1	4	0	5	0	220	0	220	257	0	0	257	482
Total Volume	6	10	0	16	0	951	0	951	1059	0	11	1070	2037
% App. Total	37.5	62.5	0		0	100	0		99	0	1		
PHF	.750	.625	.000	.800	.000	.843	.000	.843	.795	.000	.550	.791	.816





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File Name : 03-aft_GarfieldAv @ OakSt
Site Code : 03
Start Date : 11/19/2014
Page No : 1

Groups Printed- All Vehicles - Group 1

Start Time	Oak St. Eastbound				Oak St. Westbound				Garfield Ave. Northbound				Garfield Ave. Southbound				Int. Total
	Left	Thru	Right	Peds	Left	Thru	Right	Peds	Left	Thru	Right	Peds	Left	Thru	Right	Peds	
07:00 AM	3	1	7	2	1	1	3	0	7	228	3	0	1	63	6	7	333
07:15 AM	3	1	12	0	0	0	9	0	11	333	1	0	1	108	2	5	486
07:30 AM	6	3	11	1	0	4	3	0	15	292	1	0	3	100	5	6	450
07:45 AM	4	9	18	0	2	5	5	1	24	244	0	0	2	150	11	8	483
Total	16	14	48	3	3	10	20	1	57	1097	5	0	7	421	24	26	1752
08:00 AM	9	10	15	4	0	4	9	3	16	266	5	0	2	115	14	6	478
08:15 AM	12	5	18	8	1	1	7	4	8	201	2	0	0	137	3	3	410
08:30 AM	4	2	6	3	2	4	2	1	19	340	4	0	3	159	7	5	561
08:45 AM	10	5	17	11	0	2	3	2	19	278	2	1	3	164	12	9	538
Total	35	22	56	26	3	11	21	10	62	1085	13	1	8	575	36	23	1987
BREAK																	
04:00 PM	5	2	17	5	0	11	4	0	12	209	4	3	9	179	5	3	468
04:15 PM	4	3	12	5	0	1	8	2	5	224	2	4	4	187	5	12	478
04:30 PM	10	2	16	2	0	1	1	1	19	182	3	0	4	287	9	1	538
04:45 PM	6	2	14	0	2	7	0	0	14	228	7	3	5	251	10	1	550
Total	25	9	59	12	2	20	13	3	50	843	16	10	22	904	29	17	2034
05:00 PM	9	1	15	0	1	2	5	6	8	218	2	1	6	229	18	9	530
05:15 PM	7	3	16	5	0	0	4	3	15	313	4	0	6	258	15	6	655
05:30 PM	9	9	15	7	4	5	5	0	15	248	4	2	3	215	11	2	554
05:45 PM	11	16	25	2	0	0	3	0	21	221	3	0	5	353	10	4	674
Total	36	29	71	14	5	7	17	9	59	1000	13	3	20	1055	54	21	2413
Grand Total	112	74	234	55	13	48	71	23	228	4025	47	14	57	2955	143	87	8186
Apprch %	23.6	15.6	49.3	11.6	8.4	31	45.8	14.8	5.3	93.3	1.1	0.3	1.8	91.1	4.4	2.7	
Total %	1.4	0.9	2.9	0.7	0.2	0.6	0.9	0.3	2.8	49.2	0.6	0.2	0.7	36.1	1.7	1.1	
All Vehicles	112	74	234	55	13	48	71	23	228	4025	47	14	57	2955	143	87	8186
% All Vehicles	100	100	100	100	100	100	100	100	100	100	100	100	100	100	100	100	100
Group 1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Group 1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

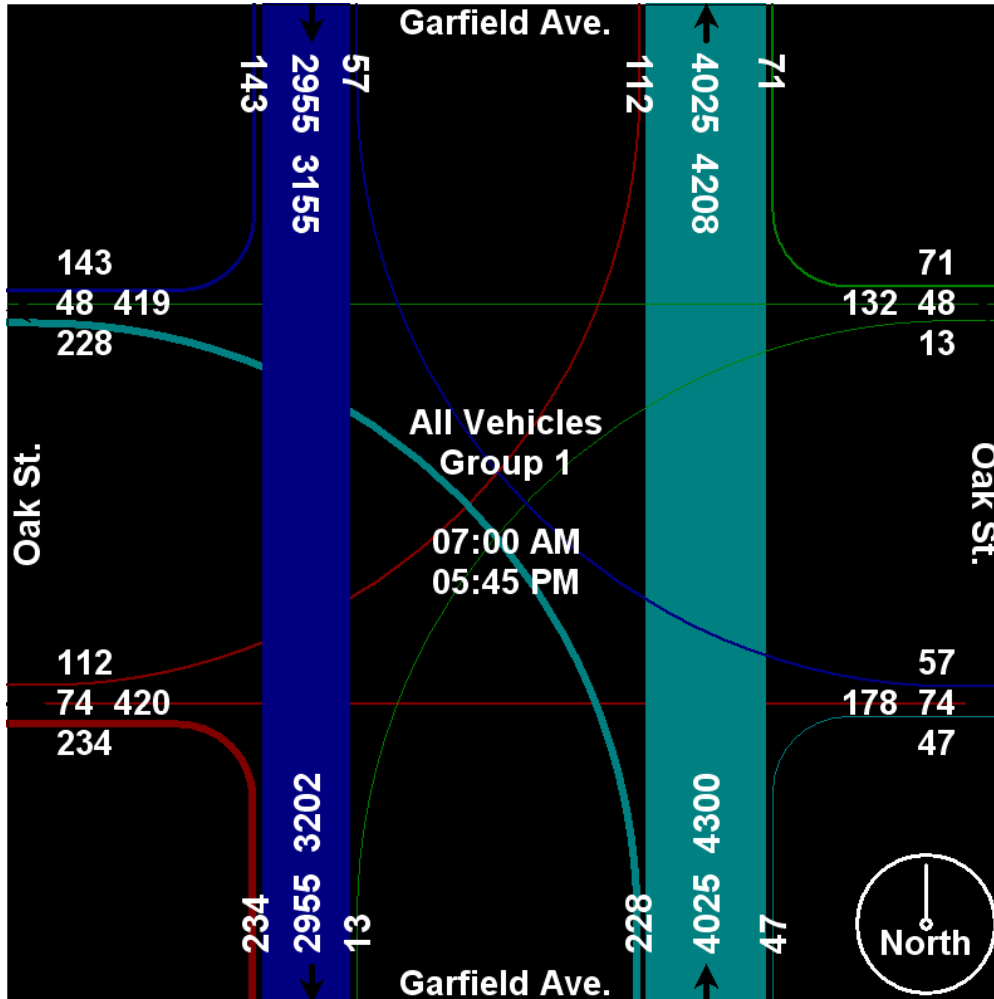


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File Name : 03-aft_GarfieldAv @ OakSt
Site Code : 03
Start Date : 11/19/2014
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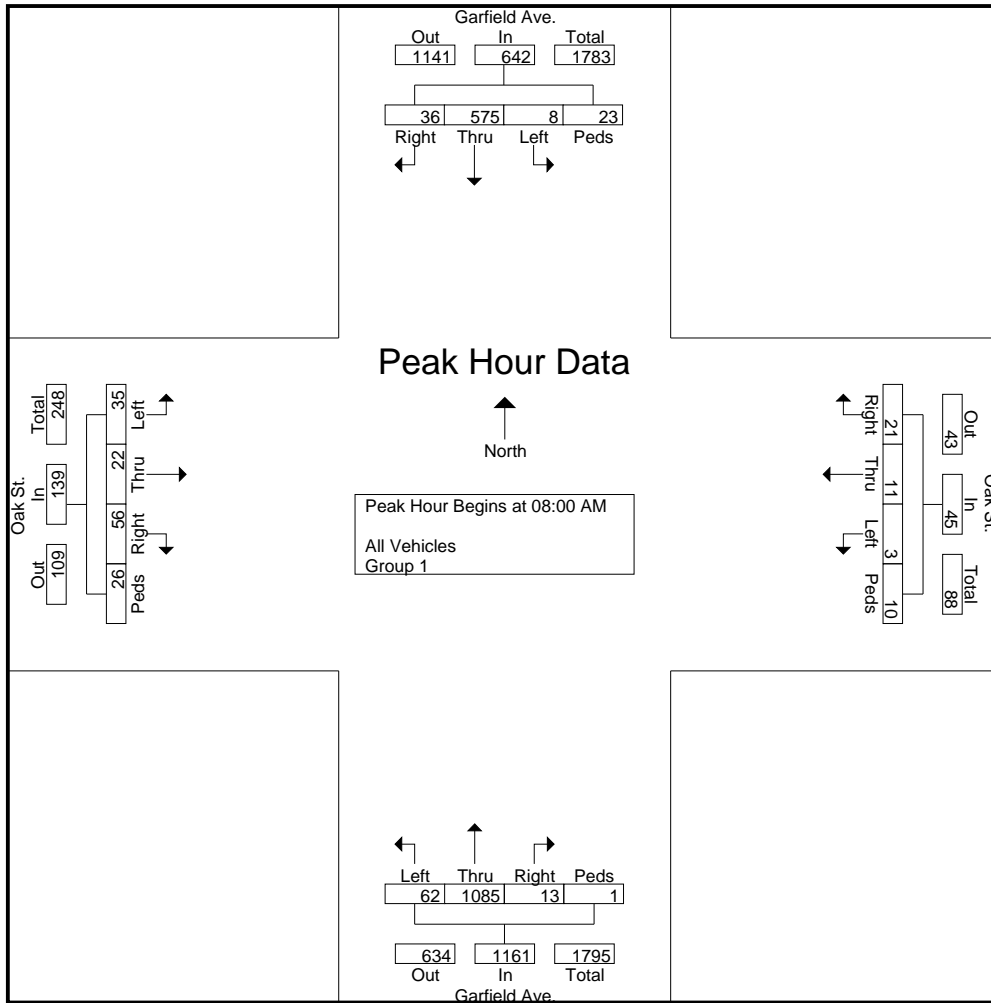
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Start Date : 11/19/2014
Page No : 3

Start Time	Oak St. Eastbound					Oak St. Westbound					Garfield Ave. Northbound					Garfield Ave. Southbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 08:00 AM																					
08:00 AM	9	10	15	4	38	0	4	9	3	16	16	266	5	0	287	2	115	14	6	137	478
08:15 AM	12	5	18	8	43	1	1	7	4	13	8	201	2	0	211	0	137	3	3	143	410
08:30 AM	4	2	6	3	15	2	4	2	1	9	19	340	4	0	363	3	159	7	5	174	561
08:45 AM	10	5	17	11	43	0	2	3	2	7	19	278	2	1	300	3	164	12	9	188	538
Total Volume	35	22	56	26	139	3	11	21	10	45	62	1085	13	1	1161	8	575	36	23	642	1987
% App. Total	25.2	15.8	40.3	18.7		6.7	24.4	46.7	22.2		5.3	93.5	1.1	0.1		1.2	89.6	5.6	3.6		
PHF	.729	.550	.778	.591	.808	.375	.688	.583	.625	.703	.816	.798	.650	.250	.800	.667	.877	.643	.639	.854	.885





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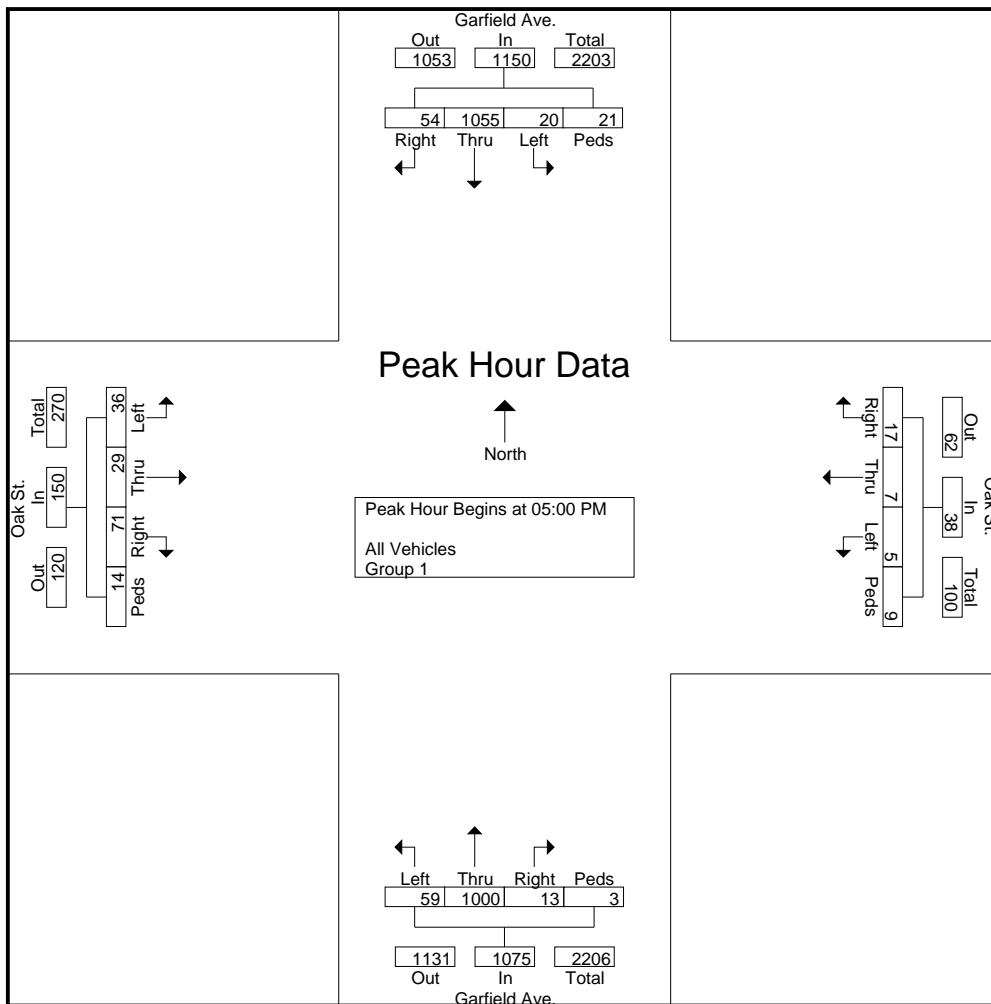
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File Name : 03-aft_GarfieldAv @ OakSt
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Page No : 4

Start Time	Oak St. Eastbound					Oak St. Westbound					Garfield Ave. Northbound					Garfield Ave. Southbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
05:00 PM	9	1	15	0	25	1	2	5	6	14	8	218	2	1	229	6	229	18	9	262	530
05:15 PM	7	3	16	5	31	0	0	4	3	7	15	313	4	0	332	6	258	15	6	285	655
05:30 PM	9	9	15	7	40	4	5	5	0	14	15	248	4	2	269	3	215	11	2	231	554
05:45 PM	11	16	25	2	54	0	0	3	0	3	21	221	3	0	245	5	353	10	4	372	674
Total Volume	36	29	71	14	150	5	7	17	9	38	59	1000	13	3	1075	20	1055	54	21	1150	2413
% App. Total	24	19.3	47.3	9.3		13.2	18.4	44.7	23.7		5.5	93	1.2	0.3		1.7	91.7	4.7	1.8		
PHF	.818	.453	.710	.500	.694	.313	.350	.850	.375	.679	.702	.799	.813	.375	.809	.833	.747	.750	.583	.773	.895

Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1

Peak Hour for Entire Intersection Begins at 05:00 PM





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Site Name : 04-aft_OakSt @ CamdenPky-SDwy(In)
Site Code : 04
Start Date : 11/20/2014
Page No : 1

Groups Printed- All Vehicles - Group 1

Start Time	Oak St. Eastbound				Oak St. Westbound				Camden Pkwy. Northbound				S. Dwy. (In) Southbound				Int. Total
	Left	Thru	Right	Peds	Left	Thru	Right	Peds	Left	Thru	Right	Peds	Left	Thru	Right	Peds	
07:00 AM	1	16	0	4	1	23	0	6	2	0	3	0	0	0	0	0	56
07:15 AM	0	12	1	1	1	22	0	5	4	0	1	0	1	0	1	0	49
07:30 AM	0	11	0	7	0	24	1	2	2	1	4	0	0	0	0	0	52
07:45 AM	0	25	2	1	0	50	0	10	3	0	5	0	0	0	0	0	96
Total	1	64	3	13	2	119	1	23	11	1	13	0	1	0	1	0	253
08:00 AM	0	32	0	7	0	22	2	0	9	1	1	2	1	0	0	0	77
08:15 AM	0	32	1	1	3	22	2	3	8	0	2	0	0	0	0	0	74
08:30 AM	1	20	4	4	5	36	1	4	6	1	4	0	0	0	0	0	86
08:45 AM	0	24	2	3	2	18	0	2	0	0	2	0	1	0	0	0	54
Total	1	108	7	15	10	98	5	9	23	2	9	2	2	0	0	0	291
BREAK																	
04:00 PM	0	23	3	5	1	32	2	2	1	0	1	0	0	0	0	0	70
04:15 PM	3	43	4	3	1	32	1	3	3	0	3	0	0	0	0	0	96
04:30 PM	0	47	4	2	1	28	1	1	7	0	5	0	0	0	0	0	96
04:45 PM	0	26	0	4	0	22	2	2	4	1	1	6	0	0	0	0	68
Total	3	139	11	14	3	114	6	8	15	1	10	6	0	0	0	0	330
05:00 PM	2	28	2	1	0	47	1	3	4	1	1	2	0	0	0	0	92
05:15 PM	3	48	3	2	1	41	1	2	1	0	0	4	0	0	0	0	106
05:30 PM	3	43	10	7	2	40	0	1	5	0	3	1	0	0	1	0	116
05:45 PM	4	37	3	5	3	72	3	2	3	0	12	3	0	0	0	0	147
Total	12	156	18	15	6	200	5	8	13	1	16	10	0	0	1	0	461
Grand Total	17	467	39	57	21	531	17	48	62	5	48	18	3	0	2	0	1335
Apprch %	2.9	80.5	6.7	9.8	3.4	86.1	2.8	7.8	46.6	3.8	36.1	13.5	60	0	40	0	
Total %	1.3	35	2.9	4.3	1.6	39.8	1.3	3.6	4.6	0.4	3.6	1.3	0.2	0	0.1	0	
All Vehicles	17	467	39	57	21	531	17	48	62	5	48	18	3	0	2	0	1335
% All Vehicles	100	100	100	100	100	100	100	100	100	100	100	100	100	0	100	0	100
Group 1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Group 1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

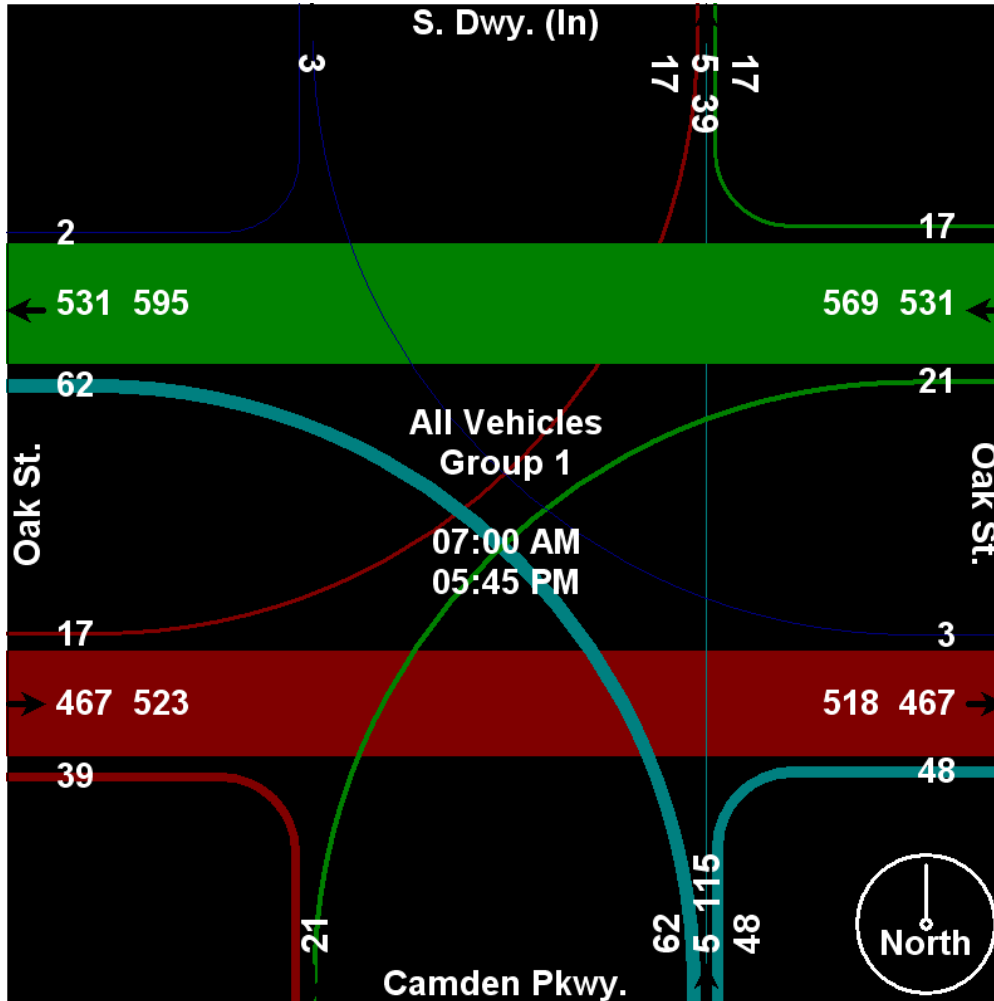


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File Name : 04-aft_OakSt @ CamdenPky-SDwy(In)
Site Code : 04
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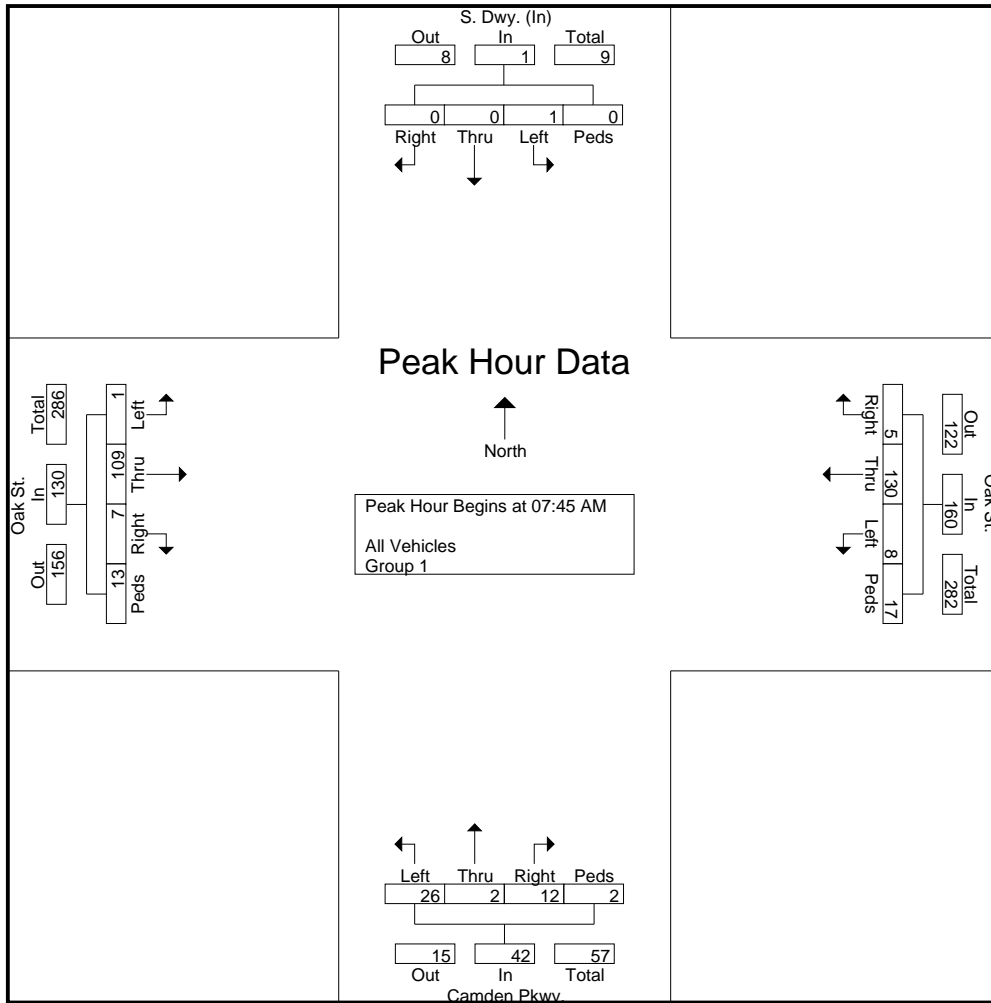
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Project Name : 04-aft_OakSt @ CamdenPky-SDwy(In)
Site Code : 04
Start Date : 11/20/2014
Page No : 3

Start Time	Oak St. Eastbound					Oak St. Westbound					Camden Pkwy. Northbound					S. Dwy. (In) Southbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:45 AM																					
07:45 AM	0	25	2	1	28	0	50	0	10	60	3	0	5	0	8	0	0	0	0	0	96
08:00 AM	0	32	0	7	39	0	22	2	0	24	9	1	1	2	13	1	0	0	0	1	77
08:15 AM	0	32	1	1	34	3	22	2	3	30	8	0	2	0	10	0	0	0	0	0	74
08:30 AM	1	20	4	4	29	5	36	1	4	46	6	1	4	0	11	0	0	0	0	0	86
Total Volume	1	109	7	13	130	8	130	5	17	160	26	2	12	2	42	1	0	0	0	1	333
% App. Total	0.8	83.8	5.4	10		5	81.2	3.1	10.6		61.9	4.8	28.6	4.8		100	0	0	0		
PHF	.250	.852	.438	.464	.833	.400	.650	.625	.425	.667	.722	.500	.600	.250	.808	.250	.000	.000	.000	.250	.867





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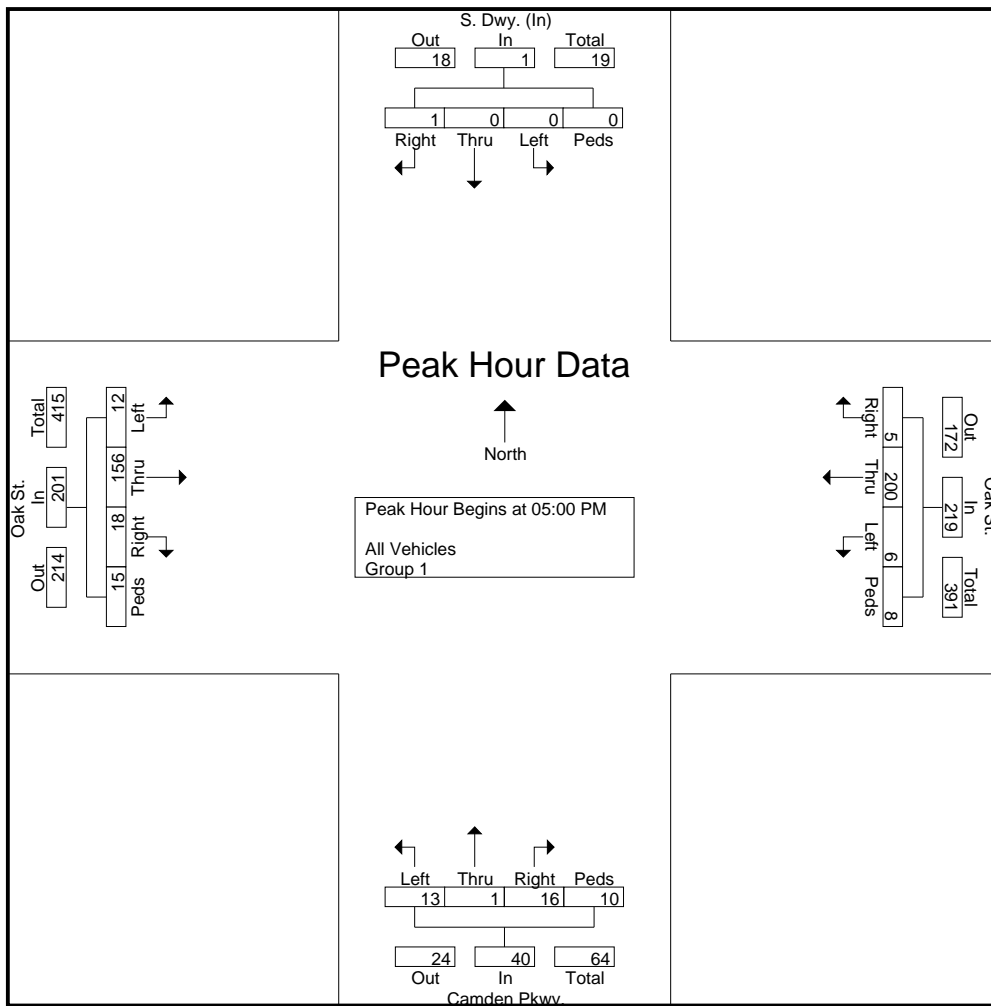
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Site Name : 04-aft_OakSt @ CamdenPky-SDwy(In)
Site Code : 04
Start Date : 11/20/2014
Page No : 4

Start Time	Oak St. Eastbound					Oak St. Westbound					Camden Pkwy. Northbound					S. Dwy. (In) Southbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
05:00 PM	2	28	2	1	33	0	47	1	3	51	4	1	1	2	8	0	0	0	0	0	92
05:15 PM	3	48	3	2	56	1	41	1	2	45	1	0	0	4	5	0	0	0	0	0	106
05:30 PM	3	43	10	7	63	2	40	0	1	43	5	0	3	1	9	0	0	1	0	1	116
05:45 PM	4	37	3	5	49	3	72	3	2	80	3	0	12	3	18	0	0	0	0	0	147
Total Volume	12	156	18	15	201	6	200	5	8	219	13	1	16	10	40	0	0	1	0	1	461
% App. Total	6	77.6	9	7.5		2.7	91.3	2.3	3.7		32.5	2.5	40	25		0	0	100	0		
PHF	.750	.813	.450	.536	.798	.500	.694	.417	.667	.684	.650	.250	.333	.625	.556	.000	.000	.250	.000	.250	.784

Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1

Peak Hour for Entire Intersection Begins at 05:00 PM





Appendix B: SYNCHRO 10.0 Reports



HCM 6th TWSC
1: Garfield & Driveway 1

12/27/2020

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	0	0	0	1020	684	3
Future Vol, veh/h	0	0	0	1020	684	3
Conflicting Peds, #/hr	12	12	12	0	0	12
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	0	0	1109	743	3

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1878	769	758	0	-	0
Stage 1	757	-	-	-	-	-
Stage 2	1121	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	78	401	853	-	-	-
Stage 1	463	-	-	-	-	-
Stage 2	311	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	76	392	843	-	-	-
Mov Cap-2 Maneuver	76	-	-	-	-	-
Stage 1	458	-	-	-	-	-
Stage 2	308	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	0	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	843	-	-	-	-
HCM Lane V/C Ratio	-	-	-	-	-
HCM Control Delay (s)	0	-	0	-	-
HCM Lane LOS	A	-	A	-	-
HCM 95th %tile Q(veh)	0	-	-	-	-

HCM 6th TWSC
2: Garfield & Driveway 2

12/27/2020

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↔			↑	↑	
Traffic Vol, veh/h	0	1	0	1020	684	0
Future Vol, veh/h	0	1	0	1020	684	0
Conflicting Peds, #/hr	12	12	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	1	0	1109	743	0

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1864	755	-	0	-	0
Stage 1	743	-	-	-	-	-
Stage 2	1121	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	-	-
Pot Cap-1 Maneuver	80	409	0	-	-	0
Stage 1	470	-	0	-	-	0
Stage 2	311	-	0	-	-	0
Platoon blocked, %				-	-	
Mov Cap-1 Maneuver	80	404	-	-	-	-
Mov Cap-2 Maneuver	80	-	-	-	-	-
Stage 1	470	-	-	-	-	-
Stage 2	311	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	13.9	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT EBLn1	SBT
Capacity (veh/h)	- 404	-
HCM Lane V/C Ratio	- 0.003	-
HCM Control Delay (s)	- 13.9	-
HCM Lane LOS	- B	-
HCM 95th %tile Q(veh)	- 0	-

HCM 6th TWSC
3: Garfield & Oak

12/27/2020

Intersection												
Int Delay, s/veh	7.6											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	31	10	47	11	11	5	75	875	6	3	379	61
Future Vol, veh/h	31	10	47	11	11	5	75	875	6	3	379	61
Conflicting Peds, #/hr	19	0	21	21	0	7	21	0	21	7	0	19
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	34	11	51	12	12	5	82	951	7	3	412	66

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1618	1615	487	1643	1645	995	499	0	0	979	0	0
Stage 1	472	472	-	1140	1140	-	-	-	-	-	-	-
Stage 2	1146	1143	-	503	505	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	83	104	581	80	99	297	1065	-	-	705	-	-
Stage 1	573	559	-	244	276	-	-	-	-	-	-	-
Stage 2	242	275	-	551	540	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	60	83	558	55	79	286	1044	-	-	691	-	-
Mov Cap-2 Maneuver	60	83	-	55	79	-	-	-	-	-	-	-
Stage 1	467	544	-	199	225	-	-	-	-	-	-	-
Stage 2	184	224	-	478	526	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	98.9		79.5		0.7		0.1	
HCM LOS	F		F					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1044	-	-	122	76	691	-	-
HCM Lane V/C Ratio	0.078	-	-	0.784	0.386	0.005	-	-
HCM Control Delay (s)	8.7	0	-	98.9	79.5	10.2	0	-
HCM Lane LOS	A	A	-	F	F	B	A	-
HCM 95th %tile Q(veh)	0.3	-	-	4.6	1.5	0	-	-

HCM 6th TWSC
4: Camden/Driveway 3 & Oak

12/27/2020

Intersection												
Int Delay, s/veh	1.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕				
Traffic Vol, veh/h	1	93	14	2	124	5	29	2	13	0	0	0
Future Vol, veh/h	1	93	14	2	124	5	29	2	13	0	0	0
Conflicting Peds, #/hr	11	0	28	28	0	11	28	0	28	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	16965	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	1	101	15	2	135	5	32	2	14	0	0	0

Major/Minor	Major1			Major2			Minor1		
Conflicting Flow All	151	0	0	144	0	0	309	294	165
Stage 1	-	-	-	-	-	-	139	139	-
Stage 2	-	-	-	-	-	-	170	155	-
Critical Hdwy	4.12	-	-	4.12	-	-	6.42	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	5.42	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	5.42	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318
Pot Cap-1 Maneuver	1430	-	-	1438	-	-	683	617	879
Stage 1	-	-	-	-	-	-	888	782	-
Stage 2	-	-	-	-	-	-	860	769	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1430	-	-	1400	-	-	645	0	833
Mov Cap-2 Maneuver	-	-	-	-	-	-	645	0	-
Stage 1	-	-	-	-	-	-	863	0	-
Stage 2	-	-	-	-	-	-	835	0	-

Approach	EB	WB	NB
HCM Control Delay, s	0.1	0.1	10.6
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR
Capacity (veh/h)	693	1430	-	-	1400	-	-
HCM Lane V/C Ratio	0.069	0.001	-	-	0.002	-	-
HCM Control Delay (s)	10.6	7.5	0	-	7.6	0	-
HCM Lane LOS	B	A	A	-	A	A	-
HCM 95th %tile Q(veh)	0.2	0	-	-	0	-	-

HCM 6th TWSC
1: Garfield & Driveway 1

12/27/2020

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	0	0	1	878	1161	4
Future Vol, veh/h	0	0	1	878	1161	4
Conflicting Peds, #/hr	11	11	11	0	0	11
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	0	1	954	1262	4

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	2242	1286	1277	0	-	0
Stage 1	1275	-	-	-	-	-
Stage 2	967	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	46	201	544	-	-	-
Stage 1	263	-	-	-	-	-
Stage 2	369	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	45	197	538	-	-	-
Mov Cap-2 Maneuver	45	-	-	-	-	-
Stage 1	259	-	-	-	-	-
Stage 2	365	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	0	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	538	-	-	-	-
HCM Lane V/C Ratio	0.002	-	-	-	-
HCM Control Delay (s)	11.7	0	0	-	-
HCM Lane LOS	B	A	A	-	-
HCM 95th %tile Q(veh)	0	-	-	-	-

HCM 6th TWSC
2: Garfield & Driveway 2

12/27/2020

Intersection						
Int Delay, s/veh	0.1					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↔			↑	↑	
Traffic Vol, veh/h	2	3	0	877	1161	0
Future Vol, veh/h	2	3	0	877	1161	0
Conflicting Peds, #/hr	11	11	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	3	0	953	1262	0

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	2226	1273	-	0	-	0
Stage 1	1262	-	-	-	-	-
Stage 2	964	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	-	-
Pot Cap-1 Maneuver	47	204	0	-	-	0
Stage 1	266	-	0	-	-	0
Stage 2	370	-	0	-	-	0
Platoon blocked, %				-	-	
Mov Cap-1 Maneuver	47	202	-	-	-	-
Mov Cap-2 Maneuver	47	-	-	-	-	-
Stage 1	266	-	-	-	-	-
Stage 2	370	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	49.1	0	0
HCM LOS	E		

Minor Lane/Major Mvmt	NBT EBLn1	SBT
Capacity (veh/h)	- 87	-
HCM Lane V/C Ratio	- 0.062	-
HCM Control Delay (s)	- 49.1	-
HCM Lane LOS	- E	-
HCM 95th %tile Q(veh)	- 0.2	-

HCM 6th TWSC
3: Garfield & Oak

12/27/2020

Intersection												
Int Delay, s/veh	41.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	26	15	78	1	19	13	75	887	6	12	733	36
Future Vol, veh/h	26	15	78	1	19	13	75	887	6	12	733	36
Conflicting Peds, #/hr	14	0	14	9	0	5	14	0	9	5	0	14
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	28	16	85	1	21	14	82	964	7	13	797	39

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2020	2001	845	2048	2017	991	850	0	0	980	0	0
Stage 1	857	857	-	1141	1141	-	-	-	-	-	-	-
Stage 2	1163	1144	-	907	876	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	43	60	363	41	58	299	788	-	-	704	-	-
Stage 1	352	374	-	244	275	-	-	-	-	-	-	-
Stage 2	237	275	-	330	367	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	~ 20	44	353	18	42	292	777	-	-	698	-	-
Mov Cap-2 Maneuver	~ 20	44	-	18	42	-	-	-	-	-	-	-
Stage 1	268	356	-	186	210	-	-	-	-	-	-	-
Stage 2	155	210	-	228	349	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s/\$	613.4		130.5		0.8		0.2	
HCM LOS	F		F					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	777	-	-	64	60	698	-	-
HCM Lane V/C Ratio	0.105	-	-	2.021	0.598	0.019	-	-
HCM Control Delay (s)	10.2	0	-	\$ 613.4	130.5	10.3	0	-
HCM Lane LOS	B	A	-	F	F	B	A	-
HCM 95th %tile Q(veh)	0.3	-	-	12.2	2.5	0.1	-	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

HCM 6th TWSC
4: Camden/Driveway 3 & Oak

12/27/2020

Intersection												
Int Delay, s/veh	1.6											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕				
Traffic Vol, veh/h	3	130	16	8	106	0	19	0	17	0	0	0
Future Vol, veh/h	3	130	16	8	106	0	19	0	17	0	0	0
Conflicting Peds, #/hr	14	0	25	25	0	14	25	0	25	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	16965	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	3	141	17	9	115	0	21	0	18	0	0	0

Major/Minor	Major1			Major2			Minor1		
Conflicting Flow All	129	0	0	183	0	0	339	328	200
Stage 1	-	-	-	-	-	-	181	181	-
Stage 2	-	-	-	-	-	-	158	147	-
Critical Hdwy	4.12	-	-	4.12	-	-	6.42	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	5.42	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	5.42	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318
Pot Cap-1 Maneuver	1457	-	-	1392	-	-	657	591	841
Stage 1	-	-	-	-	-	-	850	750	-
Stage 2	-	-	-	-	-	-	871	775	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1457	-	-	1359	-	-	620	0	801
Mov Cap-2 Maneuver	-	-	-	-	-	-	620	0	-
Stage 1	-	-	-	-	-	-	828	0	-
Stage 2	-	-	-	-	-	-	844	0	-

Approach	EB			WB			NB		
HCM Control Delay, s	0.2			0.5			10.5		
HCM LOS							B		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR
Capacity (veh/h)	694	1457	-	-	1359	-	-
HCM Lane V/C Ratio	0.056	0.002	-	-	0.006	-	-
HCM Control Delay (s)	10.5	7.5	0	-	7.7	0	-
HCM Lane LOS	B	A	A	-	A	A	-
HCM 95th %tile Q(veh)	0.2	0	-	-	0	-	-

HCM 6th TWSC
1: Garfield & Driveway 1

12/27/2020

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	0	0	0	1022	685	3
Future Vol, veh/h	0	0	0	1022	685	3
Conflicting Peds, #/hr	12	12	12	0	0	12
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	0	0	1111	745	3

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1882	771	760	0	-	0
Stage 1	759	-	-	-	-	-
Stage 2	1123	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	78	400	852	-	-	-
Stage 1	462	-	-	-	-	-
Stage 2	311	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	76	391	842	-	-	-
Mov Cap-2 Maneuver	76	-	-	-	-	-
Stage 1	457	-	-	-	-	-
Stage 2	308	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	0	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	842	-	-	-	-
HCM Lane V/C Ratio	-	-	-	-	-
HCM Control Delay (s)	0	-	0	-	-
HCM Lane LOS	A	-	A	-	-
HCM 95th %tile Q(veh)	0	-	-	-	-

HCM 6th TWSC
2: Garfield & Driveway 2

12/27/2020

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	Y			↑	↑	
Traffic Vol, veh/h	0	1	0	1022	685	0
Future Vol, veh/h	0	1	0	1022	685	0
Conflicting Peds, #/hr	12	12	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	1	0	1111	745	0

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1868	757	-	0	-	0
Stage 1	745	-	-	-	-	-
Stage 2	1123	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	-	-
Pot Cap-1 Maneuver	80	408	0	-	-	0
Stage 1	469	-	0	-	-	0
Stage 2	311	-	0	-	-	0
Platoon blocked, %				-	-	
Mov Cap-1 Maneuver	80	403	-	-	-	-
Mov Cap-2 Maneuver	80	-	-	-	-	-
Stage 1	469	-	-	-	-	-
Stage 2	311	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	14	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT EBLn1	SBT
Capacity (veh/h)	- 403	-
HCM Lane V/C Ratio	- 0.003	-
HCM Control Delay (s)	- 14	-
HCM Lane LOS	- B	-
HCM 95th %tile Q(veh)	- 0	-

HCM 6th TWSC
3: Garfield & Oak

12/27/2020

Intersection												
Int Delay, s/veh	7.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	31	10	47	11	11	5	75	877	6	3	380	61
Future Vol, veh/h	31	10	47	11	11	5	75	877	6	3	380	61
Conflicting Peds, #/hr	19	0	21	21	0	7	21	0	21	7	0	19
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	34	11	51	12	12	5	82	953	7	3	413	66

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1621	1618	488	1646	1648	997	500	0	0	981	0	0
Stage 1	473	473	-	1142	1142	-	-	-	-	-	-	-
Stage 2	1148	1145	-	504	506	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	83	103	580	79	99	296	1064	-	-	704	-	-
Stage 1	572	558	-	244	275	-	-	-	-	-	-	-
Stage 2	242	274	-	550	540	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	60	82	557	54	79	285	1043	-	-	690	-	-
Mov Cap-2 Maneuver	60	82	-	54	79	-	-	-	-	-	-	-
Stage 1	466	543	-	199	224	-	-	-	-	-	-	-
Stage 2	183	223	-	477	526	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	98.9	81	0.7	0.1
HCM LOS	F	F		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1043	-	-	122	75	690	-
HCM Lane V/C Ratio	0.078	-	-	0.784	0.391	0.005	-
HCM Control Delay (s)	8.7	0	-	98.9	81	10.2	0
HCM Lane LOS	A	A	-	F	F	B	A
HCM 95th %tile Q(veh)	0.3	-	-	4.6	1.5	0	-

HCM 6th TWSC
4: Camden/Driveway 3 & Oak

12/27/2020

Intersection												
Int Delay, s/veh	1.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕				
Traffic Vol, veh/h	1	93	14	2	124	5	29	2	13	0	0	0
Future Vol, veh/h	1	93	14	2	124	5	29	2	13	0	0	0
Conflicting Peds, #/hr	11	0	28	28	0	11	28	0	28	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	16965	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	1	101	15	2	135	5	32	2	14	0	0	0

Major/Minor	Major1			Major2			Minor1		
Conflicting Flow All	151	0	0	144	0	0	309	294	165
Stage 1	-	-	-	-	-	-	139	139	-
Stage 2	-	-	-	-	-	-	170	155	-
Critical Hdwy	4.12	-	-	4.12	-	-	6.42	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	5.42	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	5.42	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318
Pot Cap-1 Maneuver	1430	-	-	1438	-	-	683	617	879
Stage 1	-	-	-	-	-	-	888	782	-
Stage 2	-	-	-	-	-	-	860	769	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1430	-	-	1400	-	-	645	0	833
Mov Cap-2 Maneuver	-	-	-	-	-	-	645	0	-
Stage 1	-	-	-	-	-	-	863	0	-
Stage 2	-	-	-	-	-	-	835	0	-

Approach	EB			WB			NB		
HCM Control Delay, s	0.1			0.1			10.6		
HCM LOS							B		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR
Capacity (veh/h)	693	1430	-	-	1400	-	-
HCM Lane V/C Ratio	0.069	0.001	-	-	0.002	-	-
HCM Control Delay (s)	10.6	7.5	0	-	7.6	0	-
HCM Lane LOS	B	A	A	-	A	A	-
HCM 95th %tile Q(veh)	0.2	0	-	-	0	-	-

HCM 6th TWSC
1: Garfield & Driveway 1

12/27/2020

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	0	0	1	880	1163	4
Future Vol, veh/h	0	0	1	880	1163	4
Conflicting Peds, #/hr	11	11	11	0	0	11
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	0	1	957	1264	4

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	2247	1288	1279	0	-	0
Stage 1	1277	-	-	-	-	-
Stage 2	970	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	46	200	543	-	-	-
Stage 1	262	-	-	-	-	-
Stage 2	368	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	45	196	537	-	-	-
Mov Cap-2 Maneuver	45	-	-	-	-	-
Stage 1	258	-	-	-	-	-
Stage 2	364	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	0	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	537	-	-	-	-
HCM Lane V/C Ratio	0.002	-	-	-	-
HCM Control Delay (s)	11.7	0	0	-	-
HCM Lane LOS	B	A	A	-	-
HCM 95th %tile Q(veh)	0	-	-	-	-

HCM 6th TWSC
2: Garfield & Driveway 2

12/27/2020

Intersection						
Int Delay, s/veh	0.1					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			↑		
Traffic Vol, veh/h	2	3	0	879	1163	0
Future Vol, veh/h	2	3	0	879	1163	0
Conflicting Peds, #/hr	11	11	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	3	0	955	1264	0

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	2230	1275	-	0	-	0
Stage 1	1264	-	-	-	-	-
Stage 2	966	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	-	-
Pot Cap-1 Maneuver	47	204	0	-	-	0
Stage 1	266	-	0	-	-	0
Stage 2	369	-	0	-	-	0
Platoon blocked, %				-	-	
Mov Cap-1 Maneuver	47	202	-	-	-	-
Mov Cap-2 Maneuver	47	-	-	-	-	-
Stage 1	266	-	-	-	-	-
Stage 2	369	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	49.1	0	0
HCM LOS	E		

Minor Lane/Major Mvmt	NBT EBLn1	SBT
Capacity (veh/h)	- 87	-
HCM Lane V/C Ratio	- 0.062	-
HCM Control Delay (s)	- 49.1	-
HCM Lane LOS	- E	-
HCM 95th %tile Q(veh)	- 0.2	-

HCM 6th TWSC
3: Garfield & Oak

12/27/2020

Intersection												
Int Delay, s/veh	41.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	26	15	78	1	19	13	75	889	6	12	735	36
Future Vol, veh/h	26	15	78	1	19	13	75	889	6	12	735	36
Conflicting Peds, #/hr	14	0	14	9	0	5	14	0	9	5	0	14
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	28	16	85	1	21	14	82	966	7	13	799	39

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2024	2005	847	2052	2021	993	852	0	0	982	0	0
Stage 1	859	859	-	1143	1143	-	-	-	-	-	-	-
Stage 2	1165	1146	-	909	878	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	43	59	362	41	58	298	787	-	-	703	-	-
Stage 1	351	373	-	243	275	-	-	-	-	-	-	-
Stage 2	237	274	-	329	366	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	~ 20	43	352	17	42	292	777	-	-	697	-	-
Mov Cap-2 Maneuver	~ 20	43	-	17	42	-	-	-	-	-	-	-
Stage 1	267	355	-	185	210	-	-	-	-	-	-	-
Stage 2	154	209	-	227	348	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s\$	613.4		134.2		0.8		0.2	
HCM LOS	F		F					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	777	-	-	64	59	697	-	-
HCM Lane V/C Ratio	0.105	-	-	2.021	0.608	0.019	-	-
HCM Control Delay (s)	10.2	0	-	\$ 613.4	134.2	10.3	0	-
HCM Lane LOS	B	A	-	F	F	B	A	-
HCM 95th %tile Q(veh)	0.3	-	-	12.2	2.5	0.1	-	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

HCM 6th TWSC
4: Camden/Driveway 3 & Oak

12/27/2020

Intersection												
Int Delay, s/veh	1.6											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕				
Traffic Vol, veh/h	3	130	16	8	106	0	19	0	17	0	0	0
Future Vol, veh/h	3	130	16	8	106	0	19	0	17	0	0	0
Conflicting Peds, #/hr	14	0	25	25	0	14	25	0	25	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	16965	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	3	141	17	9	115	0	21	0	18	0	0	0

Major/Minor	Major1			Major2			Minor1		
Conflicting Flow All	129	0	0	183	0	0	339	328	200
Stage 1	-	-	-	-	-	-	181	181	-
Stage 2	-	-	-	-	-	-	158	147	-
Critical Hdwy	4.12	-	-	4.12	-	-	6.42	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	5.42	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	5.42	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318
Pot Cap-1 Maneuver	1457	-	-	1392	-	-	657	591	841
Stage 1	-	-	-	-	-	-	850	750	-
Stage 2	-	-	-	-	-	-	871	775	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1457	-	-	1359	-	-	620	0	801
Mov Cap-2 Maneuver	-	-	-	-	-	-	620	0	-
Stage 1	-	-	-	-	-	-	828	0	-
Stage 2	-	-	-	-	-	-	844	0	-

Approach	EB			WB			NB		
HCM Control Delay, s	0.2			0.5			10.5		
HCM LOS							B		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR
Capacity (veh/h)	694	1457	-	-	1359	-	-
HCM Lane V/C Ratio	0.056	0.002	-	-	0.006	-	-
HCM Control Delay (s)	10.5	7.5	0	-	7.7	0	-
HCM Lane LOS	B	A	A	-	A	A	-
HCM 95th %tile Q(veh)	0.2	0	-	-	0	-	-

HCM 6th TWSC
1: Garfield & Driveway 1

12/27/2020

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	0	0	0	1022	685	3
Future Vol, veh/h	0	0	0	1029	685	11
Conflicting Peds, #/hr	12	12	12	0	0	12
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	0	0	1118	745	12

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1893	775	769	0	-	0
Stage 1	763	-	-	-	-	-
Stage 2	1130	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	77	398	845	-	-	-
Stage 1	460	-	-	-	-	-
Stage 2	308	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	75	389	835	-	-	-
Mov Cap-2 Maneuver	75	-	-	-	-	-
Stage 1	455	-	-	-	-	-
Stage 2	305	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	0	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	835	-	-	-	-
HCM Lane V/C Ratio	-	-	-	-	-
HCM Control Delay (s)	0	-	0	-	-
HCM Lane LOS	A	-	A	-	-
HCM 95th %tile Q(veh)	0	-	-	-	-

HCM 6th TWSC
2: Garfield & Driveway 2

12/27/2020

Intersection						
Int Delay, s/veh	0.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	↔			↑	↑	
Traffic Vol, veh/h	0	1	0	1022	685	0
Future Vol, veh/h	7	21	0	1022	685	0
Conflicting Peds, #/hr	12	12	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	8	23	0	1111	745	0

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1868	757	-	0	-	0
Stage 1	745	-	-	-	-	-
Stage 2	1123	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	-	-
Pot Cap-1 Maneuver	80	408	0	-	-	0
Stage 1	469	-	0	-	-	0
Stage 2	311	-	0	-	-	0
Platoon blocked, %				-	-	
Mov Cap-1 Maneuver	80	403	-	-	-	-
Mov Cap-2 Maneuver	80	-	-	-	-	-
Stage 1	469	-	-	-	-	-
Stage 2	311	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	26.1	0	0
HCM LOS	D		

Minor Lane/Major Mvmt	NBT EBLn1	SBT
Capacity (veh/h)	- 201	-
HCM Lane V/C Ratio	- 0.151	-
HCM Control Delay (s)	- 26.1	-
HCM Lane LOS	- D	-
HCM 95th %tile Q(veh)	- 0.5	-

HCM 6th TWSC
3: Garfield & Oak

12/27/2020

Intersection												
Int Delay, s/veh	8.6											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	31	10	47	11	11	5	75	877	6	3	380	61
Future Vol, veh/h	31	10	47	11	13	5	77	877	6	4	381	79
Conflicting Peds, #/hr	19	0	21	21	0	7	21	0	21	7	0	19
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	34	11	51	12	14	5	84	953	7	4	414	86

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1639	1635	499	1663	1675	997	521	0	0	981	0	0
Stage 1	486	486	-	1146	1146	-	-	-	-	-	-	-
Stage 2	1153	1149	-	517	529	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	80	101	572	77	95	296	1045	-	-	704	-	-
Stage 1	563	551	-	242	274	-	-	-	-	-	-	-
Stage 2	240	273	-	541	527	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	56	79	549	52	75	285	1024	-	-	690	-	-
Mov Cap-2 Maneuver	56	79	-	52	75	-	-	-	-	-	-	-
Stage 1	455	536	-	196	221	-	-	-	-	-	-	-
Stage 2	178	221	-	467	512	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB			
HCM Control Delay, s	113.1		89.3		0.7		0.1			
HCM LOS	F		F							

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1024	-	-	115	72	690	-
HCM Lane V/C Ratio	0.082	-	-	0.832	0.438	0.006	-
HCM Control Delay (s)	8.8	0	-	113.1	89.3	10.2	0
HCM Lane LOS	A	A	-	F	F	B	A
HCM 95th %tile Q(veh)	0.3	-	-	4.9	1.7	0	-

HCM 6th TWSC
4: Camden/Driveway 3 & Oak

12/27/2020

Intersection												
Int Delay, s/veh	2.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕				
Traffic Vol, veh/h	1	93	14	2	124	5	29	2	13	0	0	0
Future Vol, veh/h	19	93	14	5	139	9	29	5	13	0	0	0
Conflicting Peds, #/hr	11	0	28	28	0	11	28	0	28	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	16965	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	21	101	15	5	151	10	32	5	14	0	0	0

Major/Minor	Major1			Major2			Minor1		
Conflicting Flow All	172	0	0	144	0	0	373	361	165
Stage 1	-	-	-	-	-	-	179	179	-
Stage 2	-	-	-	-	-	-	194	182	-
Critical Hdwy	4.12	-	-	4.12	-	-	6.42	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	5.42	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	5.42	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318
Pot Cap-1 Maneuver	1405	-	-	1438	-	-	628	566	879
Stage 1	-	-	-	-	-	-	852	751	-
Stage 2	-	-	-	-	-	-	839	749	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1405	-	-	1400	-	-	583	0	833
Mov Cap-2 Maneuver	-	-	-	-	-	-	583	0	-
Stage 1	-	-	-	-	-	-	815	0	-
Stage 2	-	-	-	-	-	-	813	0	-

Approach	EB			WB			NB		
HCM Control Delay, s	1.1			0.2			11.1		
HCM LOS							B		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR
Capacity (veh/h)	643	1405	-	-	1400	-	-
HCM Lane V/C Ratio	0.079	0.015	-	-	0.004	-	-
HCM Control Delay (s)	11.1	7.6	0	-	7.6	0	-
HCM Lane LOS	B	A	A	-	A	A	-
HCM 95th %tile Q(veh)	0.3	0	-	-	0	-	-

HCM 6th TWSC
1: Garfield & Driveway 1

12/27/2020

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	0	0	1	880	1163	4
Future Vol, veh/h	0	0	1	888	1163	11
Conflicting Peds, #/hr	11	11	11	0	0	11
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	0	1	965	1264	12

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	2259	1292	1287	0	-	0
Stage 1	1281	-	-	-	-	-
Stage 2	978	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	45	199	539	-	-	-
Stage 1	261	-	-	-	-	-
Stage 2	364	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	44	195	533	-	-	-
Mov Cap-2 Maneuver	44	-	-	-	-	-
Stage 1	257	-	-	-	-	-
Stage 2	360	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	0	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	533	-	-	-	-
HCM Lane V/C Ratio	0.002	-	-	-	-
HCM Control Delay (s)	11.8	0	0	-	-
HCM Lane LOS	B	A	A	-	-
HCM 95th %tile Q(veh)	0	-	-	-	-

HCM 6th TWSC
2: Garfield & Driveway 2

12/27/2020

Intersection						
Int Delay, s/veh	1.1					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			↑	↑	
Traffic Vol, veh/h	2	3	0	879	1163	0
Future Vol, veh/h	10	28	0	879	1163	0
Conflicting Peds, #/hr	11	11	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	11	30	0	955	1264	0

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	2230	1275	-	0	-	0
Stage 1	1264	-	-	-	-	-
Stage 2	966	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	-	-
Pot Cap-1 Maneuver	47	204	0	-	-	0
Stage 1	266	-	0	-	-	0
Stage 2	369	-	0	-	-	0
Platoon blocked, %				-	-	
Mov Cap-1 Maneuver	47	202	-	-	-	-
Mov Cap-2 Maneuver	47	-	-	-	-	-
Stage 1	266	-	-	-	-	-
Stage 2	369	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	57.6	0	0
HCM LOS	F		

Minor Lane/Major Mvmt	NBT EBLn1	SBT
Capacity (veh/h)	- 108	-
HCM Lane V/C Ratio	- 0.382	-
HCM Control Delay (s)	- 57.6	-
HCM Lane LOS	- F	-
HCM 95th %tile Q(veh)	- 1.6	-

HCM 6th TWSC
3: Garfield & Oak

12/27/2020

Intersection												
Int Delay, s/veh	47.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	26	15	78	1	19	13	75	889	6	12	735	36
Future Vol, veh/h	26	15	78	1	20	13	76	889	6	14	737	57
Conflicting Peds, #/hr	14	0	14	9	0	5	14	0	9	5	0	14
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	28	16	85	1	22	14	83	966	7	15	801	62

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	2044	2024	860	2072	2052	993	877	0	0	982	0	0
Stage 1	876	876	-	1145	1145	-	-	-	-	-	-	-
Stage 2	1168	1148	-	927	907	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	42	58	356	40	56	298	770	-	-	703	-	-
Stage 1	344	367	-	243	274	-	-	-	-	-	-	-
Stage 2	236	273	-	322	355	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	~ 18	41	347	16	40	292	760	-	-	697	-	-
Mov Cap-2 Maneuver	~ 18	41	-	16	40	-	-	-	-	-	-	-
Stage 1	259	347	-	183	207	-	-	-	-	-	-	-
Stage 2	151	206	-	219	336	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/\$	716.5	150.7	0.8	0.2
HCM LOS	F	F		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	760	-	-	58	56	697	-
HCM Lane V/C Ratio	0.109	-	-	2.23	0.66	0.022	-
HCM Control Delay (s)	10.3	0	-	\$ 716.5	150.7	10.3	0
HCM Lane LOS	B	A	-	F	F	B	A
HCM 95th %tile Q(veh)	0.4	-	-	12.7	2.7	0.1	-

Notes
 -: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

HCM 6th TWSC
4: Camden/Driveway 3 & Oak

12/27/2020

Intersection												
Int Delay, s/veh	1.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕				
Traffic Vol, veh/h	3	130	16	8	106	0	19	0	17	0	0	0
Future Vol, veh/h	19	130	16	11	124	2	19	3	17	0	0	0
Conflicting Peds, #/hr	14	0	25	25	0	14	25	0	25	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	16965	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	21	141	17	12	135	2	21	3	18	0	0	0

Major/Minor	Major1			Major2			Minor1		
Conflicting Flow All	151	0	0	183	0	0	402	392	200
Stage 1	-	-	-	-	-	-	217	217	-
Stage 2	-	-	-	-	-	-	185	175	-
Critical Hdwy	4.12	-	-	4.12	-	-	6.42	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	5.42	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	5.42	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318
Pot Cap-1 Maneuver	1430	-	-	1392	-	-	604	544	841
Stage 1	-	-	-	-	-	-	819	723	-
Stage 2	-	-	-	-	-	-	847	754	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1430	-	-	1359	-	-	561	0	801
Mov Cap-2 Maneuver	-	-	-	-	-	-	561	0	-
Stage 1	-	-	-	-	-	-	786	0	-
Stage 2	-	-	-	-	-	-	818	0	-

Approach	EB			WB			NB		
HCM Control Delay, s	0.9			0.6			10.9		
HCM LOS							B		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR
Capacity (veh/h)	653	1430	-	-	1359	-	-
HCM Lane V/C Ratio	0.065	0.014	-	-	0.009	-	-
HCM Control Delay (s)	10.9	7.6	0	-	7.7	0	-
HCM Lane LOS	B	A	A	-	A	A	-
HCM 95th %tile Q(veh)	0.2	0	-	-	0	-	-

ATTACHMENT 6

Public Comments

**Gary D. Penn
2065 Oak Street
South Pasadena, CA 91030**

July 28, 2021

TO WHOM IT MAY CONCERN:

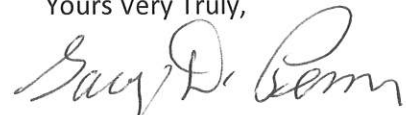
My wife and I have lived at 2065 Oak Street in South Pasadena for more than sixty years. We live across the street from the Oneonta Congregational Church which hosts the local My Kkids Christian School. As a result of the recent pandemic some of the activities have moved from an inside facility within the church property, where they had traditionally been carried out, to an outside area which is also part of the church property. Obviously, carrying out youth activities in an outdoor setting like this is substantially more healthful during the current pandemic than confining these children indoors.

I want to take this opportunity to state that I strongly support these outdoor youth activities at the Oneonta Congregational Church and I urge others to support them also. These children in many ways represent the future of this country and we should help them to carry forth their youth programs in a wholesome and healthy environment.

I believe this type of youth program should be encouraged and supported and I am hopeful that other church neighbors will act responsibly and support them as well.

Thank you for your consideration.

Yours Very Truly,



Gary D. Penn

To whomever it may concern:

My name is Tomas Quiroz and I am a proud alumni of the Kinesthetic Kids now known as MyKkids daycare program. From kindergarten to 8th grade, KKids has been my second home where I spent all my after-school hours as well as my summers. Looking back, KKids played such a key role in my formative years because they put an emphasis on shaping young boys and girls into mature, well mannered young adults. All the staff instilled core values of respect, honesty and living the "Golden Rule", which created an educational environment in addition to the fun of daily activities, games and field trips.

I always looked forward to KKids every day because it was such a great place for me to express my creative side and burn off some energy as well. KKids always went above and beyond which manifested itself in the activities, where we would build forts, have time for Free Play, play games in groups, do arts and crafts, have story time, and much more. There was never a time where I was bored and I never wanted to be anywhere else. As I got older, I would always laugh about the new kids who would beg their parents to stay at the end of the day, after crying about leaving their parents that morning. Kkids has this effect on the children because they treat every kid like family.

I am currently a rising junior at Washington University in St. Louis where I am double majoring in Dance and Economics with a minor in Math, while also being a punter for the university's football team. As a kid I was constantly running around to my different activities and Kinesthetics provided me with rides so that I could pursue all of my passions. In addition to helping with logistics, they encouraged me to follow my heart. This support has helped me live out my dream to dance and play football at a prestigious university. Even being a boy in ballet, I never had to worry about bullying because Kkids created a safe and accepting environment where I could be myself.

Kkids is part of who I am and I hope that they can continue to shape and develop the youth of their community for generations to come.

Sincerely,

Tomas Quiroz

July 20, 2021

Dear Planning Commission,

Hello! I'm Susan Rozler and I live across the street from Oneonta Church at the corner of Oak near Garfield. I am very optimistic about My K Kids Christian Daycare across the street. The teachers and children are very happy, polite and a positive addition to our neighborhood.

I'm hoping you approve their application submitted for you to review. My phone is 716 912 5552 and email susanrozler@gmail.com if you'd like to ask me questions.

Thank-you very much

Susan Rozler

[EXT] Support Letter for MyKKids

Feridey Carr <feridey.carr@gmail.com>

Tue 6/22/2021 8:58 AM

To: Office <office@mykkids.org>

Cc: Karolina Gorska <kgorska@rinconconsultants.com>

CAUTION: This email originated from outside of Rincon Consultants. Be cautious before clicking on any links, or opening any attachments, until you are confident that the content is safe .

Letter of Support for MyKKids

6/22/2021

To Whom It May Concern:

Our family would like to add our voices of support and commendation for MyKKids. Our son (a rising third grader at Monterey Hills Elementary School and participant in the Dual Immersion Spanish Program) has attended MyKKids (then called SP Kinesthetic Kids) since he completed preschool. He has attended the MyKKids Summer Camp for the past four years as well as their AM Kinder program and After School Program. He loves the outdoor games that the instructors play with the kids and values both the relationships he's developed with the MyKKids instructors as well as the close friendships he's developed with his peers. He particularly enjoys practicing the Spanish he's been learning in the Immersion Program with some of his MyKKids instructors. As parents, we appreciate the relationships we've also developed with the teachers at MyKKids. At a previous childcare facility, we experienced high instructor turnover and it was difficult to develop trust. At MyKKids, the instructors there currently are the same as when our son first started four years ago and that is perhaps our favorite part of their organization. For us, MyKKids is not just a childcare program but it has become our extended family.

During the last 18 months of the pandemic, our son lost predictability, time with his friends, playtime and experienced a disruption of normalcy. Though we pulled our son out of MyKKids twice during both COVID-19 surges, we continued to pay partial tuition to keep our son's spot there and were happy to do it. We wanted to ensure not only that he could return when it was safe to do so but to ensure that MyKKids would not be forced to let go of any of their instructors. Like many families, we had some tough days working and managing distance learning from home. Our son began exhibiting clear signs of depression. At various times, MyKKids would drive their school van by so that our son could wave from a distance and see some of his instructors' faces. Seeing his face light up was everything. Though we weren't comfortable sending him back to in-person school this past Spring, we did trust MyKKids with his care and to facilitate his distance learning through a daytime support program. When our son finally returned to MyKKids this past April, his depression symptoms quickly resolved. He is back to his happy, tired self when we pick him up at the end of the day and for our family, that is priceless.

We support and need MyKKids to continue in South Pasadena, now more than ever given the losses and changes of this past 18 months. Their organization has been a core part of how our family manages work, schooling and childcare. We urge the city of South Pasadena to consider how MyKKids has functioned in the community and provided many families with crucial support this past year.

Sincerely,

Feridey Carr, PhD
Kevin Cebula, DDS

Michael Reynolds, JD
Aly Parker, JD
(Parents and Step-Parents of Oren Reynolds, Monterey Hills Elementary)

--

Feridey N. Carr, PhD
Clinical Health Psychologist PSY 26895
Behavioral Sleep Medicine

301 S. Fair Oaks Ave, Suite 402
Pasadena, CA 91105
(424) 259-3096
www.DrCarrCBT.com

June 19, 2021

To: office@mykkids.org
cc: kgorska@rinconconsultants.com

Re: Letter of Support

To whom it may concern,

Our family has relied on MyKkids / SP Kinesthetic Kids for many years now. As a family with two working parents, their services have been invaluable over the years and remain incredibly vital to us. As an affordable, local option with extended hours and the ability to drop off and pick up our children to/from school is really unmatched here in South Pasadena. All of the teachers are amazing and have gone above and beyond to connect with our family. We simply cannot say enough good things about this organization. Our experience has been very positive and we believe this resource is essential to the South Pasadena community, to help support and augment the unparalleled public education we all benefit from. In fact, when our son and daughter were in South Pas' half-day Kindergarten (3 hours total per day), this simply would have been an option without MyKkids / SP Kinesthetic Kids.

We write this letter of support in hopes that the Planning Commission will approve their request and allow them to continue operating without disruption.

Please don't hesitate to contact me for any further clarification.

Thank you,



Stephanie Rigolot
310-903-8054



Scott Severson
383-6198

Daughter: Sadie Severson, age 9
Son: Sawyer Severson, age 7

Rigolot Seversons

Stephanie Rigolot
stephrigolot@gmail.com
310-903-8054

Scott Severson
scottaseverson@gmail.com
310-383-6198

1616 Bushnell Avenue
South Pasadena, CA 91030

[EXT] MyKKids

Luani Lee <luanidesai@gmail.com>

Sat 6/19/2021 8:10 AM

To: Karolina Gorska <kgorska@rinconconsultants.com>

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To whom it may concern,

Kinesthetic Kids saved our family. Our family was fortunate enough to have two parents both retain our jobs during the pandemic: one work from home and one essential medical doctor working out of the home.

Thank God for the staff at Kinesthetic Kids who were able to have our children start daycare during the pandemic so that they could flourish mentally and physically. Without Kkids, we would not have been able to maintain some normalcy during these trying times. Our children have remained happy and healthy and excited to go to school each day.

It has been a comfort this last year to drop off our precious children at Kkids with staff so warm and loving. Our youngest, who is 4, even often refers to his preschool teacher as his "school mommy".

We have been blessed to have all of the staff in our lives this year and would be crushed if the MyKkids program cannot continue. The preschool and afterschool programs are essential to our family, and for many others in the community as well.

Sincerely,

Luani Lee

[EXT] KKids

Nirav Desai <nirav@quberawealth.com>

Sat 6/19/2021 12:30 PM

To: office@mykkids.org <office@mykkids.org>; Karolina Gorska <kgorska@rinconconsultants.com>

CAUTION: This email originated from outside of Rincon Consultants. Be cautious before clicking on any links, or opening any attachments, until you are confident that the content is safe .

To whom it may concern,

I just wanted to say that the support of KKids during this past year had been invaluable to me and my family, not to mention my business.

As a small business owner, I cannot earn my livelihood if I have to take care to two young children during business hours.

The staff at KKids have really rallied during this past year, which had been challenging for everyone. I know my children are in good hands with loving teachers who care about their well-being.

KKids is an essential service and deserves to stay open. All preschool and after-school programs have wait-lists, so it's not an option to just place my kids somewhere else. If they shut down, even temporarily, not only will my income suffer but so will the kid's education, as I can't possibly match their level of expertise.

Regards,

Nirav Desai
Managing Principal
Qubera Wealth Management Inc.
225 S. Lake Ave., Suite 300,
Pasadena, CA 91101

O: (323) 999 1095

M: (858) 243 4258

nirav@quberawealth.com

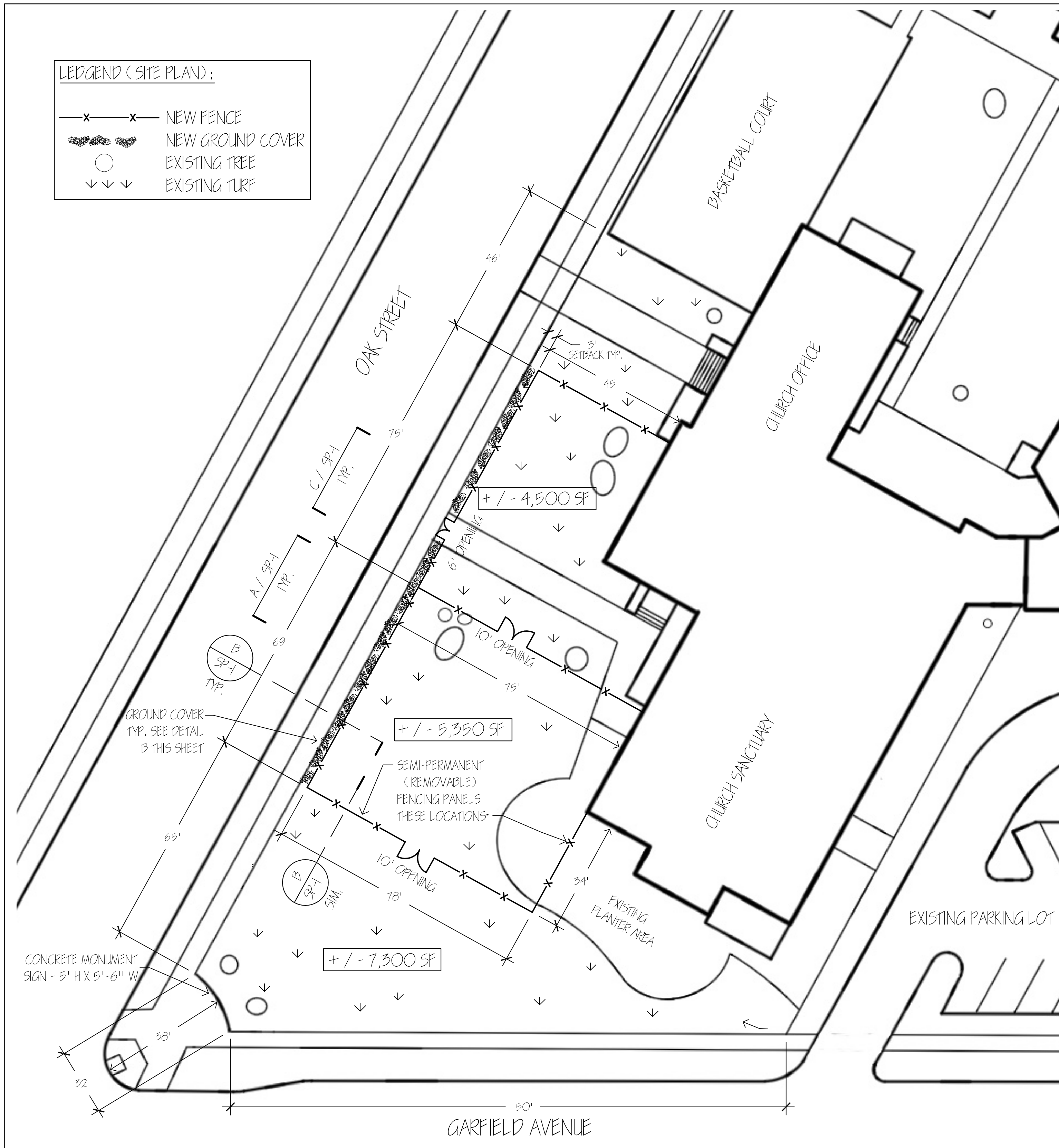
<http://quberawealth.com>

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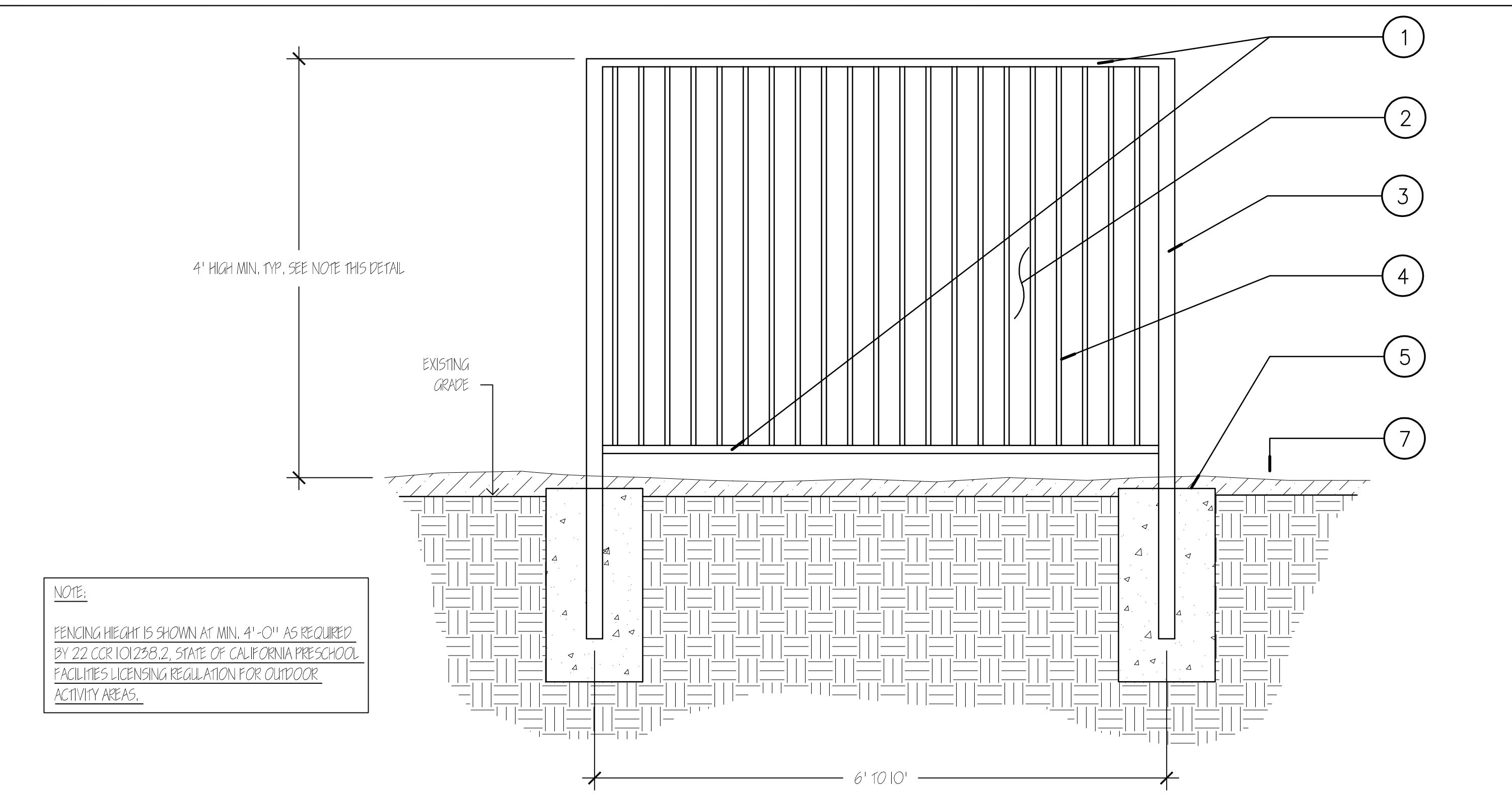
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ATTACHMENT 7

Fence Plan

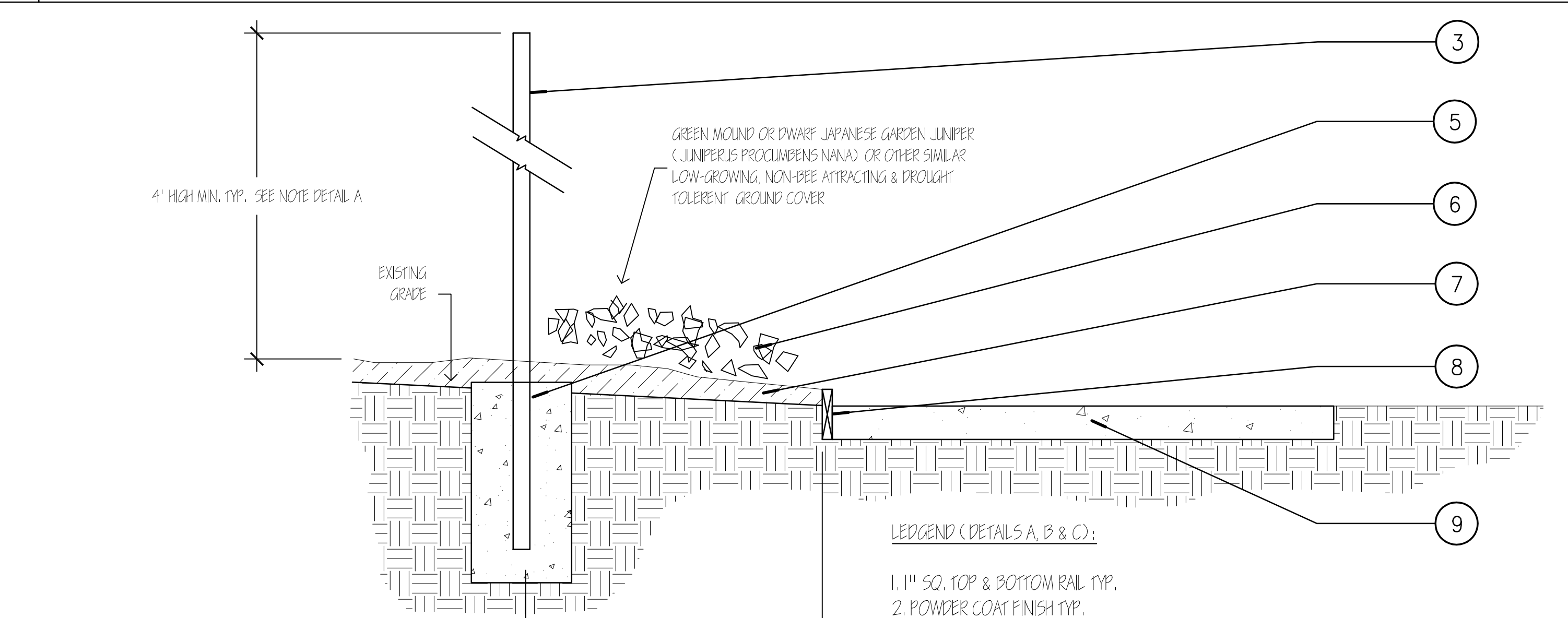


FENCING PLAN - SCALE: 1:20



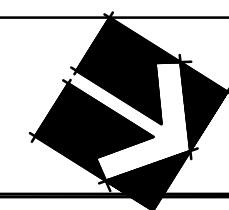
NOTE:
 FENCING HEIGHT IS SHOWN AT MIN. 4'-0" AS REQUIRED BY 22 CCR 101238.2, STATE OF CALIFORNIA PRESCHOOL FACILITIES LICENSING REGULATION FOR OUTDOOR ACTIVITY AREAS.

A TYP. FENCE ELEVATION - NO SCALE



LEGEND (DETAILS A, B & C):
 1. 1" SQ. TOP & BOTTOM RAIL TYP.
 2. POWDER COAT FINISH TYP.
 3. 2" SQ. POST TYP.
 4. 3/8" SQ. PICKET @ 4" O.C. (3 3/4" CLEAR) TYP.
 5. CONCRETE FOOTING PER MANUFACTURER'S RECOMMENDATION TYP. PROVIDE SLEEVE @ SEMI-PERMANENT (REMOVABLE) POST FOOTINGS
 6. GROUND COVER TYP. SEE NOTE THIS DETAIL.
 7. MULCH TYP.
 8. HEADER TYP.
 9. EX. 5' W CONCRETE SIDEWALK TYP.

B TYP. FENCE SECTION - NO SCALE



PARTIAL SITE AND FENCING PLAN SP-1

DESIGNED BY: DGT	REVISIONS				ONEONTA CONGREGATIONAL CHURCH	
DRAWN BY: DGT	MARK	DATE	DESCRIPTION	BY	CHKD.	APPR'D
CHECKED BY: XXX	xxx		SUBMITTED FOR CUP			
			AS BUILT			
	PROJECT: PRESCHOOL FENCING				SHEET 1 OF 1	
	LIMITS: 1515 GARFIELD AVENUE, SO. PASADENA, CA 91030				DWG. NO.	
	SCALE: AS SHOWN					

ATTACHMENT 8

Parking and Access

Parking Weekday Needs at Oneonta 7:00am - 7:00pm

Service - Activity	Operation Days	Operation Hours	Staff Parking Required	Staff Parking Proposed	Parent Parking Required	Parent Parking Proposed	Total Parking 104 spaces 3 Handicap	Kids Klub School Year	Kids Klub Summer Camp Non School 8 weeks
My Kkids Preschool (Button Hall Bldg.) Max. Children: 25* Max. Staff: 3*	M-F	7am-7pm	1 stall for each person	3 Stalls Provided	1 stall for every 5 children	5 Stalls provided	8 stalls	8 stalls	8 stalls
My Kkids Afterschool (Evelyn H.C. Memorial Bldg.) Max. Children: 52* Max Staff: 4*	M-F	7am-7pm	1 stall for each person	4 Stalls Provided	1 stall for every 5 children	11 Stalls provided	14 stalls	14 stalls	14 stalls
Oneonta Co-op (Memorial Bldg. 1 st Flr) Max. Children: 60 Max. Staff: 7	M-F	8:30am-12:30pm	1 stall for each person; 7 stalls	7 stalls provided	1 stall for every 5 children; 12 Stalls	12 stalls provided	19 stalls	19 stalls	19 stalls
Kids Klub Afterschool (Whitmore Rm. & Oak View Room) Max.Children:70 school year, Max. Staff: 15 school yr,	M-F	7am-6pm	1 stall for each person; 15 stalls required	15 stalls provided	1 stall for every 5 children; 14 stalls required	14 stalls provided	29 stalls required	34 stalls provided	
Kids Klub 8 week Summer Camp (Whitmore Rm. &OakView Room) Max Children: 120 Non school. Max Staff 30	M-F	7am-6pm	1 stall for each person; 30 stalls required	30 stalls provided	1 stall for every 5 children; 24 stalls required	24 stalls provided	54 stalls required		54 stalls provided
Church Services/Events During the Week	Currently none	7:00am -7:00pm							
Church Staff & Volunteer Max 4 per day	T - F	9:00am 4:00pm	1 per person	4 stalls			4 stalls	4 stalls	4 stalls
							Total Stalls 104 spaces 3 Handicap	79 stalls	99 stalls

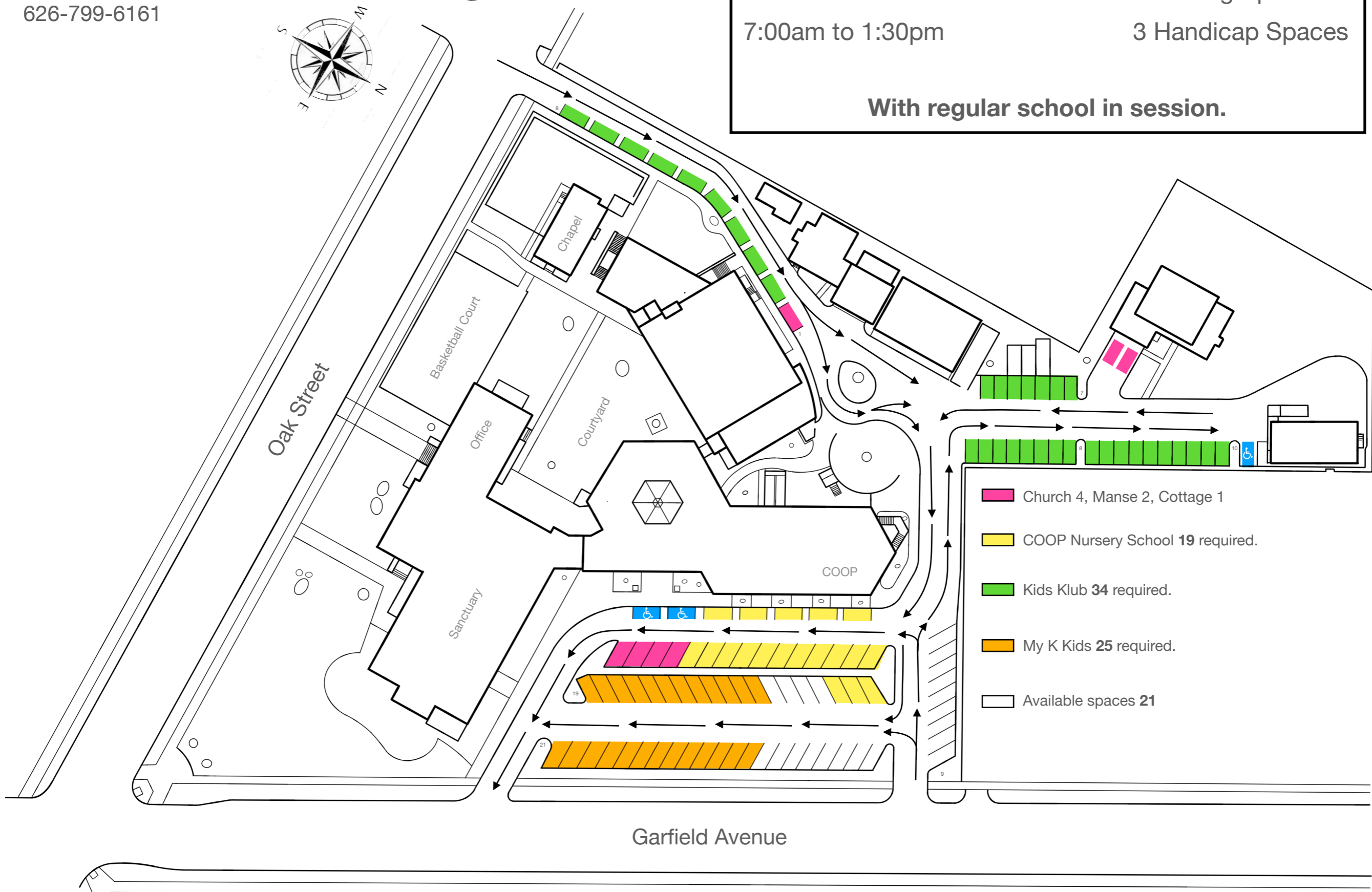
*Use the maximum number that would be on-site at any one time.

Oneonta Congregational Church

1515 Garfield Avenue
South Pasadena, CA 91030
626-799-6161



Campus Parking:
Monday - Friday
7:00am to 1:30pm
104 Parking Spaces
3 Handicap Spaces
With regular school in session.



- Church 4, Manse 2, Cottage 1
- COOP Nursery School 19 required.
- Kids Klub 34 required.
- My K Kids 25 required.
- Available spaces 21

Garfield Avenue

Oneonta Congregational Church

1515 Garfield Avenue
South Pasadena, CA 91030
626-799-6161



Campus Parking:

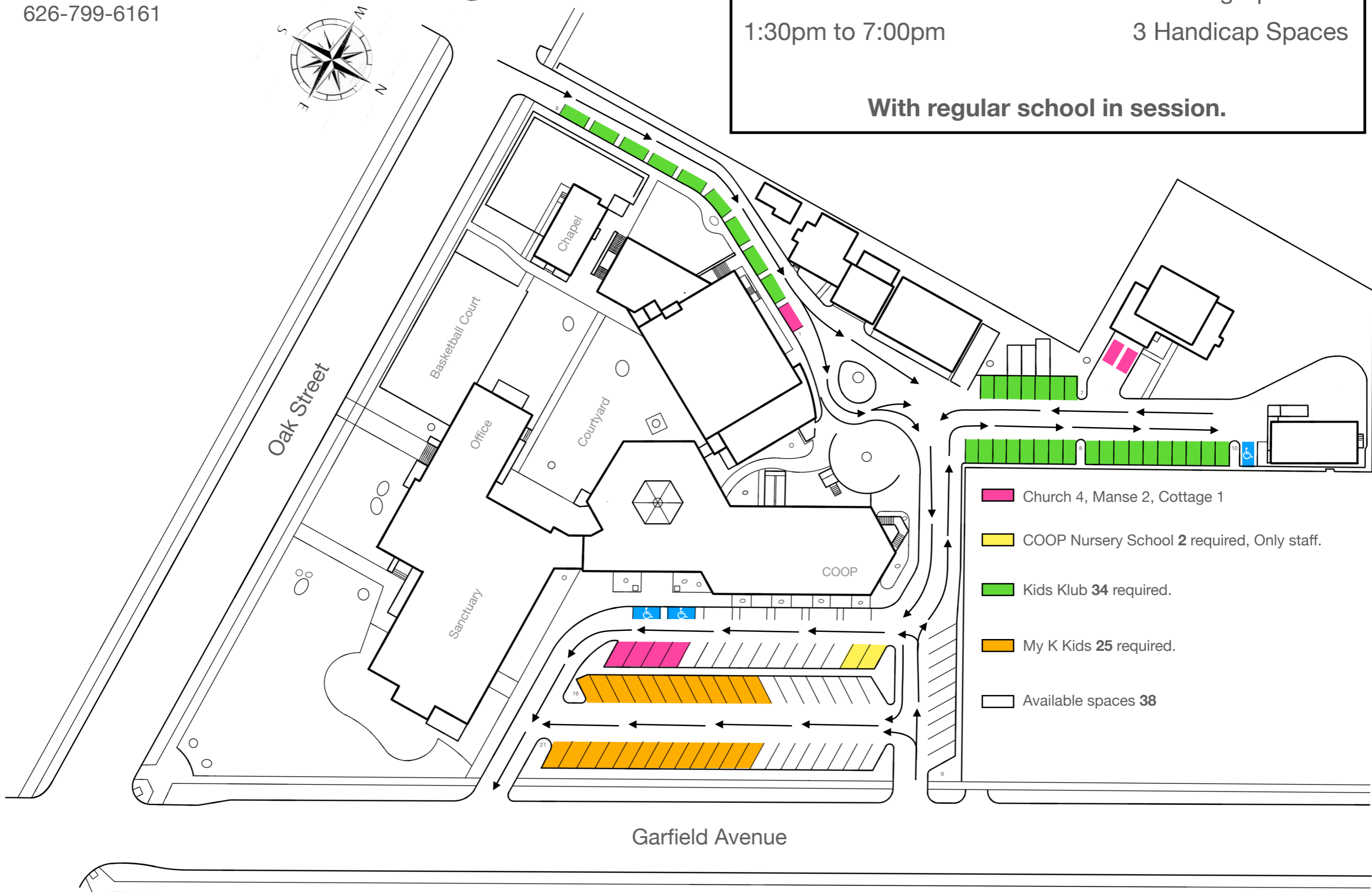
Monday - Friday






1:30pm to 7:00pm

104 Parking Spaces

3 Handicap Spaces

With regular school in session.



-  Church 4, Manse 2, Cottage 1
-  COOP Nursery School 2 required, Only staff.
-  Kids Klub 34 required.
-  My K Kids 25 required.
-  Available spaces 38

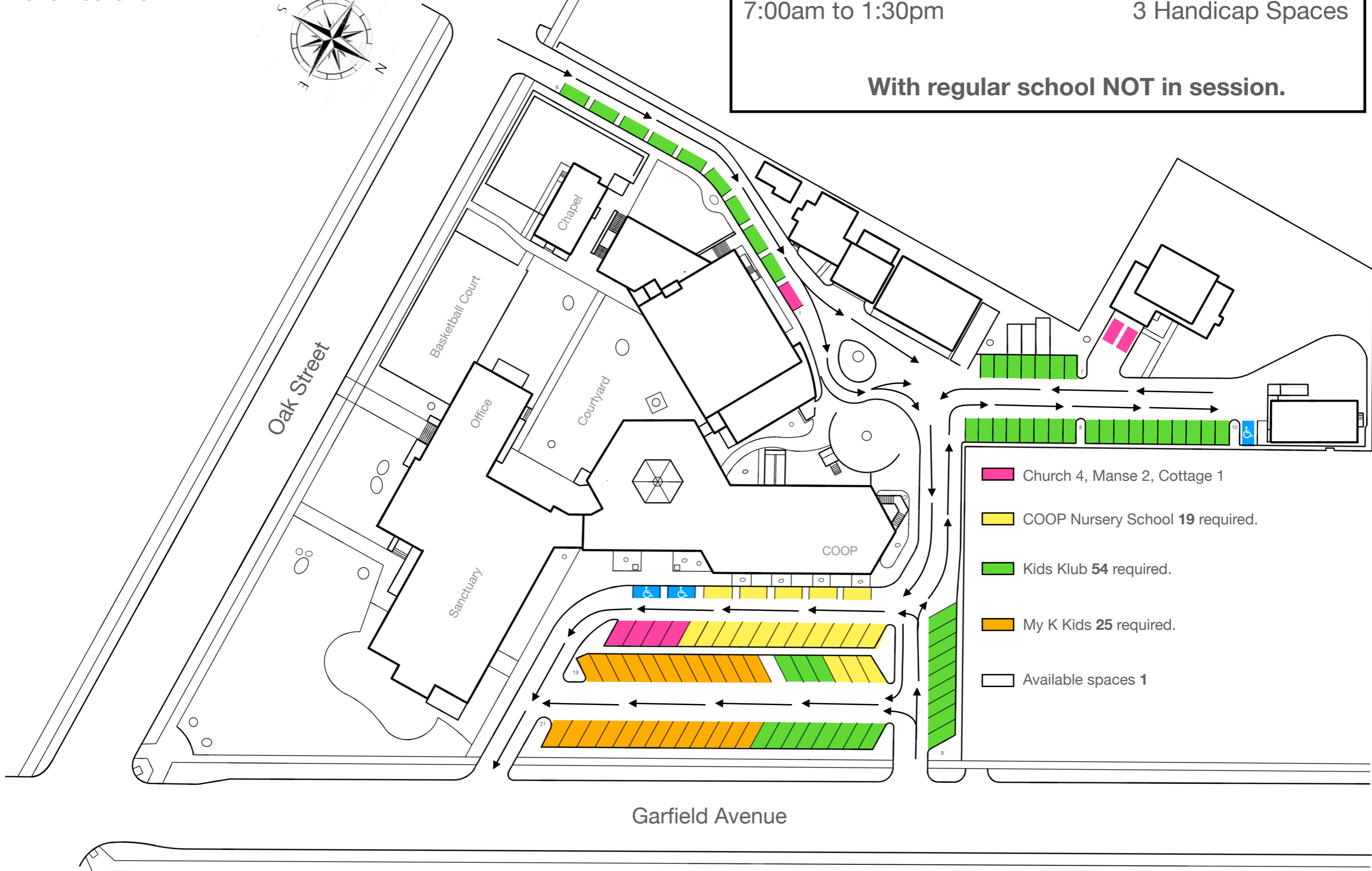
Garfield Avenue

Oneonta Congregational Church

1515 Garfield Avenue
South Pasadena, CA 91030
626-799-6161



Campus Parking:
Monday - Friday
7:00am to 1:30pm
104 Parking Spaces
3 Handicap Spaces
With regular school NOT in session.



- Church 4, Manse 2, Cottage 1
- COOP Nursery School 19 required.
- Kids Klub 54 required.
- My K Kids 25 required.
- Available spaces 1

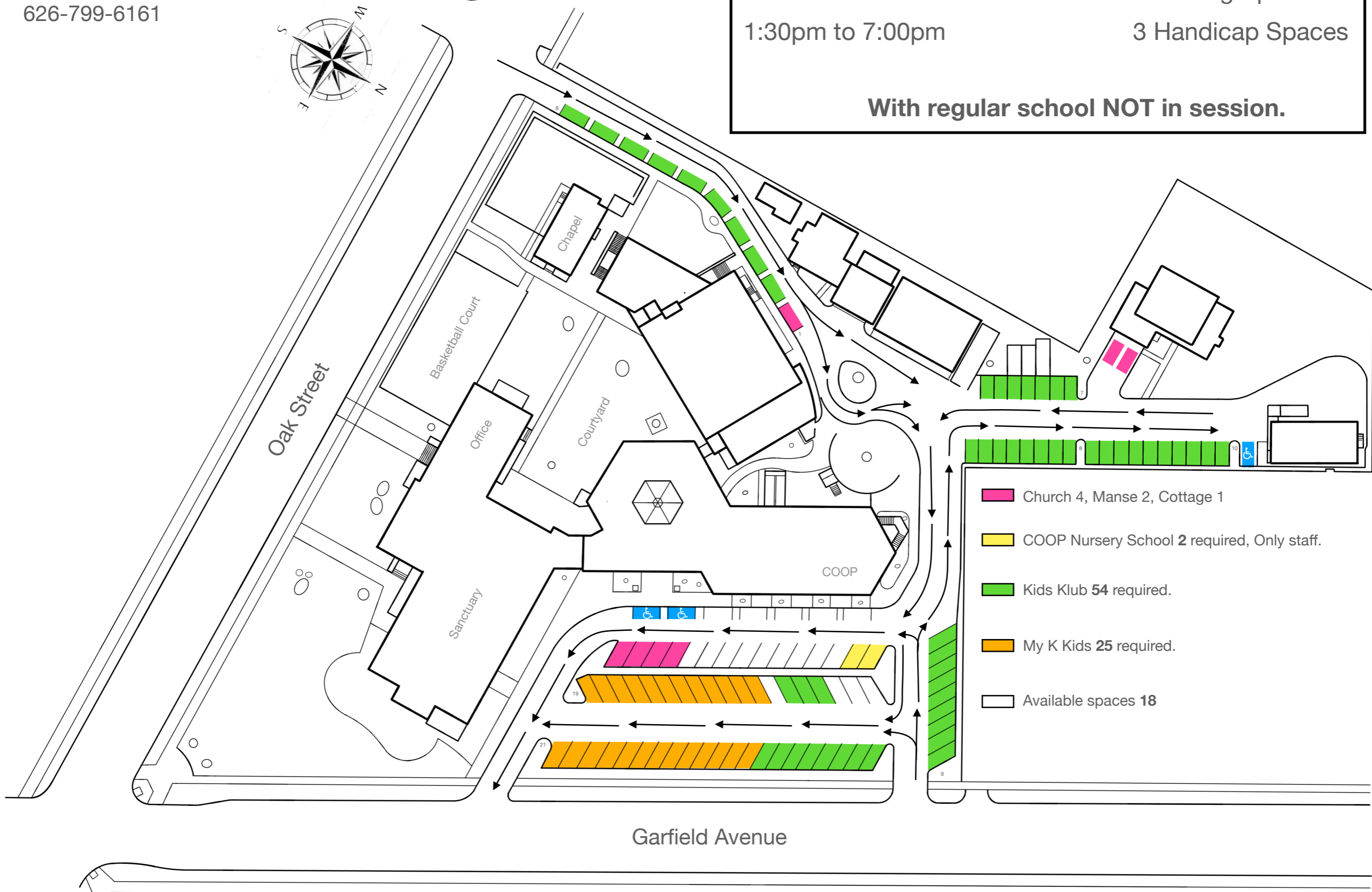
Garfield Avenue

Oneonta Congregational Church

1515 Garfield Avenue
South Pasadena, CA 91030
626-799-6161



Campus Parking:
Monday - Friday
1:30pm to 7:00pm
104 Parking Spaces
3 Handicap Spaces
With regular school NOT in session.



- Church 4, Manse 2, Cottage 1
- COOP Nursery School 2 required, Only staff.
- Kids Klub 54 required.
- My K Kids 25 required.
- Available spaces 18

Garfield Avenue

Oneonta Congregational Church

1515 Garfield Avenue
South Pasadena, CA 91030
626-799-6161

COOP Drop off and Pick Up

Campus Parking:

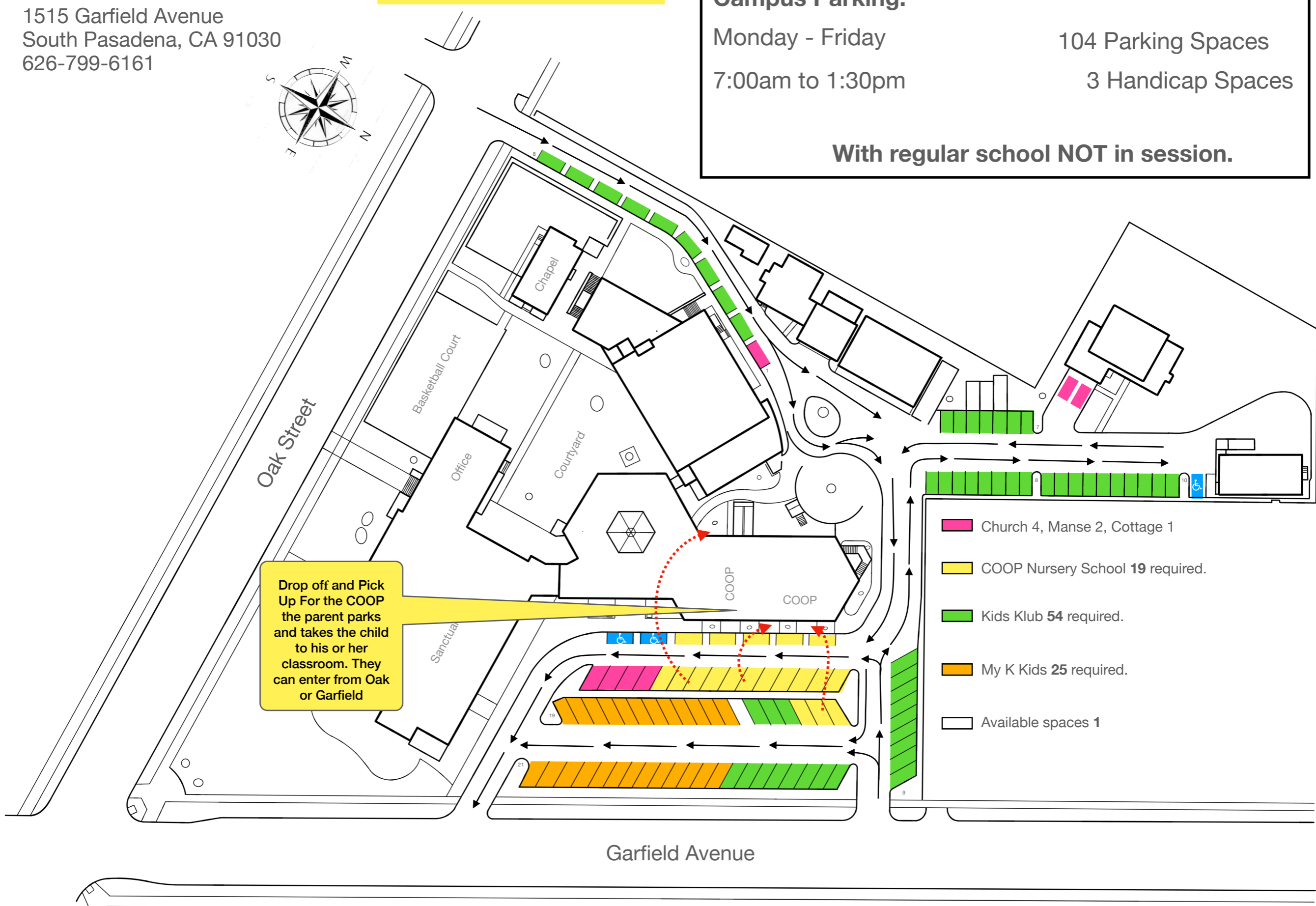
Monday - Friday

7:00am to 1:30pm

104 Parking Spaces

3 Handicap Spaces

With regular school NOT in session.



Drop off and Pick Up For the COOP the parent parks and takes the child to his or her classroom. They can enter from Oak or Garfield

- Church 4, Manse 2, Cottage 1
- COOP Nursery School 19 required.
- Kids Klub 54 required.
- My K Kids 25 required.
- Available spaces 1

Garfield Avenue

Oneonta Congregational Church

1515 Garfield Avenue
South Pasadena, CA 91030
626-799-6161



Kids Klub Pick Up

Campus Parking:

Monday - Friday

5:00pm to 6:00pm

104 Parking Spaces

3 Handicap Spaces

With regular school NOT in session.

Pick Up

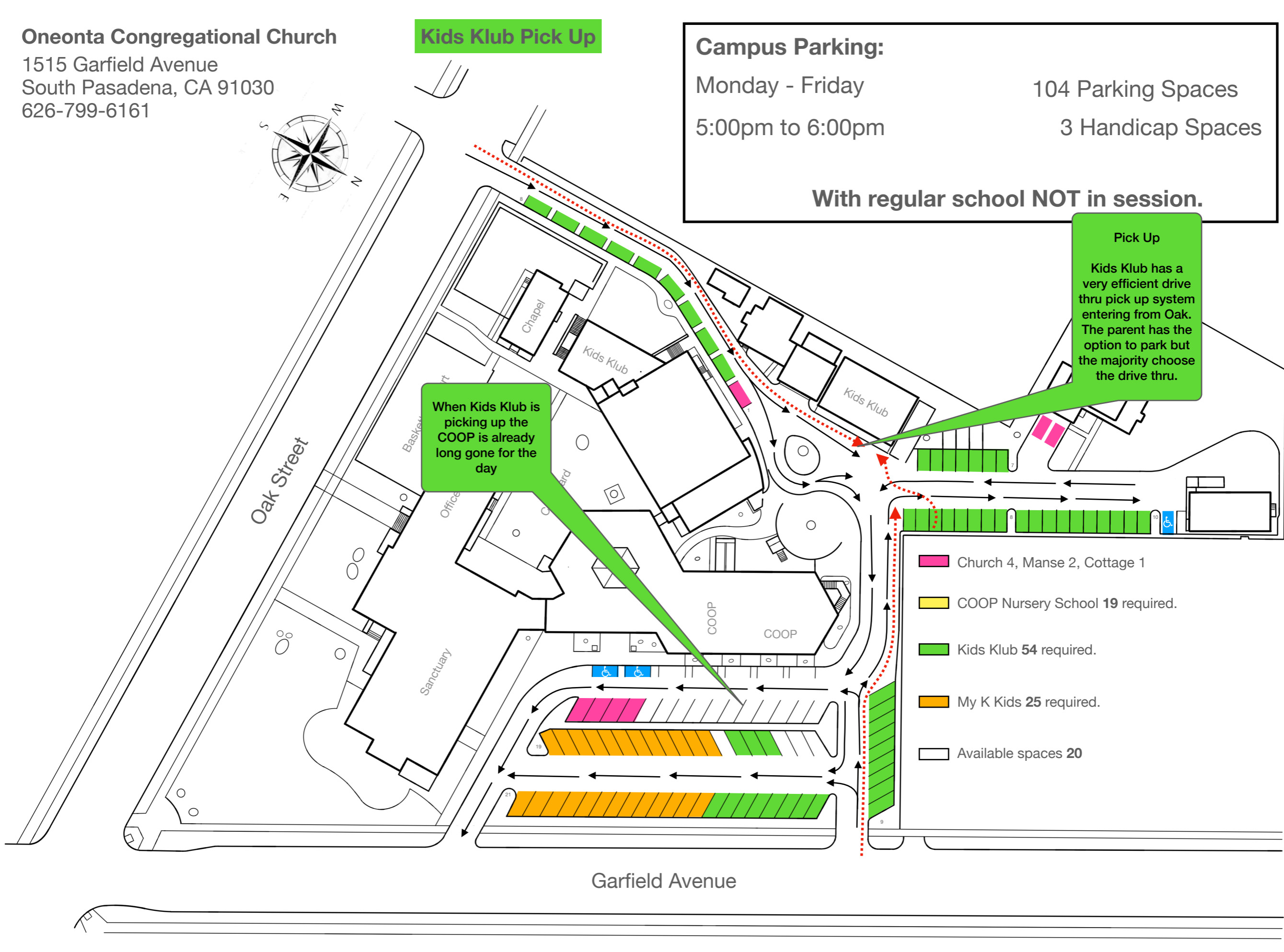
Kids Klub has a very efficient drive thru pick up system entering from Oak. The parent has the option to park but the majority choose the drive thru.

When Kids Klub is picking up the COOP is already long gone for the day

Oak Street

Garfield Avenue

- Church 4, Manse 2, Cottage 1
- COOP Nursery School 19 required.
- Kids Klub 54 required.
- My K Kids 25 required.
- Available spaces 20





Planning Commission Agenda Report

ITEM NO. 3

DATE: October 11, 2022

TO: Planning Commission

FROM: Angelica Frausto-Lupo, Community Development Director
Matt Chang, Planning Manager

PREPARED BY: Ben Jarvis, AICP, Interim Senior Planner

SUBJECT: **Project No. 2456-CUP-DRX-TRP**

A request for a Conditional Use Permit, Design Review, and a Tree Removal Permit for a 1,957 square-foot (SF) addition to an existing 3,216 SF veterinary hospital. The project site is located at 2140 Huntington Drive, Assessor Parcel Number (APN) 5321-015-020, in the Commercial General (CG) zone. The Subject Property is 14,376 SF in size (approximately .33 acre). The project would include the demolition of a 354 SF portion of the existing building, expansion of the veterinary hospital into the entire building, and the removal of one olive tree that is located in the private yard adjacent to Garfield Avenue. The project also includes a request for extended hours of operation from 11:00 p.m. to 6:00 a.m. which would allow the veterinary hospital to operate 24 hours a day, seven days a week.

Recommendation

Staff recommends the Planning Commission adopt Resolution 22-06 (Exhibit A), approving Project No. 2456-CUP-DRX-TRP, subject to the Conditions of Approval (Attachment 1).

Background

On June 16, 2021, Dr. Rebecca Swimmer (Applicant) applied for a Conditional Use Permit (CUP) and Design Review Permit (DRX) to expand an existing building and to extend the operating hours of an existing veterinary hospital use (Vet Villa Animal Hospital) located on the Subject Property at 2140 Huntington Drive (APN 5321-015-020) in the Commercial General (CG) zone. The application was deemed complete on September 20, 2022. The Subject Property is located on the northwest corner of Huntington Drive and Garfield Avenue, and was first developed in the early 1960s. A summary of past development projects is listed below.

Previous Development on the Subject Property

The Subject Property was originally developed as a service station in 1962. A car rental business operated on the site alongside the service station in the early 1970s. Commercial uses ceased on the property in the 1990s, and the parcel remained vacant into the 2000s. Photos 1 and 2, located below, show the original service station on the site dating back to the 1960s.



Photos 1 and 2; top left and right: the Subject Property showing the original service station in the mid-1960s (photo Credit: City of South Pasadena).

On June 27, 2005, in a 3-0 vote with one abstention, the Planning Commission approved Project No. 0479-DRX-CUP, that allowed for the construction of a carwash. The carwash was designed in a Spanish-Mediterranean style, with mission tile roofing, a smooth plaster finish, wood-framed windows, low-pitched gable roof lines, and extensive trellises. A signature clocktower was situated prominently in the center of the structure, creating a dramatic landmark for the City's eastern gateway. While the carwash commenced construction in the 2005-2006 timeframe, the project was never completed, leaving an unfinished building at the intersection for several years. The architectural style originally approved in 2005 has been carried through to all subsequent project approvals for the site.

On September 27, 2011, the Design Review Board approved Project No. 1499-DRX, which allowed the partially constructed building to be remodeled and used as a professional office building. The approval included a 421 SF single story addition and a 285 SF basement. The approval permitted the side trellis along Garfield Avenue to be enclosed and converted the former car wash drying area into a private courtyard. Photo 3 shows the Subject Property as it appeared in 2012, after the carwash project had been abandoned, prior to construction commencing on the office building conversion. In 2014, construction on the new office building was well underway, but the project remained unfinished by 2017.



Photo 3: the abandoned and unfinished carwash structure as seen in 2012 (photo credit: Google Maps).

The next approval for the Subject Property came on August 28, 2017, when, in a 5-0 vote, the Planning Commission approved Project No. 2035 CUP-DRX that allowed for a veterinary hospital and professional office (design studio) on the site. The unfinished carwash structure was remodeled to accommodate the new uses (see Photos 4 and 5).



Photo 4 and Photo 5: Vet Villa Animal Hospital as it appears today

Subject Property Description

The Subject Property is designated as General Commercial under the City’s General Plan and Commercial General under the City’s Zoning Ordinance (CG zone). The project site is also located in the Huntington-Garfield Focus Area Overlay zone. The building is not historic nor is the Subject Property located within a historic district, although the site is located in the Huntington/Garfield Commercial District as identified in the City’s Commercial Design Guidelines document. A rendering of the existing animal hospital is found on page 29 of that document, showing the type of architectural styling envisioned for the intersection of Garfield Avenue and Huntington Drive. The Subject Property abuts the City of San Marino to the east/northeast, and the City of Alhambra is located to the southeast of the property. The Subject Property contains

the first commercial building people see as they enter the City from westbound Huntington Drive or northbound on Garfield Avenue. The existing building is 3,216 square feet (SF), 28 feet in height, and has 15 parking spaces. The central tower element and the trellises in the front of the property give the building a distinct look and provides an attractive entry feature into the City.

Table I provides a land use and zoning summary for the Subject Property as well as for surrounding parcels:

Table I: Land Use and Zoning Summary

Parcel	City Zoning	Existing Land Use	Jurisdiction
Subject Property	Commercial General	Veterinary Hospital	South Pasadena
North	Residential Medium Density	Vacant	South Pasadena
South	Commercial General	Supermarket	South Pasadena
East	R-1 (Residential)	Medical Office Building	San Marino
West	Commercial General	Office Building	South Pasadena

Project Description

The Applicant proposes to remodel the existing building to eliminate the 1,087 SF professional office space/design studio, expand the building, and use the entire structure for the veterinary hospital. The project requires a CUP for the animal hospital expansion and also for extended business hours that would allow the veterinary hospital to operate up to 24 hours per day, seven days per week. The building expansion and extended hours would better serve the community with emergency veterinary services around the clock. The project requires Design Review that will allow for the demolition of 354 SF of the existing structure along with the 1,957 SF expansion. Upon completion, the remodeled building would be 4,819 SF, with a Floor Area Ratio (FAR) of .34, which is an increase over the current FAR of .22. The existing building would be expanded to the east, into an area currently used as a private courtyard. This is the area that was originally slated as the vehicle drying area as part of the 2005 car wash approval. No additional parking is proposed, nor would the proposed addition require any change to the existing traffic circulation on-site. No signage is proposed as part of the project and the veterinary hospital would continue to use the existing signage.

The project would add four new exam rooms, a new dental treatment room, an isolation room, a dog ward, a new restroom, work area, and an expanded reception and waiting area. The new building would be constructed up to the existing courtyard's privacy wall along Garfield Avenue, and the existing planter bed between the sidewalk and wall would be preserved.

Project Analysis: Zoning Compliance

Zoning Analysis: Land Use

The Subject Property is located in the Commercial General (CG) zone and the Huntington-Garfield Focus Area Overlay (HG Overlay) zone. The CG zone is "appropriate for a wide range of commercial retails and service land uses. The CG zoning district is consistent with the General

Commercial land use designation of the General Plan” (South Pasadena Municipal Code (SPMC) Chapter 36.230.020.A). A CUP is required for the proposed extended operating hours between 11:00 p.m. and 6:00 a.m., and also for an Animal Hospital Use in the CG zone (SPMC 36.230.030). The HG Overlay zone offers a significant opportunity for the intensification of commercial uses as a hub to serve the cities of San Marino and Alhambra, as well as South Pasadena, by attracting business revenue from surrounding communities with minimal traffic impacts (South Pasadena General Plan Section 2.7B.1).

Zoning Analysis: Consistency with CG Zone Development Standards

Development standards for the CG zone can be found in SPMC Chapter 36.230.040, Table 2-5. Table II summarizes the relevant development standards for the project:

Table II: CG Zone Development Standards

Front Setback (south frontage, along Huntington Drive)	Not required
Side Setback (west boundary, facing the adjacent office building)	No Required
Streetside Setback (east frontage, along Garfield Avenue)	Not required
Rear Setback (north frontage along the alley, facing a vacant lot)	5' from alley
Lot Coverage	No maximum
Maximum Height	35'

As proposed, the project would meet the development standards listed above. The addition would be set back 3'-6" from the property line along Garfield Avenue and would retain the existing 3'-6" planter area. The new addition would utilize the existing building line along the alley, and the new roofline would be 14'-4" in height, which is below the current primary roofline that measures 15'-3½" in height. The new addition would also be substantially lower than the tower element which is approximately 28' high. These heights are consistent with the maximum height permitted in the CG zone, which is 35'. The project floor area ratio (FAR) is .34, which is also consistent with the CG zone development standards given there is no maximum FAR/lot coverage requirement.

Zoning Analysis: Parking (SPMC 36.310.040)

The parking requirement for a Veterinary Hospital Use is three (3) parking spaces for every 1,000 SF of building area, plus one (1) space for every 1,000 SF of boarding area. These requirements are summarized in Table III:

Table III: CG Zone Parking Standards

Proposed Land Use and Parking Standards (SPMC 36.310.040)	Spaces
Veterinary Hospital Base Requirement: (Proposed 4,819 SF) / (3 space per 1,000 SF)	15 ¹
Boarding Space Requirement: (0 SF) / (1 space per 1,000 SF)	0
Total Parking Required:	15
Existing Parking Provided:	15
Parking Surplus/(Deficit)	0

¹ The actual number is 14.47, which is rounded up to 15 spaces.

As proposed, the project meets the City’s parking requirement. No commercial boarding services will be provided outside of the customary overnight hospital stays, as stipulated in Condition #PL5 in the attached Conditions of Approval.

Zoning Analysis: Landscaping

The proposed project will not alter the existing parking lot landscaping. The building addition would extend into the existing private courtyard area adjacent to Garfield Avenue, building up to and replacing the current privacy wall. The existing 3.5’ wide planter area would be preserved. An olive tree that was planted in the courtyard as part of the building’s construction in 2017, will be removed to accommodate the new addition. A Tree Removal Permit is required and is included in Condition of Approval #PW16.

Zoning Analysis: Extended Business Hours

A CUP is required to allow for “extended hour businesses”, defined as those businesses that operate between the hours of 11:00 p.m. and 6:00 a.m. (SPMC 36.230.030, Table 2-4). The Applicant proposes overnight operation of the animal hospital to provide urgent and emergency care for animals. This type of late night/early morning emergency service is not currently offered within the City or its immediate vicinity. Should the Planning Commission approve the requested CUP for the extended business hours, these services would be available to City residents 24 hours a day, seven days a week.

Conditional Use Permit (CUP) Findings

In order to approve a CUP, the Planning Commission must make certain findings listed in SPMC section 36.410.060. The required findings are listed below:

- 1. The proposed use is allowed with Conditional Use Permit approval within the applicable zoning district and complies with all applicable provisions of this Zoning Code;**

The proposed veterinary hospital expansion, including the extended operating hours between 11:00 p.m. and 6:00 a.m., is conditionally permitted within the CG zone. As proposed, the project would be consistent with the development standards for the

CG zone, including setbacks, building height, parking, and other development requirements.

2. The proposed use is consistent with the General Plan and any applicable specific plan;

The proposed project is consistent with the following Goals and Policies of the General Plan:

Goal 1: *To support existing businesses.*

Policy 1.1: *Help local businesses retain and increase their competitive edge.*

Goal 2: *To increase city revenues by fostering commercial development.*

Policy 2.3: *Secure the City's share of the San Gabriel Valley region's economic investment and development.*

Goal 4: *To target development interests into the focus areas that will maximize benefits to the City.*

Policy 4.1: *Promote the revitalization, intensification, and development potential...for the Focus Areas.*

The proposed project supports the General Plan Goals and Policies listed above because it would allow an existing business to expand and increase the veterinary hospital's competitive advantage through extended business hours up to 24 hours a day. The project would foster commercial development to help secure the City's share of economic activity in the San Gabriel Valley by providing additional emergency veterinary services, which could attract business and revenue from outside the City. Lastly, the project would intensify commercial development at the northwest corner of Huntington Drive and Garfield Avenue, one of the City's designated focus areas where the General Plan envisions auto-oriented and retail uses that draw from the surrounding area.

3. The establishment, maintenance, or operation of the use would not, under the circumstances of the particular case, be detrimental to the health, safety, or general welfare of persons residing or working in the neighborhood of the proposed use;

The proposed project would expand an existing veterinary hospital and provide additional services to the community, including potential urgent and emergency services during overnight hours. The building addition would be constructed in the same Spanish-Mediterranean style of the current structure, using similar materials and colors. The existing parking lot configuration and access points would remain

as they are today. Primary access to the Subject Property is taken from Huntington Drive, a major regional thoroughfare, and any overnight services would be provided within the building without any anticipated impacts to local residents. As the project would expand a business that already operates at the location, the project would not be expected to be detrimental to the health, safety, or general welfare of people working or residing in the neighborhood because the types of uses that would be provided by the project already exist.

4. The use, as described and conditionally approved, would not be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the City;

As stated above, the project consists of an expansion to an existing veterinary hospital. The building expansion would not impact the Subject Property's parking capacity, driveways, or circulation. As animal hospital services are currently provided on the site without impacts to the surrounding neighborhood, it is not anticipated the project would create additional impacts given the services provided would be entirely contained within the building. Therefore, the project would not be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the City.

5. The subject site is adequate in terms of size, shape, topography, and circumstances and has sufficient access to streets and highways which are adequate in width and pavement type to carry the quantity and quality of traffic expected to be generated by the proposed use; and

The Subject Property can adequately accommodate the proposed veterinary hospital expansion in terms of size, shape, topography, and circumstances. The Subject Property is fully developed as a commercial use (a veterinary hospital and design studio), is generally flat, and is typical of other commercial properties in the area. The Subject Property has sufficient access to streets and highways, which are adequate in width and pavement type, to carry the quantity and quality of traffic expected to be generated by the proposed use. The Subject Property takes its primary access from Huntington Drive, a six-lane east/west arterial that fronts the project site. Secondary access is provided via an alley that extends from Garfield Avenue on the east to La Senda Place on the west. The Subject Property has 15 parking spaces which satisfy the code-required parking for the proposed project.

6. The design, location, operating characteristics, and size of the proposed use would be compatible with the existing and future land uses in the vicinity, in terms of aesthetics, character, scale, impacts on neighboring properties.

The proposed project is located at a gateway to the City at the northwest corner of Huntington Drive and Garfield Avenue, an area with auto-oriented land uses that serve not only City residents but also people who live elsewhere in the San Gabriel

Valley and beyond. The expanded veterinary hospital would function much the same as the existing veterinary hospital, albeit in an expanded footprint and with emergency services available 24 hours a day, seven days a week. The proposed use is compatible with other existing and potential future uses in the vicinity, including office uses immediately west of the Subject Property and the medical offices to the east in the City of San Marino. The addition would be designed in the same Spanish-Mediterranean style as the existing building, with similar scale and massing. Roof slopes and eaves would match the existing structure, and exterior walls would be finished with the same smooth, traditional stucco as the current building. As such, the project would be compatible with the aesthetics, character, and scale of the area, and would not be expected to impact neighboring properties.

Based on the information above, the Planning Commission can make all of the necessary findings to support the issuance of the Conditional Use Permit required for the project.

Design Review Findings

The project is subject to a Design Review Permit as specified in SPMC 36.410.040.B.1.b and SPMC 36.410.040.D.1.a. The findings for Design Review found in SPMC 36.410.040.I (Required Findings) are listed below and require that the proposed project:

- 1. Is consistent with the General Plan, any adopted design guidelines and any applicable design criteria for specialized areas (e.g. designated historic or other special districts, plan developments, or specific plans);**

The proposed project is the type of land use contemplated in the CG land use designation under the General Plan. With the issuance of a CUP, the proposed project would be consistent with the Zoning Ordinance in terms of the land use and the extended hours of operation. The project is located in the Huntington-Garfield Commercial District, a focus area identified in both the General Plan and the City's Commercial Design Guidelines. Objectives for the Huntington-Garfield District include the conversion of lower density commercial uses to a denser assemblage of retail uses which should remain auto-oriented. The proposed project would expand an existing veterinary hospital, thereby using the land more efficiently and providing denser commercial use, consistent with the General Plan and Commercial Design Guidelines objectives. The addition would respect the Spanish/Mediterranean style of the existing building, creating an inviting gateway into South Pasadena from both the south and east via Huntington Drive, Garfield Avenue, and Atlantic Boulevard/Los Robles Avenue.

- 2. Will adequately accommodate the functions and activities proposed for the site, will not unreasonably interfere with the use and enjoyment of neighboring, existing, or future developments, and will not create adverse pedestrian or traffic hazards;**

The proposed project would not interfere with the use and enjoyment of neighboring, existing, or future developments, and will not create adverse pedestrian or traffic hazards because the commercial activities associated with the proposed project would occur within the expanded building and no changes to the existing sidewalks or streets are proposed. The existing 3.5'-wide planter along Garfield Avenue would be retained and the new building addition would not extend beyond the limit of the existing privacy wall. No alterations to driveways or the parking areas are proposed as part of the project.

- 3. Is compatible with the existing character of the surrounding neighborhood and that all reasonable design efforts have been made to maintain the attractive, harmonious, and orderly development contemplated by this section and the General Plan; and**

The proposed project would be consistent with the existing building in terms of height, color, materials, and massing. The addition would be designed in a similar scale and would be subordinate to the massing of the existing building. Exterior walls would be finished with smooth, traditional stucco, matching the current building. The width, form, and rooflines of the new addition would likewise complement the existing building. Roof slopes and eaves projections would also match, with wood eaves accents and traditional rooflines that include Spanish-style roof tiles. The existing trellis structures in the parking lot would remain, and the addition would better emphasize the building's customer entrance. Therefore, the project would be compatible with character of the community and would maintain the attractive, harmonious, and orderly development contemplated by the General Plan.

- 4. Would provide a desirable environment for its occupants and neighbors, and is aesthetically of good composition, materials and texture that would remain aesthetically appealing with a reasonable level of maintenance and upkeep.**

The proposed project would be developed in the same Spanish-Mediterranean style as the existing building, using similar materials, colors, and massing. The project would utilize good composition, materials, and texture (stucco, Spanish-style roof tiles, and wood-framed windows), thereby continuing to provide the desirable environment the site's current occupants and neighbors have become accustomed to.

Figures 1 and 2 show what the proposed building expansion would look like:



Figure 1: The proposed addition as viewed from Garfield Avenue. Note the window detail and roofing materials that match the existing building. The use of lighting sconces will further tie the addition back to the existing building. The existing planter area along the sidewalk will be preserved.



Figure 2: This elevation shows what the proposed addition would look like from the existing parking lot facing Huntington Drive. The existing shade trellises in the parking lot would be retained, as would the landscaping adjacent to the parking lot. The windows in the new addition would match the windows in the existing building. The existing signage on the tower element would remain and no new signage is proposed.

The new addition would use La Habra stucco with a Santa Barbara finish, with a French Vanilla color to match the existing structure. The addition would use 3-pane Marvin casement windows with blue trim. All windows would be recessed by six inches and would have flare stucco

jamb/headers to match the existing building. Roofing materials would also match the existing building, utilizing two-piece clay tiles with random mortar packing and booster tiles at eaves and ridges.

Huntington-Garfield Focus Area Overlay Findings

The project is subject to findings listed in SPMC 36.250.040, requiring:

- 1. That the proposed project is consistent with, and will successfully implement the objectives and guidelines of the General Plan for the applicable focus area; and**
- 2. All other findings required by this Zoning Code for the discretionary land use permit.**

The proposed project is consistent with the objectives and guidelines listed in Chapter 2.7B.1 of the General Plan for the Huntington-Garfield Focus Area. Specifically, the project would intensify commercial land use, maintain the “auto orientation” of the area taking advantage of the adjacency of three major arterial roads, and visually strengthen the important City entry with a desired architectural image (Guidelines 1.2, 1.5, 1.6, and 1.7). As the project is consistent with SPMC 36.250.040 and all other findings required by the Zoning Code listed above, the Planning Commission can make the findings to support the project.

Tree Removal Permit

As part of the project, an existing olive tree that was planted as part of the original approval in 2017, would be removed to accommodate the expanded animal hospital. In accordance with SPMC Section 34.12-5, two replacement trees will be provided to the City and the applicant will pay a deposit of \$670. This requirement is memorialized in Condition of Approval #PW16. SPMC Chapter 34 does not require that findings be made for approval of a Tree Removal Permit.

Environmental Analysis

This project is exempt from the California Environmental Quality Act (CEQA) as a Class 1 Categorical Exemption (Existing Facilities). Class I Categorical Exemptions include additions to existing structures up to 10,000 square feet, provided the project site is in an area where all public services and facilities are available, and provided the project site is not located in an environmentally sensitive area (CEQA, Section 15031). As the Subject Property is fully developed, has had commercial activities on the site for the past 60 years, and is not located in an environmentally sensitive area; and given the proposed project consists of a 1,957 square-foot expansion to an existing veterinary hospital in a commercial area where such uses are contemplated by the General Plan, Zoning Ordinance, and where all utilities and public facilities are available to serve the proposed project, therefore, a Class 1 Categorical Exemption applies.

Staff Recommendation

Based on the information and materials provided by the applicant, and the proposed project's consistency with the General Plan and Zoning Ordinance, staff has drafted the necessary findings in support of the project and recommends the Planning Commission adopt Resolution 22-06 approving Project No. 2456-CUP-DRX-TRP, subject to the Conditions of Approval (Attachment 1).

Alternatives to Consider

If the Commission does not agree with the staff recommendation, the following options are available:

1. The Planning Commission can Approve the project with modified condition(s) and/or findings;
2. The Planning Commission can Continue the project, providing the applicant with clear recommendations to revise the proposal; or
3. The Planning Commission can Deny the project if the Commission finds the project is not consistent with the General Plan, Zoning Ordinance, and/or does not satisfy the required findings for a Conditional Use Permit and/or Development Review Permit.

Public Noticing/Public Comment

A Public Hearing Notice was published on September 30, 2022, in the South Pasadena Review. Hearing notices were also sent to all properties within a 300-foot radius on September 29, 2022 (96 notices, total). Additionally, this item was included on the legally noticed public agenda that was posted at both City Hall and on the City's website. At the time of the preparation of this agenda report, staff has not received any comments on the project.

Attachments

1. Resolution 22-06 - Conditions of Approval (Exhibit A)
2. Vicinity Map
3. Zoning Map
4. Project Plans

ATTACHMENT 1

Resolution 22-06 and Conditions of Approval (Exhibit A)

P.C. RESOLUTION NO. 22-06

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF SOUTH PASADENA, CALIFORNIA, APPROVING PROJECT NO. 2456-CUP-DRX-TRP, INCLUDING A CONDITIONAL USE PERMIT, DESIGN REVIEW PERMIT, AND A TREE REMOVAL PERMIT FOR A 1,957 SQUARE-FOOT EXPANSION TO AN EXISTING ANIMAL HOSPITAL INCLUDING EXTENDED BUSINESS HOURS FOR UP TO 24-HOUR OPERATION, AND DESIGN REVIEW FOR THE PROJECT LOCATED AT 2140 HUNTINGTON DRIVE (ASSESSOR PARCEL NUMBER 5321-015-020), IN THE CITY OF SOUTH PASADENA, CALIFORNIA, SUBJECT TO THE CONDITIONS OF APPROVAL (ATTACHMENT 1).

WHEREAS, on June 16, 2021, the applicant, Dr. Rebecca Swimmer, submitted an application for a Conditional Use Permit (CUP), Design Review Permit (DRX), and a Tree Removal Permit (TRP), to expand an existing animal hospital use previously approved by the Planning Commission on August 28, 2017, the project consisting of the demolition of 354 square feet (SF) of the existing building and a 1,957 SF addition to create a new building size of approximately 4,819 SF with extended operating hours that would allow the business to provide services 24 hours per day, seven days a week on the Subject Property located at 2140 Huntington Drive (Assessor Parcel Number 5321-015-020); and

WHEREAS, the subject property is zoned Commercial General (CG) and has a General Plan Land Use Designation of General Commercial, and is located in the Huntington-Garfield Focus Area Overlay; and

WHEREAS, the Planning Division evaluated the project for consistency with the City's General Plan, City of South Pasadena Municipal Code, and all other applicable state and local regulations; and

WHEREAS, on September 29, 2022, a public hearing notice was mailed to each property owner within a 300-foot radius of the Subject Property in accordance with the requirements of South Pasadena Municipal code for the required conditional use permit by the Planning Commission for the hearing on October 11, 2022; and

WHEREAS, on September 30, 2022, the City of South Pasadena Planning Division, published a legal notice in the *South Pasadena Review*, a local newspaper of general circulation, indicating the date, time, and location of the public hearing in compliance with state law concerning Project No. 2456-CUP-DRX-TRP; and

WHEREAS, the Planning Commission held a duly noticed public hearing on October 11, 2022, at which time the Commission considered the staff report, oral report, public testimony, and written evidence submitted by and on behalf of the applicant and by members of the public concerning Project No. 2456-CUP-DRX-TRP, and considered

the proposed CUP, DRX, and TRP to expand the existing veterinary hospital and to allow for extended operating hours.

NOW, THEREFORE, THE PLANNING COMMISSION OF THE CITY OF SOUTH PASADENA, CALIFORNIA, DOES HEREBY FIND, DETERMINE, AND RESOLVE AS FOLLOWS:

SECTION 1: ENVIRONMENTAL REVIEW FINDING

The Planning Commission has determined the proposed project is Categorically Exempt from the provisions of the California Environmental Quality Act (CEQA), under Article 19 Section 15301, Class 1 – Existing Facilities the California Guidelines for Implementation of CEQA. Class 1 Categorical Exemptions include additions to existing structures provided that the addition will not result in an increase of more than 10,000 square feet, in which the project site is in an area where all public facilities are available and is not located in an environmentally sensitive area.

SECTION 2: CONDITIONAL USE PERMIT FINDINGS

The Planning Commission finds that the proposed project is consistent with all applicable findings for approval of a Conditional Use Permit pursuant to South Pasadena Municipal Code (SPMC) Section 36.410.060, as follows:

- 1. The proposed use is allowed with Conditional Use Permit approval within the applicable zoning district and complies with all applicable provisions of this Zoning Code;**

The proposed veterinary hospital expansion, including the extended operating hours between 11:00 p.m. and 6:00 a.m., is conditionally permitted within the CG zone. As proposed, the project would be consistent with the development standards for the CG zone, including setbacks, building height, parking, and other development requirements.

- 2. The proposed use is consistent with the General Plan and any applicable specific plan;**

The proposed project is consistent with the following Goals and Policies of the General Plan:

Goal 1: *To support existing businesses.*

Policy 1.1: *Help local businesses retain and increase their competitive edge.*

Goal 2: *To increase city revenues by fostering commercial development.*

- Policy 2.3: *Secure the City's share of the San Gabriel Valley region's economic investment and development.*
- Goal 4: *To target development interests into the focus areas that will maximize benefits to the City.*
- Policy 4.1: *Promote the revitalization, intensification, and development potential...for the Focus Areas.*

The proposed project supports the General Plan Goals and Policies listed above because it would allow an existing business to expand and increase the veterinary hospital's competitive advantage through extended business hours up to 24 hours a day. The project would foster commercial development to help secure the City's share of economic activity in the San Gabriel Valley by providing additional emergency veterinary services, which could attract business and revenue from outside the City. Lastly, the project would intensify commercial development at the northwest corner of Huntington Drive and Garfield Avenue, one of the City's designated focus areas where the General Plan envisions auto-oriented and retail uses that draw from the surrounding area.

- 3. The establishment, maintenance, or operation of the use would not, under the circumstances of the particular case, be detrimental to the health, safety, or general welfare of persons residing or working in the neighborhood of the proposed use;**

The proposed project would expand an existing veterinary hospital and provide additional services to the community, including potential urgent and emergency services during overnight hours. The building addition would be constructed in the same Spanish-Mediterranean style of the current structure, using similar materials and colors. The existing parking lot configuration and access points would remain as they are today. Primary access to the Subject Property is taken from Huntington Drive, a major regional thoroughfare, and any overnight services would be provided within the building without any anticipated impacts to local residents. As the project would expand a business that already operates at the location, the project would not be expected to be detrimental to the health, safety, or general welfare of people working or residing in the neighborhood because the types of uses that would be provided by the project already exist.

- 4. The use, as described and conditionally approved, would not be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the City;**

As stated above, the project consists of an expansion to an existing veterinary hospital. The building expansion would not impact the Subject Property's parking capacity, driveways, or circulation. As animal hospital services are currently provided on the site without impacts to the surrounding neighborhood, it is not

anticipated the project would create additional impacts given the services provided would be entirely contained within the building. Therefore, the project would not be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the City.

- 5. The subject site is adequate in terms of size, shape, topography, and circumstances and has sufficient access to streets and highways which are adequate in width and pavement type to carry the quantity and quality of traffic expected to be generated by the proposed use; and**

The Subject Property can adequately accommodate the proposed veterinary hospital expansion in terms of size, shape, topography, and circumstances. The Subject Property is fully developed as a commercial use (a veterinary hospital and design studio), is generally flat, and is typical of other commercial properties in the area. The Subject Property has sufficient access to streets and highways, which are adequate in width and pavement type, to carry the quantity and quality of traffic expected to be generated by the proposed use. The Subject Property takes its primary access from Huntington Drive, a six-lane east/west arterial that fronts the project site. Secondary access is provided via an alley that extends from Garfield Avenue on the east to La Senda Place on the west. The Subject Property has 15 parking spaces which satisfy the code-required parking for the proposed project.

- 6. The design, location, operating characteristics, and size of the proposed use would be compatible with the existing and future land uses in the vicinity, in terms of aesthetics, character, scale, impacts on neighboring properties.**

The proposed project is located at a gateway to the City at the northwest corner of Huntington Drive and Garfield Avenue, an area with auto-oriented land uses that serve not only City residents but also people who live elsewhere in the San Gabriel Valley and beyond. The expanded veterinary hospital would function much the same as the existing veterinary hospital, albeit in an expanded footprint and with emergency services available 24 hours a day, seven days a week. The proposed use is compatible with other existing and potential future uses in the vicinity, including office uses immediately west of the Subject Property and the medical offices to the east in the City of San Marino. The addition would be designed in the same Spanish-Mediterranean style as the existing building, with similar scale and massing. Roof slopes and eaves would match the existing structure, and exterior walls would be finished with the same smooth, traditional stucco as the current building. As such, the project would be compatible with the aesthetics, character, and scale of the area, and would not be expected to impact neighboring properties.

SECTION 3: DESIGN REVIEW PERMIT FINDINGS

The Planning Commission finds that the proposed project is consistent with all applicable findings for approval of a Design Review Permit pursuant to SPMC Section 36.410.040, requiring that the proposed project:

- 1. Is consistent with the General Plan, any adopted design guidelines and any applicable design criteria for specialized areas (e.g. designated historic or other special districts, plan developments, or specific plans);**

The proposed project is the type of land use contemplated in the CG land use designation under the General Plan. With the issuance of a CUP, the proposed project would be consistent with the Zoning Ordinance in terms of the land use and the extended hours of operation. The project is located in the Huntington-Garfield Commercial District, a focus area identified in both the General Plan and the City's Commercial Design Guidelines. Objectives for the Huntington-Garfield District include the conversion of lower density commercial uses to a denser assemblage of retail uses which should remain auto-oriented. The proposed project would expand an existing veterinary hospital, thereby using the land more efficiently and providing denser commercial use, consistent with the Commercial Design Guidelines objectives. The addition would respect the Spanish/Mediterranean style of the existing building, creating an inviting gateway into South Pasadena from both the south and east via Huntington Drive, Garfield Avenue, and Atlantic Boulevard/Los Robles Avenue.

- 2. Will adequately accommodate the functions and activities proposed for the site, will not unreasonably interfere with the use and enjoyment of neighboring, existing, or future developments, and will not create adverse pedestrian or traffic hazards;**

The proposed project would not interfere with the use and enjoyment of neighboring, existing, or future developments, and will not create adverse pedestrian or traffic hazards because the commercial activities associated with the proposed project would occur within the expanded building and no changes to the existing sidewalks or streets are proposed. The existing 3.5'-wide planter along Garfield Avenue would be retained and the new building addition would not extend beyond the limit of the existing privacy wall. No alterations to driveways or the parking areas are proposed as part of the project.

- 3. Is compatible with the existing character of the surrounding neighborhood and that all reasonable design efforts have been made to maintain the attractive, harmonious, and orderly development contemplated by this section and the General Plan; and**

The proposed project would be consistent with the existing building in terms of height, color, materials, and massing. The addition would be designed in a similar scale and would be subordinate to the massing of the existing building. Exterior walls would be finished with smooth, traditional stucco, matching the current building. The width, form, and rooflines of the new addition would

likewise complement the existing building. Roof slopes and eaves projections would also match, with wood eaves accents and traditional rooflines that include Spanish-style roof tiles. The existing trellis structures in the parking lot would remain, and the addition would better emphasize the building's customer entrance. Therefore, the project would be compatible with the character of the community and would maintain the attractive, harmonious, and orderly development contemplated by the General Plan.

- 4. Would provide a desirable environment for its occupants and neighbors, and is aesthetically of good composition, materials and texture that would remain aesthetically appealing with a reasonable level of maintenance and upkeep.**

The proposed project would be developed in the same Spanish-Mediterranean style as the existing building, using similar materials, colors, and massing. The project would utilize good composition, materials, and texture (stucco, Spanish-style roof tiles, and wood-framed windows), thereby continuing to provide the desirable environment the site's current occupants and neighbors have become accustomed to.

SECTION 4: HUNTINGTON-GARFIELD FOCUS AREA OVERLAY FINDINGS

The Planning Commission finds that the proposed project is consistent with all applicable findings for development within the Huntington-Garfield Focus Area Overlay District, pursuant to SPMC 36.250.040, requiring:

- 1. That the proposed project is consistent with, and will successfully implement the objectives and guidelines of the General Plan for the applicable focus area; and**
- 2. All other findings required by this Zoning Code for the discretionary land use permit.**

The proposed project is consistent with the objectives and guidelines listed in Chapter 2.7B.1 of the General Plan for the Huntington-Garfield Focus Area. Specifically, the project would intensify commercial land use, maintain the "auto orientation" of the area taking advantage of the adjacency of three major arterial roads, and visually strengthen the important City entry with a desired architectural image (Guidelines 1.2, 1.5, 1.6, and 1.7). As the project is consistent with SPMC 36.250.040 and all other findings required by the Zoning Code listed above, the Planning Commission can make the findings to support the project.

SECTION 5: TREE REMOVAL PERMIT

Based on the evidence presented to the Planning Commission, the submitted materials, staff report, public testimony, and discussion, the Planning Commission approves the Tree Removal Permit for the project. Two replacement trees will be provided to the City and the applicant shall pay the required fee/deposit of \$670. SPMC Chapter 34 does not contain required findings for tree removals. As such, the Planning Commission can approve the Tree Removal Permit without making findings.

SECTION 6: RECORD OF PROCEEDING

The documents and other materials that constitute the record of the proceedings upon which the Planning Commission's decision is based, which include, but are not limited to, the staff reports, as well as all materials that support the staff reports for the proposed project, and are located in the Community Development Department of the City of South Pasadena at 1414 Mission Street, South Pasadena, CA 91030. The custodian of these documents is the City Clerk of the City of South Pasadena.

SECTION 7: DETERMINATION

Based upon the findings outlined in Sections 1 through 5 above and provided during the public hearing, the Planning Commission of the City of South Pasadena hereby approves Project No. 2456-CUP-DRX-TRP, consisting of a Conditional Use Permit, Design Review Permit, and Tree Removal Permit to approve a 1,957 square-foot expansion to an existing veterinary hospital (for a total building area of 4,819 square feet), to approve extended operating hours between 11:00 p.m. and 6:00 a.m., allowing for services to be offered up to 24 hours a day, seven days a week, and to approve the removal of one (1) olive tree, for the Subject Property located at 2140 Huntington Drive in the City of South Pasadena, California, Assessor Parcel Number 5321-015-020, subject to the Conditions of Approval (Attachment 1).

SECTION 8: APPEALS

Any interested person may appeal this decision or any portion of this decision to the City Council. Pursuant to the South Pasadena Municipal Code, any such appeal must be filed with the City, in writing, and with appropriate appeal fee, no later than 15 days, following the date of the Planning Commission's final action.

SECTION 9: CERTIFICATION OF THE RESOLUTION

The Secretary shall certify that the foregoing Resolution was adopted by the Planning Commission of the City of South Pasadena at a duly noticed regular meeting held on the 11th day of October 2022.

PASSED, APPROVED, AND ADOPTED this 11th day of October 2022 by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

John Lesak, Chair

ATTEST:

Amitabh Barthakur, Secretary to the Planning Commission

ATTACHMENT 1
CONDITIONS OF APPROVAL
Conditional Use Permit and Design Review Permit

PROJECT NO. 2456-CUP-DRX-TRP
2140 Huntington Drive (APN: 5321-015-020)

CONDITIONS OF APPROVAL

PLANNING DIVISION:

Project Specific Conditions

- PL1. This Conditional Use Permit (CUP) is an approval to add 1,957 square feet (SF) to an existing 3,216 SF veterinary hospital that was previously approved under 2035-CUP-DRX. This approval allows for extended operating hours of up to 24 hours, daily. This approval also includes 354 SF of demolition to accommodate the new addition. Upon completion, the expanded building would be 4,819 SF, and the veterinary hospital would occupy the entire commercial space.
- PL2. No additional signage is approved as part of this project. New signs, if desired, shall require a separate permit.
- PL3. The applicant shall obtain any applicable permits from the Building Division prior to commencement of any construction for the project. All construction/remodel/tenant improvement plans shall be in substantial conformance with the plans approved by the Planning Commission.
- PL4. A business license from the City's Finance Department, if necessary for the veterinary hospital expansion, shall be obtained prior to occupancy of the building addition.
- PL5. Overnight boarding within the veterinary clinic shall only be for hospitalized animals. Non-hospital related boarding/kennel uses are prohibited.
- PL6. The hours of operation for the veterinary clinic are permitted to be 24 hours a day, seven days a week.
- PL7. If operation of the veterinary clinic results in conflicts related to interior or exterior noise, odors, or other factors verified by the Community Improvement Coordinator, at the Discretion of the Community Development Director, this CUP may be submitted to the Planning Commission for subsequent review at a public hearing.
- PL8. Animal and veterinary clinic medical waste shall be disposed in compliance with the applicable California State and Los Angeles County health regulations. All other non-veterinary clinic/animal waste shall be disposed in the refuse containers located in the service area behind the clinic.

General Conditions

- PL9. Approval by the Planning Commission does not constitute a building permit or authorization to begin construction. An appropriate permit issued by the South Pasadena Building Division shall be obtained prior to construction, enlargement, relocation, conversion or demolition of any building or structure on any of the Subject Property.
- PL10. This approval and all rights hereunder shall terminate within twelve (12) months of the effective date of their approval by the Planning Commission unless otherwise conditioned and/or unless action is taken to secure Building & Safety Permits and maintain active Building Permits with the Building Division beginning with the submittal of the plans for Plan Check Review.
- PL11. All other requirements of any law, ordinance, or regulation of the State of California, City of South Pasadena, and any other government entity shall be complied with.
- PL12. Compliance with, and execution of, all conditions listed herein shall be necessary prior to obtaining any occupancy inspection clearance and/or prior to obtaining any occupancy clearance.
- PL13. The applicant shall demonstrate that proposed building materials, and other architectural/site features, are rated for appropriate durability and longevity. Final plans shall incorporate all changes as conditioned herein and shall recognize all easements or deed restrictions pertaining to the subject property. Any appreciable modification shall require the prior approval of the Planning Division or, if applicable, the Planning Commission.
- PL14. The applicant and each successor in interest to the property which is the subject of this project approval, shall defend, indemnify and hold harmless the City of South Pasadena and its agents, officers and employees from any claim, action or proceeding against the City or its agents, officers or employees to attack, set aside, void or annul any approval of the City, City Council or Planning Commission concerning this approval. In the event of any claim or lawsuit, the applicant and/or successor shall submit a deposit in such amount as the City reasonably determines necessary to protect the City from exposure to fees, costs or liability with respect to such claim or lawsuit.
- PL15. The applicant shall pay for all applicable City development impact fees, and all other public agency fees including plan review and permit fees.
- PL16. The construction site and the surrounding area shall be kept free of all loose materials resembling trash and debris in excess of that material used for immediate construction purposes. Such excess may include, but is not limited to, the accumulation of debris, garbage, lumber, scrap metal, concrete, asphalt, piles of earth, salvage materials, abandoned or discarded furniture, appliances, and/or other household fixtures.
- PL17. The hours of construction shall be limited to 8:00 am and 7:00 pm Monday through

Friday, 9:00 am and 7:00 pm Saturday, and on Sunday from 10:00 am to 6:00 pm.

- PL18. During construction, the clearing, grading, earth moving, or excavation operations that cause excessive fugitive dust emissions shall be controlled by regular water or other dust preventive measures using the fooling procedures:
- A. The area disturbed by clearing, grading, earth moving, or excavation operations shall be minimized to the maximum extent feasible to prevent fugitive dust;
 - B. All material excavated or graded shall be sufficiently watered to prevent dust from leaving the construction area and to create a “crust” after each day’s activities cease. Watering shall occur at least twice daily with complete coverage, preferably in the late morning and after work is done for the day;
 - C. All material transported on-site shall be either sufficiently watered or securely covered to prevent fugitive dust;
 - D. All areas of vehicle movement shall be watered appropriately to prevent dust from leaving the construction site;
 - E. Soil stockpiles for more than two days shall be covered, kept moist, or treated with soil binders to prevent dust generation;
 - F. Gravel pads shall be installed at all access points to prevent tracking of mud onto public roads; and
 - G. Visible dust beyond the property line emanating from the project shall be prevented to the maximum extent possible.
- PL19. All on-site landscaping, including the front yard and parkway areas, which are damaged during construction shall be revitalized upon completion of construction, as necessary, prior to final building permits. All landscaping, including the parkway, shall be maintained in a healthy, green, pruned, growing condition.
- PL20. A Tree Removal Permit shall be issued by Public Works for the removal of one (1) tree. All requirements and conditions of the permit shall be met prior to initiating grading and shall continue in effect as described in the permit or until the project is complete.
- PL21. A construction sign with the contact information for the contractor shall be posted on-site during construction. The applicant shall provide photographs to the Planning, Building, and Public Works Departments, illustrating that proper construction fencing is installed and signs describing construction and noise disturbance coordinator contact information are posted at the construction site.
- PL22. The construction plan shall show that all lighting on the site will be directed downward and shielded to prevent off-lighting on adjacent properties.

BUILDING & SAFETY DIVISION

- BD1. The second sheet of building and grading plans is to list all conditions of approval and to include a copy of the Planning Commission Decision letter. This information shall be incorporated into the plans prior to the first submittal for plan check.
- BD2. Fees shall be paid to the County of Los Angeles Sanitation District prior to issuance of the building permit.
- BD3. Park Impact Fee to be paid at the time of permit issuance.
- BD4. Per Chapter 16A of the City of South Pasadena Municipal Code, the Growth fee shall be paid at the time of permit issuance.
- BD5. In accordance with paragraph 5538(b) of the California Business and Professions Code, plans are to be prepared and stamped by a licensed architect.
- BD6. Structural calculations prepared under the direction of an architect, civil engineer, or structural engineer, shall be provided.
- BD7. A geotechnical and soils investigation report is required. The duties of the soils engineer of record, as indicated on the first sheet of the approved plans, shall include the following:
- A. Observation of cleared areas and benches prepared to receive fill;
 - B. Observation of the removal of all unsuitable soils and other materials;
 - C. The approval of soils to be used as fill material;
 - D. Inspection of compaction and placement of fill;
 - E. The testing of compacted fills; and
 - F. The inspection of review of drainage devices.
- BD8. The geotechnical and soils engineer shall review and approve the project grading and foundation plans to show compliance that their recommendations have been properly implemented.
- BD9. The owner shall retain the soils engineer preparing the Preliminary Soils and/or Geotechnical Investigation accepted by the City for observation of all grading, site preparation, and compaction testing. Observation and testing shall not be performed by another soils and/or geotechnical engineer unless the subsequent soils and/or geotechnical engineer submits and has accepted by Building Division, a new Preliminary Soils and/or Geotechnical Investigation.

- BD10. At the time of plan submittal, the PDF copy of the soils report shall be provided by the applicant.
- BD11. A drainage plan shall be approved prior to issuance of the building permit. The drainage plan shall indicate how all storm drainage is carried to the public way or drainage structure approved to receive storm water.
- BD12. Stormwater Planning Program LID Plan Checklist (MS4-1 Form) completed by Engineer of Record shall be copied on the first sheet of Grading Plans. The form can be found at the following link:
<https://www.dropbox.com/s/5p4yf08beipzyot/SP%20MS4-1%20LID%20Determination%20Form.pdf?dl=0>
- BD13. All State of California disability access regulations for accessibility shall be complied with.
- BD14. Foundation inspection will not be made until setback on the east side of the addition has been surveyed and the setbacks determined to be in accordance with the approved plans by a land surveyor licensed by the State of California. THIS NOTE IS TO BE PLACED ON THE FOUNDATION PLAN IN A PROMINENT LOCATION.
- BD15. Electrical plan check is required.
- BD16. Mechanical plan check is required.
- BD17. Plumbing plan check is required.
- BD18. Plumbing fixtures shall be provided as required by the Chapter 4 of the California Plumbing Code. Additional fixtures may be required if not in compliance.
- BD19. Project shall comply with the CalGreen Non-Residential mandatory requirements.
- BD20. When the common path of egress travel distance from the 2nd story office exceeds 75 feet as a result of the expansion, the second exit or exit access doorway is required per Table 1006.3.3(2) of the California Building Code. The common path of travel distance shall include the distance of exit access stairways as well as that of the path passing through the work area at the 1st floor until the second exit is available.
- BD21. If the second exit is required, the required exits shall be arranged in accordance with Section 1007.1.1. Should the second exit be designed to pass through new isolation area, this area shall not be considered as intervening room per Section 1016.2. In addition, the rear landscaped area shall be designed to provide a clear path of egress travel for exit discharge to public way per Section 1028.
- BD22. The path of egress travel distance from the most remote point of 2nd floor office to the required exit, which includes the distance of exit access stairways per Section 1017.3.1, shall not exceed 200 feet per Table 1017.2.

BD23. When required by Fire Department, all fire sprinkler hangers must be designed, and their location approved by an engineer or an architect. Calculations must be provided indicating that the hangers are designed to carry the tributary weight of the water filled pipe plus a 250-pound point load. A plan indicating this information must be stamped by the engineer or the architect and submitted for approval prior to issuance of the building permit. A separate approval is required for Fire Sprinklers.

PUBLIC WORKS DEPARTMENT

- PW1. The applicant shall obtain City approval for any modifications or revisions to the approval of this project. Deviations not identified on the plans may not be approved by the City, potentially resulting in the need for the project to be redesigned.
- PW2. The applicant shall pay all applicable City and Los Angeles County fees, including Public Works Department plan review fee of \$515 for 2 reviews and an additional fee of \$153 for each additional review and permit fees per the current adopted Master Fee Schedule which can be found on the City's website. The applicant shall provide receipts of all applicable fees paid prior to submitting plans for review.
- PW3. Huntington Drive and Garfield Avenue shall be videotaped before the start of construction and after construction for assessing the damage caused to the street by construction related traffic. The applicant will be responsible to restore the road to its original condition before the start of construction. These video tapes shall be submitted to the City before the start of the project and immediately upon completion of the project.
- PW4. Provide a 24-hour emergency contact number for the applicant and contact information of all utility agencies involved/impacted/potentially impacted by this project on the title sheet of the plans.
- PW5. Prior to issuance of a permit, the applicant shall perform a video inspection of the existing sewer lateral for obstructions and remove any obstructions observed. Provide a copy of the inspection video of the cleared pipe for review.
- PW6. The applicant shall pay all applicable City sewer and/or water capacity charges per SPMC Section 16B.3.
- PW7. Show the location of all existing utilities on adjacent street(s), as well as location and size of all existing or proposed services serving the property. Show all utility points of connection (POC).
- PW8. Show the location and area of trench sections for any proposed sewer and water lines connection within the public right-of-way. Provide a trench restoration detail per City standards if any new utility connections are proposed.
- PW9. The applicant shall contact the City of South Pasadena Water Division to verify the existing water meter connection is adequate for the proposed structure. Coordinate the

size, location, and associated fee for a new water meter connection, as applicable. Please contact the Water Operations Manager at (626) 460-6393 for additional information.

- PW10. Provide clearance letter from utility companies for any proposed relocation of utility lines that encroach on the properties prior to obtaining permits for the project, if applicable.
- PW11. If applicable, improvement plans for underground utilities (i.e. water, sewer, electrical, telecommunications, etc.) to be placed in the public right-of-way or easement that will be owned and maintained by other entities shall be reviewed by the City prior to Utility Agency approval. The City shall have a place on the title sheet to accept the plans with a statement: "The City's acceptance is limited to the placement of utilities relative to public infrastructure clearances, uses, and future plans within the right-of-way."
- PW12. Show the existing grade, location, and dimensions of all existing and proposed conditions within public right-of-way including, but not limited to: curb and gutter, sidewalk, driveway, traffic striping, signage, utilities, storm drain facilities, trees, and other features.
- PW13. The applicant shall replace all broken, damaged, or out-of-grade curb and gutter, sidewalk, and driveway and repaint all curb markings fronting the property on Huntington Drive and Garfield Avenue to the satisfaction of the City Engineer. In addition, existing sidewalk and driveway approaches that are below current city standards shall be replaced regardless of when or how such condition originally occurred per SPMC Section 31.54. The driveway approaches shall conform to SPPWC Standard Plan 110-2 Type A. All improvements within the public right-of-way shall conform to the current editions of the Standard Specifications for Public Works Construction (SSPWC) and Standard Plans for Public Works Construction (SPPWC).
- PW14. If applicable, all flood control plans to be reviewed by the City or the Los Angeles County Flood Control District shall be submitted through the City of South Pasadena, unless otherwise directed by the City Engineer. For projects requiring LACFCD review, the developer shall pay the appropriate fees to LACFCD.
- PW15. Depending on the extent of the damage caused on the public right-of-way as a result of the construction activity, the applicant may need to grind and repave the existing asphalt street fronting the property from the face of curb to the centerline of the street of the street to a minimum depth of 1.5". Asphalt shall be C2 PG 64-10 and shall conform to the current edition of the Standard Specifications for Public Works Construction (SSPWC). Any pre-existing pavement markings and traffic striping shall be restored in accordance to the latest editions of the California Manual on Uniform Traffic Control Devices (CA MUTCD), Caltrans standards, and to the satisfaction of the City Engineer. The applicant shall verify the actual limits of paving with the Public Works Department depending on the condition of the existing pavement adjacent to the property. All manholes and/or utility covers shall be adjusted within the limits of paving shall be adjusted to grade after paving has been completed.

- PW16. Show all existing and proposed trees, including size and species, and indicate their disposition. If any trees (12" in diameter or greater and/or native trees) are to be removed, apply for a tree removal permit with the Public Works Department per City Ordinance No. 2328 amending Section 34.10 of SPMC. See SPMC Section 34.12 for the required information and process for the trees that are proposed to be removed and/or impacted during construction. Replacement trees shall be planted per SPMC Section 34.12-5. Two replacement trees shall be planted per SPMC Section 34.12-5, and paying a deposit of in the amount of \$670. If existing trees are to remain on site, the applicant shall note on the plans methods of protecting existing trees during construction.
- PW17. The proposed building structure shall not be constructed within critical root zone area of any trees. For native and protected species, use the tree trunk's diameter measured at breast height (DBH) (X5) as the minimum critical root mass. For non-native and protected species, use the tree's DBH (X3) as the minimum critical root mass.
- PW18. No overnight storage of materials or equipment within the public right-of-way shall be permitted.
- PW19. Temporary bins (low boy), if used, shall be "roll off" style to be provided by Athens Services. Athens Services has an exclusive agreement with the City for the provision of trash removal services: only Athens dumpsters can be used. Any dumpsters placed on the roadway shall require a protective barrier underneath (such as plywood) to protect the pavement. The applicant shall obtain dumpster permit from the Public Works Department.
- PW20. The applicant shall obtain oversize/overload permits from the Public Works Department for any oversized equipment used during the stages of construction, including, but not limited to: demolition; clearing and grubbing; grading; material disposal; drilling for piles and/or caissons; trenching for footings; excavation for retaining walls; core sampling of soils; etc.
- PW21. The applicant shall be responsible for posting a project sign at the entrance to the project site displaying the City's construction hours per SPMC Section 19A.13. The project sign shall be 24" x 36" and made of durable weather-resistant material. The applicant shall provide a 24- hour emergency contact number for the designated contact who will be responsible for maintaining the public right-of-way during the all stages of construction until the project is complete.
- PW22. The applicant shall obtain an encroachment permit from the Public Works Department for any work proposed within the public right-of-way.

FIRE DEPARTMENT

- FD1. Required Code References: 2019 South Pasadena Municipal Code (SPMC); 2019 California Fire Code (CFC); 2019 California Building Code and NFPA standards.

- FD2. Means of egress in existing buildings shall comply with the minimum egress requirements where specified in table 1103.1 as further enumerated in Sections 1104.2 through 1104.25, and the building code that applied at the time of construction.
- FD3. Address Identification. New and existing buildings shall have *approved* address numbers, building numbers or *approved* building identification placed in a position that is plainly legible and visible from the street or road fronting the property. These numbers shall contrast with their background. Where required by the fire code official, address numbers shall be provided in additional approved locations to facilitate emergency response. Address numbers shall be Arabic numbers or alphabetical letters. Numbers shall be a minimum of 4 inches (101.6 mm) high with a minimum stroke width of 0.5 inch (12.7 mm). Where access is by means of a private road and the building cannot be viewed from the public way, a monument, pole, or other sign or means shall be used to identify the structure. Address numbers shall be maintained.
- FD4. Buildings under construction shall meet the condition of “Chapter 33 Fire Safety During Construction and Demolition” of the 2019 California Fire Code. Structures under construction, alteration, or demolition, shall be provided with no less than one 2A10BC fire extinguisher as follows:
- 1) At each stairway on all floor levels where combustibles materials have accumulated;
 - 2) In every storage and construction shed; and
 - 3) Where special hazards exist included, but not limited to, storage and use of combustible and flammable liquids.
- FD5. A set of plans must remain on the job site at all times. Appointments for inspectors should be made at least two days in advance of required inspection by calling the Fire Department at 626.403.7304.
- FD6. The City of South Pasadena Fire Department reserves the right to change or otherwise modify requirements based upon receiving additional project information or other unforeseen circumstances.
- FD7. Please contact South Pasadena Water Department or Public Works for any water related inquires at 626.403.7240.

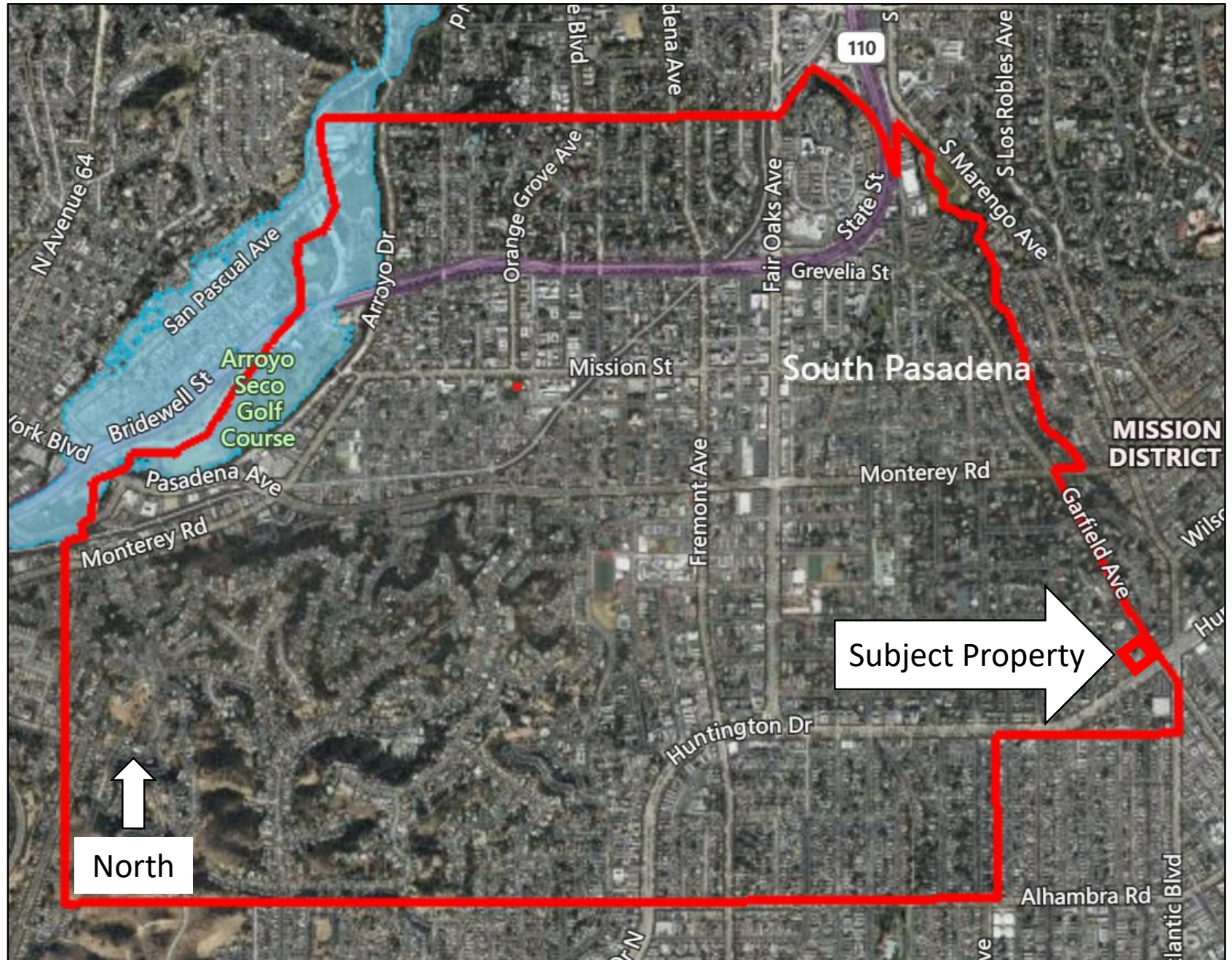
ATTACHMENT 2

Vicinity Map

Vicinity Map

2140 Huntington Drive
APN 5321-015-020

Project No. 2456 CUP-DRX



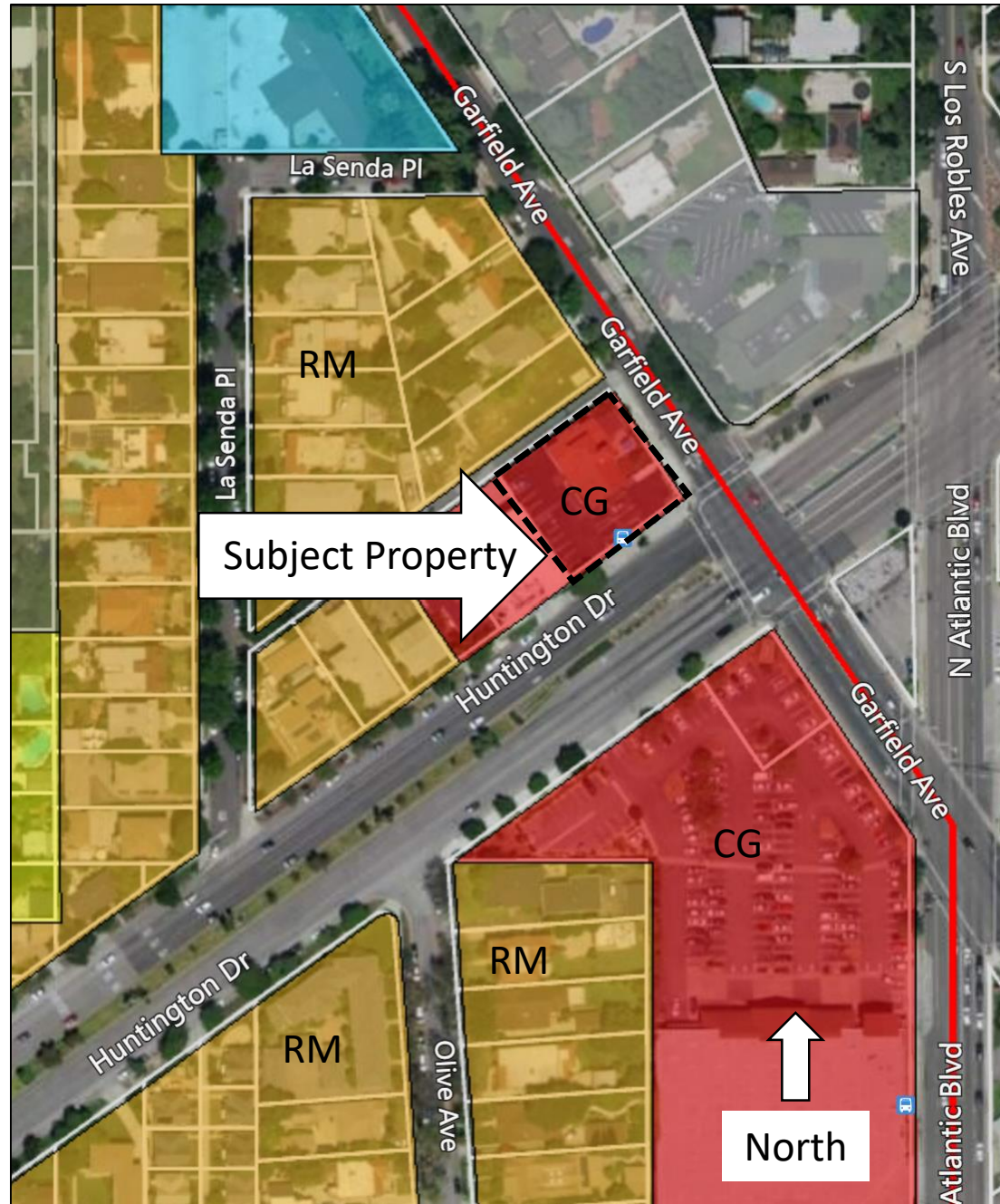
ATTACHMENT 3

Zoning Map

Zoning Map

2140 Huntington Drive
APN 5321-015-020

Project No. 2456 CUP-DRX



ATTACHMENT 4

Project Plans

Vet Villa Animal Hospital

VETERINARY OFFICE EXPANSION PROJECT

ABBREVIATIONS

AB Anchor Bolt
AFF Above Finished Floor
AGG Aggregate
ALT. Alternative
ALUM Aluminum
ANOD Anodized
ARCH Architect
ASPH Asphalt

BD Board
BIT Bitumen
BLDG Building
BLK Block
BLKG Blocking
BM Beam
B M Benchmark
BOT Bottom
BTWN Between
BUR Built-up Roofing

CHAN Channel
CI Cast Iron
CLG Ceiling
CMU Concrete Masonry Unit
COL Column
CONST Construction
CONT Continuous
CONTR Contractor
CPT Carpet
CS Countersunk
CT Ceramic Tile

DBL Double
DET Detail
DIFF Diffuser
DIM Dimension
DN Down
DPM Damp-Proof Membrane
DR Door
DS Downspout
DWG Drawing

E Existing
EA Each
ELEV Elevation
ELEC Electrical
EMER Emergency
EML Expanded Metal Lath
EQ Equal
EQUIP Equipment
EXP Expansion
EXT Exterior

FAI Fresh Air Inlet
FD Floor Drain
FDN Foundation
FIN Finish(ed)
FLR Floor
FLUOR Fluorescent
FO Finished opening
FOF Face of Finish
FOS Face of Stud
FTG Footing
FR Fire Resistant

GA Gauge
GALV Galvanized
GC General Contractor
GFR Glass Fiber Reinforced
Concrete
GL Glass
GMMU Glass Mesh Mortar Unit
GYF Gypsum

HB Hose Bib
HC Hollow Core
HDPC Handicapped
HDIR Hardware
HM Hollow Metal
HORIZ Horizontal
HR Hour
HT Height
HTG Heating
HVAC Heating Vent & Air
Conditioning

ID Inside Diameter
INSUL Insulation
INT Interior
INV Invert

JAN Janitor
JST Joist
JNT Joint

LAM Laminated
LAV Lavatory
LHS Left Hand Side
LL Live Load
LTWT Lightweight

MATL Material
MAX Maximum
MECH Mechanical
MANUF Manufacturer
MDF Medium Density
Fiberboard
MH Manhole
MIN Minimum
MISC Miscellaneous
MTL Metal

N North
NIC Not in Contract
NOM Nominal
NTS Not To Scale
NR Non-rated

OC On Center
OD Overflow Drain/ Outside
Diameter
OPNG Opening
OPP Opposite
OSB Oriented Strand Board
OVHD Overhead

PC Precast Concrete
PL Plate
P LAM Plastic Laminated

PLYWD Plywood
PNL Panel
PR Pair
PSF Pounds per Square Foot
Concrete
PSI Pounds per Square Inch
PT Post Tensioned

QT Quarry Tile
QTY Quantity

R Radius
RA Return Air
RD Roof Drain
RE: Refer/ Reference
REFL Reflected
REINF Reinforcing
REQ'D Required
RET Retaining
REV Revision
RF Roof
RHS Right Hand Side
RM Room
RO Rough Opening

SB Splash Block
SC Solid Core
SD Storm Drain
SECT Section
SIM Similar
SHTG Sheathing
SPECS Specifications
SS Stainless Steel
STD Standard
STL Steel
STRUC Structural
SUSP Suspended

T & G Tongue and Groove
TEL Telephone
TEMP Tempered
TJ Tooled Joint
TOC Top of Concrete
TOM Top of Masonry
TOS Top of Steel
TOW Top of Wall
T-STAT Thermostat
TRANSF Transformer
TYP Typical

U/S Underside
VT Vinyl Tile
VB Vapor Barrier
VERT Vertical
VIF Veerly In Field
VWC Vinyl Wall Covering

W/ With
W/O Without
WC Water Closet
WD Wood
WH Water Heater
WP Waterproofing
WWF Welded Wire Fabric

GENERAL NOTES

1. ALL WORK SHALL BE PER STATE, FEDERAL & LOCAL CODES, ORDINANCES, ETC., INCL. THE 2019 EDITION OF THE CALIFORNIA BUILDING CODE (TITLE 24) WHICH ADOPTS THE 2019 CBC, CMIC, AND CFC AND THE 2019 CEC.

1a. ALL WORK SHALL BE PER 2016 CALIFORNIA ENERGY EFFICIENCY STANDARDS AS ADOPTED BY THE CITY.

2. EACH SUB-CONTRACTOR SHALL VISIT THE SITE, VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS, NOTIFY THE ARCHITECT OF ANY DISCREPANCIES OR INCONSISTENCIES FOUND, DO NOT SCALE DRAWINGS.

3. CONSTRUCTION, DEMOLITION AND REMODEL SHALL COMPLY WITH C.F.C. SECTION 203. CONTRACTOR SHALL PATCH & REPAIR NEIGHBORING FINISHES IF DAMAGED DURING DEMOLITION OR CONSTRUCTION.

4. THE CONTRACTOR SHALL DISPOSE OF ALL DEMOLITION AND CONSTRUCTION DEBRIS OFF SITE AND IN AN APPROVED MANNER, ACCORDING TO STATE AND CITY CODES.

5. ALL EXIT DOORS SHALL BE OPENABLE FROM THE INSIDE WITHOUT THE USE OF A KEY OR ANY SPECIAL KNOWLEDGE OR EFFORT, NO DEADBOLTS, SLIDING BOLTS, ETC. PROVIDE SELF RELEASING LOCKING DEVICES ON EXIT DOORS. EXIT DOORS SHALL SWING OPEN IN DIRECTION OF EXIT TRAVEL.

6. WHERE NO CONSTRUCTION DETAILS ARE SHOWN OR NOTED FOR ANY PART OF THE WORK TO BE DONE, DETAILS SHALL BE SAME AS FOR OTHER SIMILAR WORK.

7. FIRE EXTINGUISHERS SHALL BE PROVIDED PER C.F.C. SECTION 906. EXTINGUISHERS TO BE TYPE 2A10BC WITH CURRENT STATE FIRE MARSHALL SERVICE TAG, MOUNTED IN VISIBLE LOCATIONS, NOT MORE THAN 75' WALKING DISTANCE FROM ANY POINT IN THE SPACE. UNITS TO BE INSTALLED IN SEMI-RECESSED WALL CABINETS.

8. ALL POWER ACTUATED PINS/ANCHORS SHALL BE "RAMSET" #2330 AT 32" O.C. FOR INTERIOR, NON-LOAD BEARING PARTITIONS PER ICC ESR-1799.

9. WALL, FLOOR AND CEILING MATERIALS SHALL NOT EXCEED THE FLAME SPREAD CLASSIFICATIONS IN CBC TABLE 803.3.

10. IF REQUESTED BY INSPECTOR, CONTRACTOR TO POST OCCUPANT LOAD SIGN COMPLYING WITH C.F.C. SEC. 1003 AND C.B.C. SEC. 1004.3. POST IN WAITING AREA.

12. ENSURE ALL EXIT AISLEWAYS AND CORRIDORS ARE MAINTAINED AT 36" MINIMUM CLEAR WIDTH IN OFFICE AREA.

14. ENSURE ALL FINISHES ARE MAINTAINED IN FIRE RETARDANT CONDITION, ALL NEW FINISHES SHALL BE PER CHAPTER 8 (TABLE 803.9) OF THE CBC, CLASS III, FLAME SPREAD INDEX 200 MAX. (INCLUDES CARPETS, ETC.)

16. IF REQUESTED A STATEMENT IN WRITING SHALL BE GIVEN TO THE BUILDING OFFICIAL, STATING THAT THE ARCHITECT / ENGINEER SITE OBSERVATION AND SITE VISITS HAVE BEEN MADE AND WHETHER OR NOT ANY OBSERVED DEFICIENCIES HAVE BEEN CORRECTED TO CONFORM TO THE APPROVED PLANS AND SPECIFICATIONS.

17. GENERAL CONTRACTOR SHALL OBTAIN AND PAY FOR ALL REQUIRED PERMITS.

18. PROTECT IN PLACE ALL EXISTING FINISHES WHICH ARE TO REMAIN, REPAIR OR REPLACE ANY SUCH FINISHES DAMAGED DURING CONSTRUCTION.

19. REFER TO SPECIFICATIONS ON SHEET A-5 FOR ADDITIONAL STANDARDS AND REQUIREMENTS.

20. NO HAZARDOUS MATERIALS WILL BE STORED ON SITE OR EXCEED QUANTITIES LISTED IN CBC TABLE 307.1(1) AND 307.1(2).

21. EXIT SIGNS SHALL BE READILY VISIBLE FROM ANY DIRECTION OF APPROACH AND SHALL CLEARLY INDICATE THE DIRECTION OF TRAVEL. NO POINT SHALL BE MORE THAN 100 FEET (30480 MM) FROM THE NEAREST VISIBLE SIGN. ALL SIGNS SHALL BE ILLUMINATED AND CONNECTED TO AN EMERGENCY ELECTRICAL SYSTEM. (CBC 1013)

22. CONSTRUCTION SHALL BE SUPERVISED AS REQUIRED BY TENANT/LANDLORD LEASE REQUIREMENTS, WHERE REQUIRED BY THESE DOCUMENTS, PROVIDE CONTINUOUS INSPECTION OF THE WORK. THE DUTIES OF THE INSPECTOR ARE DEFINED IN SECTION 4-342 PART 1, TITLE 24, CCR: CLASS 3.

FIRE DEPT. NOTES

1. BUILDINGS UNDERGOING CONSTRUCTION, ALTERATION OR DEMOLITION SHALL BE IN ACCORDANCE WITH CFC ARTICLE 87 (CFC 8701) AND CFC CHAPTER 33, WELDING, CUTTING, AND OTHER HOT WORK SHALL BE IN CONFORMANCE WITH CHC CAPTER 35.

2. AT LEAST ONE FIRE EXTINGUISHER WITH A MINIMUM RATING OF 2-A-10B-C SHALL BE PROVIDED WITHIN 75 FEET MAXIMUM TRAVEL DISTANCE FOR EACH 6,000 SQUARE FEET OR PORTION THEREOF ON EACH FLOOR. (CFC 1002, CFC STANDARD 10-1, CAL. CODE REGS., III, 19, 3.29)

3. IF INSTALLED, COMPLETE PLANS AND SPECIFICATIONS FOR FIRE ALARM SYSTEMS AND OTHER FIRE-PROTECTION SYSTEMS AND APPURTENANCES THERETO SHALL BE SUBMITTED TO FIRE AND LIFE SAFETY FOR REVIEW AND APPROVAL PRIOR TO INSTALLATION. (CFC 1001.3)

4. DURING CONSTRUCTION, AT LEAST ONE EXTINGUISHER SHALL BE PROVIDED ON EACH FLOOR LEVEL AT EACH STAIRWAY, IN ALL STORAGE AND CONSTRUCTION SHEDS, IN LOCATIONS WHERE FLAMMABLE OR COMBUSTIBLE LIQUIDS ARE STORED OR USED, AND WHERE OTHER SPECIAL HAZARDS ARE PRESENT PER CFC SECTION 3315.1.

5. ADDRESS IDENTIFICATION SHALL BE PROVIDED FOR ALL NEW AND EXISTING BUILDINGS IN A LOCATION THAT IS PLAINLY VISIBLE AND LEGIBLE FROM THE STREET OR ROAD FRONTING THE PROPERTY, WHERE ACCESS IS BY WAY OF A PRIVATE ROAD AND THE BUILDING ADDRESS CANNOT BE VIEWED FROM THE PUBLIC WAY, AN APPROVED SIGN OR MEANS SHALL BE USED TO IDENTIFY THE STRUCTURE. PREMISES IDENTIFICATION SHALL CONFORM TO CBC SECTION 501.2.

6. WALL, FLOOR AND CEILING FINISHES AND MATERIALS SHALL NOT EXCEED THE INTERIOR FINISH CLASSIFICATIONS IN CBC TABLE 803.9 AND SHALL MEET THE FLAME PROPAGATION PERFORMANCE CRITERIA OF THE CALIFORNIA CODE OF REGULATIONS, TITLE 19, DIVISION 1, DECORATIVE MATERIALS SHALL BE PROPERLY TREATED BY A PRODUCT OR PROCESS APPROVED BY THE STATE FIRE MARSHAL WITH APPROPRIATE DOCUMENTATION PROVIDED TO THE CITY OF SAN DIEGO.

7. DUMPSTERS AND TRASH CONTAINERS EXCEEDING 1.5 CUBIC YARDS SHALL NOT BE STORED IN BUILDINGS OR PLACED WITHIN 5 FEET OF COMBUSTIBLE WALLS, OPENINGS OR COMBUSTIBLE ROOF EAVE LINES UNLESS PROTECTED BY AN APPROVED SPRINKLER SYSTEM OR LOCATED IN A TYPE 1 OR 2A STRUCTURE SEPARATED BY 10 FEET FROM OTHER STRUCTURES. CONTAINERS LARGER THAN 1 CUBIC YARD SHALL BE OF NON- OR LIMITED-COMBUSTIBLE MATERIALS OR SIMILARLY PROTECTED OR SEPARATED. CFC 304.3.

8. EXITS, EXIT SIGNS, FIRE ALARM PANELS, HOSE CABINETS, FIRE EXTINGUISHER LOCATIONS, AND STANDPIPE CONNECTIONS SHALL NOT BE CONCEALED BY CURTAINS, MIRRORS, OR OTHER DECORATIVE MATERIAL.

9. OPEN FLAMES, FIRE, AND BURNING ON ALL PREMISES IS PROHIBITED EXCEPT AS SPECIFICALLY PERMITTED BY THE CITY OF SOUTH PASADENA AND CFC 308.

10. THE EGRESS PATH SHALL REMAIN FREE AND CLEAR OF ALL OBSTRUCTIONS AT ALL TIMES. NO STORAGE IS PERMITTED IN ANY EGRESS PATHS.

11. STORAGE, DISPENSING, OR USE OF HAZARDOUS MATERIALS SHALL COMPLY WITH CBC SECTION 414 AND CFC REGULATIONS. THE STORAGE AND USE OF HAZARDOUS MATERIALS SHALL BE REVIEWED AND APPROVED BY THE CITY OF SOUTH PASADENA PRIOR TO SUCH MATERIALS BEING BROUGHT ON SITE.

12. LOCATIONS AND CLASSIFICATIONS OF EXTINGUISHERS SHALL BE IN ACCORDANCE WITH CFC 906 AND CALIFORNIA CODE OF REGULATIONS (CCR), TITLE 19.

CITY NOTES

1. SEPARATE PERMIT SHALL FIRST BE OBTAINED FROM THE CITY PUBLIC WORKS DEPARTMENT PRIOR TO PLACEMENT OF ANY CONSTRUCTION MATERIALS OR EQUIPMENT IN THE PUBLIC WAY.

2. AT TIME OF PERMIT ISSUANCE, CONTRACTOR SHALL SHOW THEIR VALID WORKERS' COMPENSATION INSURANCE CERTIFICATE.

3. ALL WORK SHALL CONFORM TO ALL REQUIREMENTS OF STATE OF CALIFORNIA TITLE 24 REGARDLESS OF THE INFORMATION INDICATED IN THESE PLANS. IT IS THE RESPONSIBILITY OF THE INDIVIDUAL SUPERVISING THE CONSTRUCTION TO ENSURE THAT THE WORK IS DONE IN ACCORDANCE WITH CODE REQUIREMENTS PRIOR TO REQUESTING INSPECTION.

4. EXCESS WASTE CONCRETE MAY NOT BE WASHED INTO THE PUBLIC WAY OR ANY OTHER DRAINAGE SYSTEM. PROVISIONS SHALL BE MADE TO RETAIN CONCRETE WASTES ON SITE UNTIL THEY CAN BE DISPOSED OF AS SOLID WASTE.

5. SOUTH COAST AIR QUALITY MANAGEMENT DISTRICT (SCAQMD) SHALL BE NOTIFIED IN ACCORDANCE WITH CALIFORNIA STATE LAW PRIOR TO THE START OF ANY DEMOLITION, ADDITION, AND/OR REMODEL WORK. THE SCAQMD IS LOCATED AT 21865 COPLEY DRIVE, DIAMOND BAR CA. PHONE: 909-396-2000. BE ADVISED, SCAQMD MAY REQUIRE A 10 DAY WAIT PERIOD TO START OF WORK.

6. SEDIMENTS AND OTHER MATERIALS MAY NOT BE TRACKED FROM THE SITE BY VEHICLE TRAFFIC. THE CONSTRUCTION ENTRANCE ROADWAYS MUST BE STABILIZED SO AS TO INHIBIT SEDIMENTS FROM BEING DEPOSITED INTO THE PUBLIC WAY. ACCIDENTAL DEPOSITIONS MUST BE SWEEP UP IMMEDIATELY AND MAY NOT BE WASHED DOWN BY RAIN OR OTHER MEANS.

STOCKPILES OF EARTH AND OTHER CONSTRUCTION RELATED MATERIALS MUST BE PROTECTED FROM BEING TRANSPORTED FROM THE SITE BY THE FORCES OF WIND OR WATER. TRASH & CONSTRUCTION RELATED SOLID WASTES MUST BE DEPOSITED INTO A COVERED RECEPTACLE TO PREVENT CONTAMINATION OF RAINWATER AND DISPERSAL BY WIND.

FUELS, OILS, SOLVENTS AND OTHER TOXIC MATERIALS MUST BE STORED IN ACCORDANCE WITH THEIR LISTING AND ARE NOT TO CONTAMINATE THE SOIL AND SURFACE WATERS. ALL APPROVED STORAGE CONTAINERS ARE TO BE PROTECTED FROM WEATHER. SPILLS MUST BE CLEANED UP IMMEDIATELY AND DISPOSED OF IN A PROPER MANNER. SPILLS MAY NOT BE WASHED INTO THE DRAINAGE SYSTEM.

7. THE ISSUANCE OF A PERMIT SHALL NOT PREVENT THE BUILDING OFFICIAL FROM REQUIRING THE CORRECTION OF ERRORS ON THESE PLANS OR FROM PREVENTING ANY VIOLATION OF THE CODES ADOPTED BY THE CITY, RELEVANT LAWS, ORDINANCES, RULES AND/OR REGULATIONS.

8. SEPARATE ADDRESSES WILL BE PROVIDED FOR THIS BUILDING AND ARE CURRENTLY IN PROCESS AT THE CITY.

DEFERRED ITEMS

1. NONE

IT IS UNDERSTOOD THAT PLANS FOR THE PROJECT HAVE, AT THIS TIME, BEEN REVIEWED FOR COMPLIANCE WITH ALL APPLICABLE STATE AND CITY REGULATIONS, AND THAT THE PROJECT AS A WHOLE HAS BEEN APPROVED BY THE CITY, WITH THE EXCEPTION OF THE DEFERRED ITEMS LISTED. I/WE UNDERSTAND THAT I/WE WILL NOT BE AUTHORIZED ANY INSPECTION OF THE DEFERRED ITEMS PROPOSED PRIOR TO THE SUBMITTAL AND APPROVAL OF PLANS AND/OR CALCULATIONS FOR THOSE DEFERRED ITEMS.

PROJECT DIRECTORY

OWNER/TENANT:
DR. REBECCA SWIMMER
4411 LOS FELIZ BLVD., #607
LOS ANGELES, CA 90027
(949) 721 0730

ARCHITECT:
SAUNDERS + WIAAT ARCHITECTS, INC.
2700 WEST COAST HIGHWAY, #200,
NEWPORT BEACH, CA. 92663
(949) 721 0730

STRUCTURAL/MECHANICAL/ELECTRICAL/PLUMBING:
PLUMP ENGINEERING, INC.
914 E. KATELLA AVE,
ANAHEIM, CA 92805
(714) 385 1835

SCOPE OF WORK

A NEW VETERINARY OFFICE EXPANSION PROJECT IN AN EXISTING OFFICE BUILDING. **PROJECT WORK IS LIMITED TO EXPANSION ONLY.** THERE IS NO EXISTING DEVELOPMENT NUMBER ASSOCIATED WITH THIS PROJECT.

- THE PROJECT SHALL INCLUDE THE FOLLOWING:
1. NEW BEARING WALLS/ROOF/MODIFICATION TO EXIST. ROOF
 2. NEW INTERIOR NON-LOAD BEARING PARTITION WALLS
 3. NEW GYPSUM BOARD CEILING SYSTEM
 4. NEW FINISHES
 5. NEW PLUMBING FIXTURES AND PIPING
 6. HVAC MODIFICATIONS
 7. NEW ELECTRICAL & LIGHTING

CALGREEN:
PER 2019 CGBSC 301.3, BUILDING ALTERATIONS WITH A PERMIT VALUATION OF LESS THAN \$200,000 SHALL BE EXEMPT. PER 301.3.1, EXISTING NONCOMPLIANT PLUMBING FIXTURES SHALL BE BROUGHT INTO COMPLIANCE.

APPLICABLE CODES

2019 LOS ANGELES COUNTY BUILDING CODE
2019 BUILDING STANDARDS ADMINISTRATIVE CODE, PART 1, TITLE 24 C.C.R.
2019 CALIFORNIA BUILDING CODE (CBC), PART 2, TITLE 24 C.C.R.
2019 CALIFORNIA ELECTRICAL CODE (CEC), PART 3, TITLE 24 C.C.R.
2019 CALIFORNIA MECHANICAL CODE (CMC), PART 4, TITLE 24 C.C.R.
2019 CALIFORNIA PLUMBING CODE (CPC), PART 5, TITLE 24 C.C.R.
2019 CALIFORNIA FIRE CODE (CFC), PART 9, TITLE 24 C.C.R.
2019 CALIFORNIA GREEN BUILDING STANDARDS CODE (CGBSC), PART 11, TITLE 24 C.C.R.

ACCESSIBILITY NOTES (CBC ACCESSIBILITY STANDARDS SHALL TAKE PRECEDENCE)

GENERAL ACCESSIBILITY

1. ACCESSIBLE SIGNAGE SHALL BE PROVIDED AT ALL NEW TOILET FACILITIES AND EXITS. SEE SIGNAGE SCHEDULE. SIGNS TO HAVE RAISED LETTERS PLUS BRAILLE, AND ARE TO BE INSTALLED 48" MIN. TO 60" MAX. AFF. ADJACENT TO LATCH SIDE OF DOOR.

DOORS

2. ALL NEW DOORS SHALL COMPLY WITH CBC 1010 AND 11B-302. DOOR HARDWARE SHALL BE LEVER OR PUSH TYPE, MOUNTED 34" TO 48" ABOVE THE FLOOR, PER CBC SEC. 1010.1.9.2. ALL DOORS SHALL BE 3'-0" WIDE MIN.

3. MAXIMUM EFFORT TO OPERATE DOORS SHALL NOT EXCEED 5 POUNDS FOR EXTERIOR DOORS AND 5 POUNDS FOR INTERIOR DOORS. SUCH PUSH OR PULL EFFORT SHALL BE APPLIED AT RIGHT ANGLES TO HINGED DOORS. AT REQUIRED FIRE DOORS, MAX. OPERATING EFFORT MAY BE INCREASED TO 15 LBS.

4. PER CBC 11B-302 AND 11B-404.2.5, REGARDLESS OF THE OCCUPANT LOAD, THERE SHALL BE A LEVEL FLOOR OR LANDING ON EACH SIDE OF THE DOOR. THE FLOOR OR LANDING SHALL NOT BE MORE THAN 1/2" LOWER THAN THE THRESHOLD OR THE DOORWAY.

5. THE LOWER 10" OF ALL DOORS SHALL BE SMOOTH AND UNINTERRUPTED TO ALLOW THE DOOR TO BE OPENED BY A WHEELCHAIR FOOTREST WITHOUT CREATING A TRAP OR HAZARDOUS CONDITION. NARROW FRAME DOORS MAY USE A 10" HIGH SMOOTH PANEL ON THE PUSH SIDE OF THE DOOR.

SANITARY FACILITIES

6. 2 EXISTING ACCESSIBLE RESTROOMS ARE PROVIDED IN THE SUITE:

7. WATER-CLOSET / FAUCET CONTROLS SHALL BE OPERABLE WITH ONE HAND AND DO NOT REQUIRE TIGHT GRASPING, PINCHING OR TWISTING OF THE WRIST.

8. LAVATORIES SHALL COMPLY WITH THE FOLLOWING:
a) A MINIMUM OF 30" x 48" CLEAR SPACE IS PROVIDED IN FRONT OF THE LAVATORY.
b) THE CLEAR SPACE BENEATH LAVATORIES IS A MINIMUM OF 29" HIGH x 30" WIDE x 8" DEEP AT THE TOP AND 9" HIGH x 30" WIDE x 17" DEEP AT THE BOTTOM. THE MAXIMUM HEIGHT OF THE BASIN / COUNTER TOP IS 34" HEIGHT MAX.
c) ALL HOT WATER AND DRAIN PIPES UNDER THE LAVATORY ARE INSULATED.
d) FAUCET CONTROLS HAVE AN OPERATING FORCE OF NOT GREATER THAN 5 POUNDS.

9. FLOORS ARE TO BE SLIP-RESISTANT.

10. GRAB BARS SHALL BE PROVIDED. FASTENERS AND MOUNTING SUPPORT MUST BE ABLE TO WITHSTAND 250 POUNDS POINT LOAD BEARING SHEAR AND TORSION.

11. DOORS TO THE TOILET ROOMS SHALL BE IDENTIFIED BY A 12" X 1/4" THICK SIGN WITH ETCHED UNISEX SYMBOL: 12" DIAMETER CIRCULAR WITH EQUALITERAL TRIANGLE VERTEX POINTING UP.

SITE ACCESS

1. THE BUILDING AND SITE ARE EXISTING AND SHALL BE MADE FULLY ACCESSIBLE

2. PRIMARY ENTRANCES TO THE BUILDING ARE ON AN ACCESSIBLE ROUTE. GROUND SURFACES ALONG THIS ROUTE, INCLUDING CURB RAMPS, ARE ON AN ACCESSIBLE ROUTE.

3. ACCESSIBLE PARKING SPACES, INCLUDING VAN SPACES ARE PROVIDED. LOCATED ON SITE AT THE PARKING LOT SERVING THIS BUILDING. ACCESSIBLE SPACES COMPLY IN REGARD TO QUANTITY, SPACE, STRIPING AND SIGNAGE. WITH EXISTING CURB RAMPS.

4. PROVIDE TEXT "MIN. FINE \$250.00" ON SIGNS AT STALLS (MIN. 1" HIGH LETTERS AND A PERMANENT PART OF THE SIGN). ENSURE SIGN MIN. 6'-8" A.F.F.

5. ACCESS AISLES SHALL BE PAINTED PER 11B-502.3.

6. STRIPING; PERIMETER SHALL BE BLUE COLOR (105090) PER FED. STD. 595B AND CROSSHATCHING TO BE WHITE.

7. DOORS ALONG PEDESTRIAN PATH OF TRAVEL ARE REQUIRED TO MEET ALL REQUIREMENTS FOR EXITS DOORS, AS FOLLOWS:
A. 36"-48" LEAF WIDTH
B. WHERE PANIC HARDWARE IS USED, MOUNT ABOVE 36" A.F.F. OR SHOW MIN. 32" CLEAR WIDTH WHEN GATE IS OPEN 90 DEGREES.
C. MAX. SLB. OPERATING PRESSURE
D. BOTTOM 10" OF DOORS TO BE SMOOTH FROM SIDE TO SIDE
E. LEVER OR ACCESSIBLE PULL FROM PULL SIDE OF DOOR
F. REQUIRED MANEUVERING CLEARANCES PER FIG. 11B-404.2.4.1
G. MIN. HEADROOM 6'-8"
H. NO CANE BOLTS OR PADLOCKS ON DOORS WITH PANIC HARDWARE

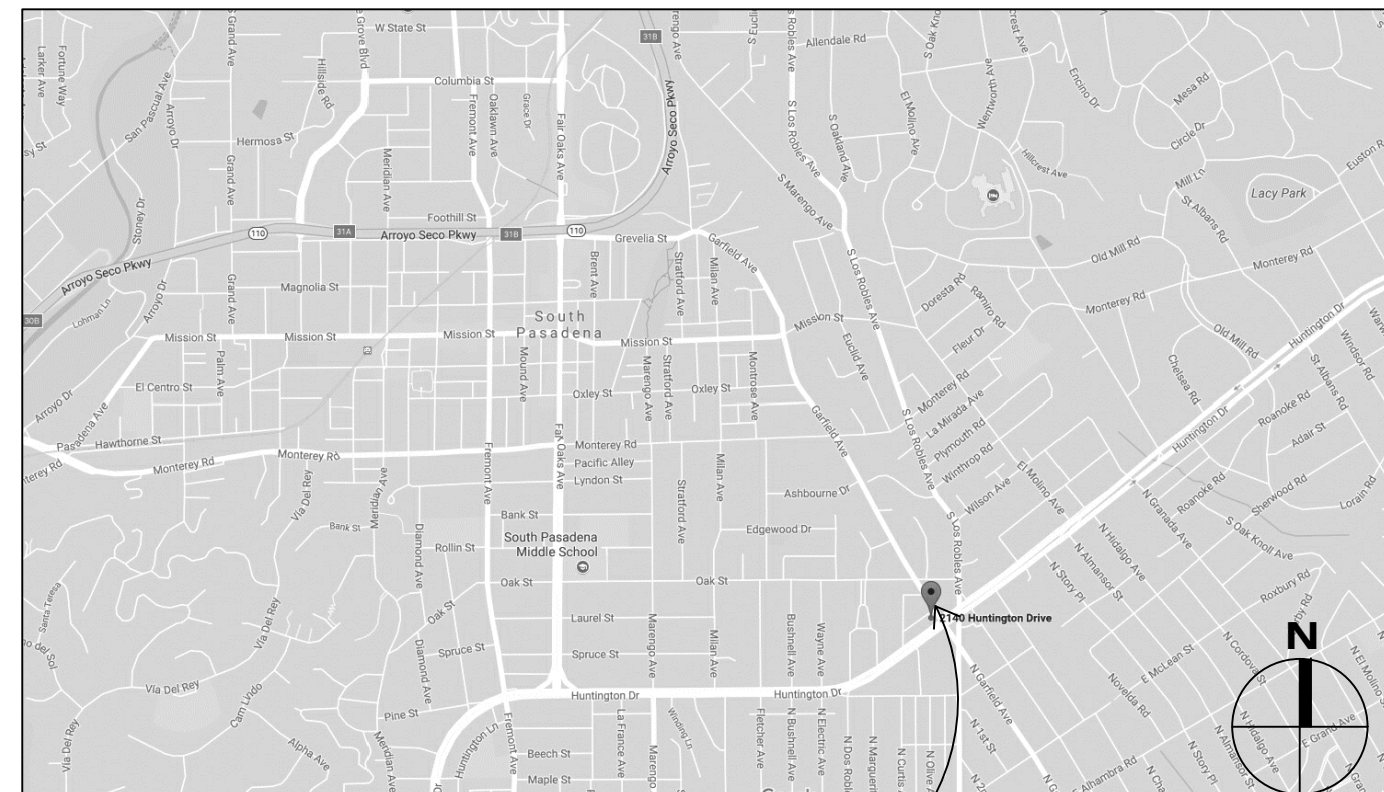
DEMOLITION NOTES

1. CARRY OUT ALL DEMOLITION WORK INDICATED IN ACCORDANCE WITH STATE AND LOCAL CODES.

2. REMOVE ALL DEBRIS FROM SITE AS WORK PROCEEDS AND DISPOSE OF AS PER CODE AND THE BUILDING MANAGEMENT REQUIREMENTS. INFORM ARCHITECT IMMEDIATELY IF ANY HAZARDOUS MATERIALS ARE ENCOUNTERED.

3. PROVIDE TEMPORARY BARRICADES AND PROTECT EXISTING FINISHES IN PLACE.

SITE LOCATION MAP



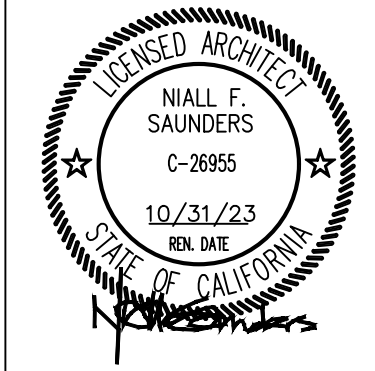
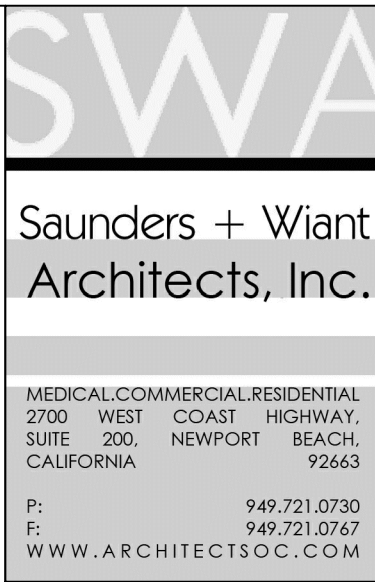
THE SITE
EXISTING BLDG.

KEY PLAN

FOR EXISTING SITE ACCESSIBILITY CONFORMANCE & ACTUAL SITE CONDITIONS, SEE SHEET A-0.1
FOR SITE ACCESSIBILITY CONFORMANCE DETAILS, SEE SHEET A-7.0

SHEET INDEX

A-0.0 COVER SHEET
A-0.01 EXISTING CONDITIONS OF APPROVAL
A-0.1 SITE PLAN
A-1.0 FLOOR PLAN
A-3.0 EXISTING EXTERIOR IMAGES
A-3.1 RENDERINGS
A-3.2 EXTERIOR ELEVATIONS (EXISTING)
A-3.3 EXTERIOR ELEVATIONS (EXISTING)
A-3.4 EXTERIOR ELEVATIONS (PROPOSED)
A-3.5 EXTERIOR ELEVATIONS (PROPOSED)



APN:
532T-015-020

BUILDING AREA:
EXIST. GROSS BLDG. AREA: 3216 S.F.
(AREA TO BE DEMOLISHED: 354 S.F.)
NEW EXPANSION AREA: 1957 S.F.
NEW GROSS BLDG. AREA: 4819 S.F.
SITE: 14376 S.F.
FLOOR AREA RATIO: 0.34

EXISTING FLOOR AREA OF SUITE:
ANIMAL CLINIC: 2129 S.F.
DESIGN OFFICE: 1087 S.F.
TOTAL EXISTING: 3216 S.F.

NEW FLOOR AREA OF SUITE:
ANIMAL CLINIC: 4819 S.F.
TOTAL NEW: 4819 S.F.

OCCUPANCY:
B (VETERINARY CLINIC)

OCCUPANCY LOAD:
TOTAL: 32
ANIMAL CLINIC (4819 S.F.): 32 (150 GROSS)

EXITS:
EXITS REQUIRED: 1
EXITS PROVIDED: 2

Vet Villa Animal Hospital
Veterinary Office Expansion Project
2140 HUNTINGTON DR. SOUTH PASADENA, CA 91030

COVER SHEET

PROJECT NO. 2020
DATE: 7/6/2021
REVISIONS:

PERMIT NO.

SHEET NO.

A-0.0

**EXHIBIT "A"
CONDITIONS OF APPROVAL
Conditional Use Permit and Design Review**

**PROJECT NO. 2035-CUP-DRX
2140 Huntington Drive (APN: 5321-015-020)**

CONDITIONS OF APPROVAL

PLANNING DIVISION:

- C-P1. This Conditional Use Permit (CUP) is an approval to subdivide a 3,216 square foot office building at 2140 Huntington Drive into a multifamily center for two businesses. This CUP also allows the establishment of a 2,129 square foot veterinary clinic. The remaining 1,087 square feet will be used as a design office.
- C-P2. This CUP also includes Design Review approval for new signs including two 23 square foot sign areas with individual letters on the tower feature of the building and a 9 square foot sign area with individual letters at the corner perimeter wall that would read "Vet Villa Animal Hospital". The individual letters of the signs are made of a stainless steel. Each letter will have a back light feature that creates a "halo effect" at night. The horizontal line separating the words "Vet Villa" from "Animal Hospital" would be a metal rail with a black smoke powder coat finish. The letters are raised two inches from the exterior surface. The design office would have a two square foot, vinyl cut sign mounted on the glass surface of the entrance doors.
- C-P3. The applicant shall obtain any applicable permits from the Building Division prior to commencement of any construction for the project, including the installation of the signs. All construction/remodel/tenant improvement plans shall be in substantial conformance with the plans approved by the Planning Commission on August 28, 2017.
- C-P4. The construction plans for the installation of the signs shall note that stainless steel mounting fixtures will be used to address the potential issue of rust stains on the wall.
- C-P5. A business license from the City's Finance Department shall be obtained prior to the start of any business activity within the multifamily building.
- C-P6. Overnight boarding within the veterinary clinic will only be for the hospitalized pets. Recreational boarding is prohibited.
- C-P7. The hours of operation for the veterinary clinic are limited between the hours of 7am to 10pm daily for the treatment of dogs and cats. Any additional hours of operation or deviations from the veterinary services described herein shall be referred to the Planning Commission for a modification to this CUP.
- C-P8. If operation of the veterinary clinic results in conflicts related to interior or exterior noise, odors, or other factors verified by the Community Improvement Coordinator, at the Discretion of the Planning and Building Director, this CUP may be submitted to the Planning Commission for subsequent review at a public hearing.
- C-P9. Animal and veterinary clinic medical waste shall be disposed in compliance with the applicable California State and Los Angeles County health regulations. All other non-

veterinary clinic/animal waste shall be disposed in the refuse containers located in the service area behind the clinic.

DEVELOPMENT REQUIREMENTS

Note: As a convenience to the applicant, the development requirements from applicable Departments/Agencies are listed herein. These requirements list what the applicant will be required to comply with in order to receive a Building Permit, a Certificate of Occupancy, or other Department-issued entitlement.

PLANNING DIVISION:

- P1. Approval by the Planning Commission does not constitute a building permit or authorization to begin any construction. An appropriate permit issued by the South Pasadena Building Division must be obtained prior to construction, enlargement, relocation, conversion or demolition of any building or structure on any of the properties involved with the Design Review and Hillside Development Permit.
- P2. All other requirements of any law, ordinance, or regulation of the State of California, City of South Pasadena, and any other government entity shall be complied with.
- P3. This CUP and all rights hereunder shall terminate within twelve (12) months of its effective date unless the use approved under the CUP is exercised or an extension is granted based on a written request submitted to the Planning and Building Department at least ten (10) days prior to the expiration of the twelve (12) month period pursuant to Section 36.420 of the South Pasadena Municipal Code. This CUP shall not be deemed "exercised" until the permittee has submitted plans to the Building Division for plan review and paid the requisite fees for plan check.
- P4. Compliance with and execution of all conditions listed herein shall be necessary prior to obtaining any occupancy inspection clearance and/or prior to obtaining any occupancy clearance.
- P5. The applicant and each successor in interest to the property which is the subject of this project approval, shall defend, indemnify and hold harmless the City of South Pasadena and its agents, officers and employees from any claim, action or proceeding against the City or its agents, officers or employees to attack, set aside, void or annul any approval of the City, City Council or City Planning Commission concerning this use.
- P6. The construction shall be kept free of all loose materials resembling trash and debris in excess of that material used for immediate construction purposes. Such excess may include, but is not limited to: the accumulation of debris, garbage, lumber, scrap metal, concrete, asphalt, piles of earth, salvage materials, abandoned or discarded furniture, appliances or other household fixtures.
- P7. The hours of construction shall be limited to 8:00am and 7:00pm Monday through Friday, 9:00am and 7:00pm Saturday, and construction on Sundays is limited from 10:00am to 6:00pm.

BUILDING AND SAFETY DIVISION:

- B1. The initial plan check fee will cover the initial plan check and one recheck **only**. Additional review required beyond the first recheck shall be paid for on an hourly basis in accordance with the current fee schedule.

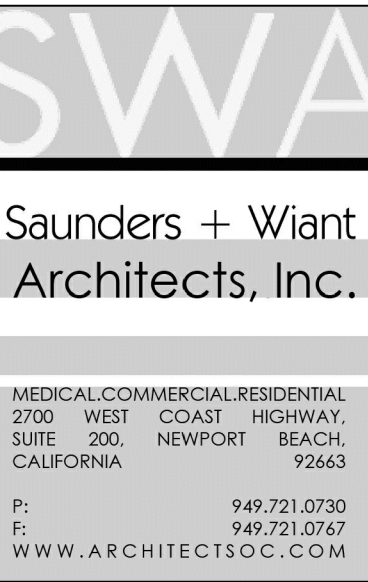
- B2. The second sheet of building plans is to list all conditions of approval and to include a copy of the Planning Commission Decision letter. This information shall be incorporated into the plans prior to the first submittal for plan check.
- B3. Fees shall be paid to the County of Los Angeles Sanitation District prior to issuance of the building permit.
- B4. Per Ordinance 2280 that took effect on April 9, 2015, unless specifically exempt Sewer/Water Impact fees shall be paid to Public Works Department prior or at the time City receives an application for utility service. Contact Public Works Department to calculate the sewer/water impact fee.
- B5. All State of California disability access regulations for accessibility and adaptability shall be complied with.
- B6. Approval is required from the Los Angeles County Health Department for X-ray equipment.
- B7. Plumbing fixtures shall be provided and available to all occupants as required by the Chapter 4 of the California Plumbing Code.
- B8. Means of egress in accordance with the applicable sections of the California Building Code shall be provided from the Basement area.

DEPARTMENT OF PUBLIC WORKS:

- PW1. Comply with all plan check corrections, revisions and conditions.
- PW2. Replace all broken, damaged, or out-of-grade sidewalk, curb and gutter, painted curb markings, asphalt/concrete damaged or disturbed during construction to the satisfaction of the City Engineer. Such items shall be repaired or restored to the documented condition found prior to the start of construction activity.
- PW3. Any storage or occupation of the public right-of-way shall be subject to an encroachment permit and the monthly permit fee.

FIRE DEPARTMENT:

- FD1. Address identification. New and existing buildings shall have *approved* address numbers, building numbers or *approved* building identification placed in a position that is plainly legible and visible from the street or road fronting the property. These numbers shall contrast with their background. Address numbers shall be Arabic numbers or alphabetical letters. Numbers shall be a minimum of 4 inches (101.6 mm) high with a minimum stroke width of 0.5 inch (12.7 mm). (CFC 505.1)
- FD2. **A Set of plans** must remain on the job site all times. Appointments for inspections should be made at least two days in advance of required inspection by calling the Fire Department at (626) 403-7304.
- FD3. The City of South Pasadena Fire Department reserves the right to change or otherwise modify requirements based upon receiving additional project information or other unforeseen circumstances.



Vet Villa Animal Hospital
Veterinary Office Expansion Project
 2140 HUNTINGTON DR. SOUTH PASADENA, CA 91030

CONDITIONS OF APPROVAL AND SUMMARY OF ACCESSIBILITY UPGRADES

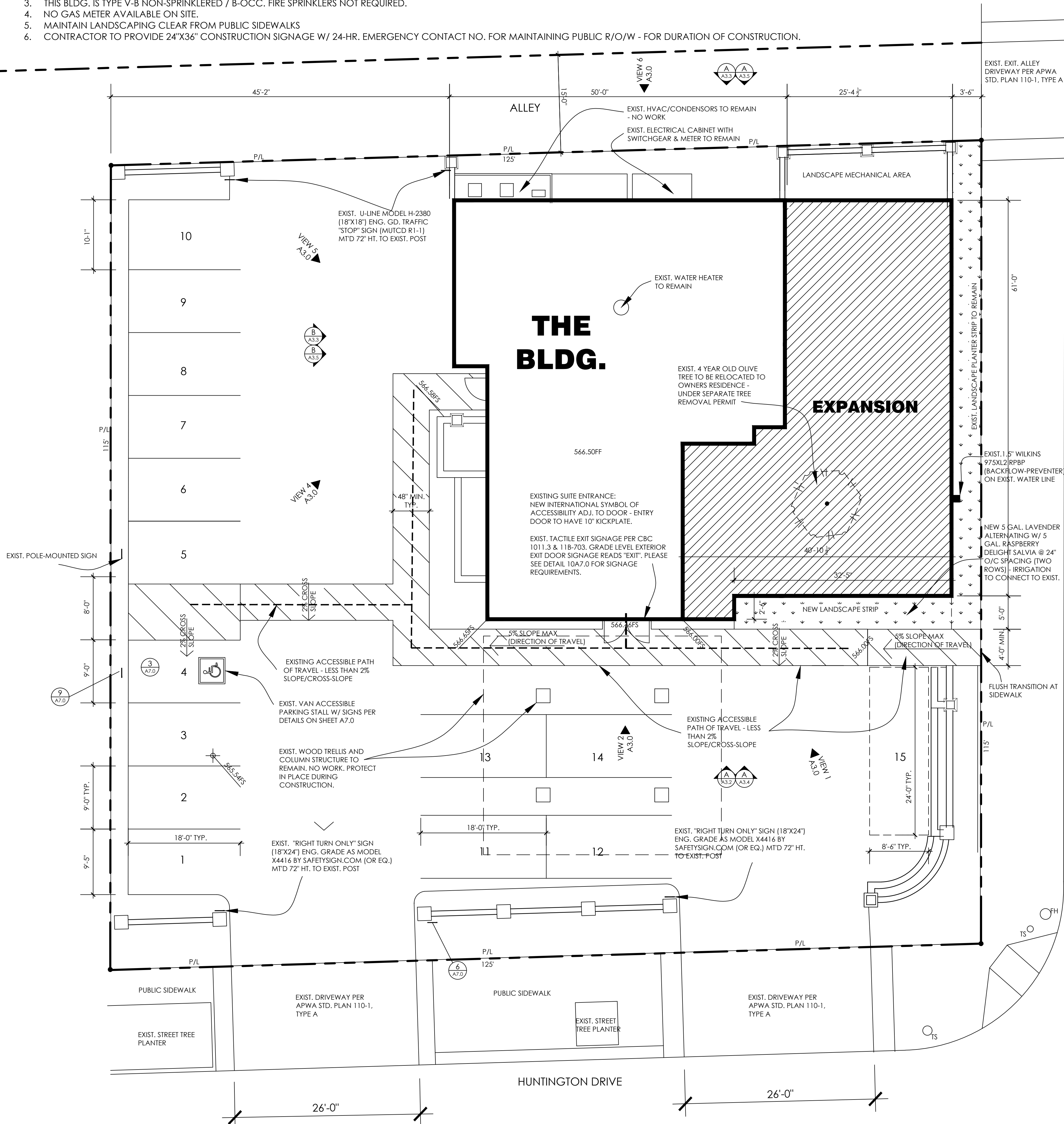
PROJECT NO. 2020
DATE: 7/6/2021
REVISIONS:

PERMIT NO.

SHEET NO.

A-0.01

- NOTE:
- IF ANY ACCESSIBLE COMPONENT IS NOT IN COMPLIANCE PER FIELD VERIFICATION BY THE CITY BUILDING INSPECTOR THEN IT WILL BE REQUIRED TO BE BROUGHT INTO FULL COMPLIANCE WITH CURRENT ACCESSIBILITY STANDARDS. CBC 107.2.5.
 - CONTRACTOR SHALL FOLLOW ALL REQUIRED BMP'S WITH REGARD TO POLLUTION CONTROL AND RUN-OFF DURING CONSTRUCTION OPERATIONS. SEE SWPP NOTES THIS SHEET.
 - THIS BLDG. IS TYPE V-B NON-SPRINKLERED / B-OCC. FIRE SPRINKLERS NOT REQUIRED.
 - NO GAS METER AVAILABLE ON SITE.
 - MAINTAIN LANDSCAPING CLEAR FROM PUBLIC SIDEWALKS
 - CONTRACTOR TO PROVIDE 24"X36" CONSTRUCTION SIGNAGE W/ 24-HR. EMERGENCY CONTACT NO. FOR MAINTAINING PUBLIC R/O/W - FOR DURATION OF CONSTRUCTION.



SWPP NOTES

STORM WATER POLLUTION PREVENTION DEVICES AND PRACTICES SHALL BE INSTALLED AND/OR INSTITUTED AS NECESSARY TO ENSURE COMPLIANCE TO THE CITY WATER QUALITY STANDARDS AND ANY EROSION CONTROL PLAN ASSOCIATED WITH THIS PROJECT. ALL SUCH DEVICES AND PRACTICES SHALL BE MAINTAINED, INSPECTED AND/OR MONITORED TO ENSURE ADEQUACY AND PROPER FUNCTION THROUGHOUT THE DURATION OF THE CONSTRUCTION PROJECT.

COMPLIANCE TO THE WATER QUALITY STANDARDS ON ANY EROSION CONTROL PLAN ASSOCIATED WITH THIS PROJECT INCLUDES, BUT IS NOT LIMITED TO THE FOLLOWING REQUIREMENTS:

- EFFECTIVE COMBINATION OF EROSION AND SEDIMENT CONTROL BMP'S SHALL BE IMPLEMENTED TO PROTECT THE EXPOSED PORTIONS OF THE SITE FROM EROSION AND TO PREVENT SEDIMENT DISCHARGES.
- SEDIMENTS AND OTHER POLLUTANTS SHALL BE RETAINED ON SITE UNTIL PROPERLY DISPOSED OF, AND MAY NOT BE TRANSPORTED FROM THE SITE VIA SHEET FLOW, SWALES, AREA DRAINS, NATURAL DRAINAGE COURSES OR WIND.
- STOCKPILES OF EARTH AND OTHER CONSTRUCTION-RELATED MATERIALS SHALL BE PROTECTED FROM BEING TRANSPORTED FROM THE SITE BY THE FORCES OF WIND AND WATER FLOW.
- FUELS, OILS, SOLVENTS, AND OTHER TOXIC MATERIALS SHALL BE STORED IN ACCORDANCE WITH THEIR LISTING AND ARE NOT TO CONTAMINATE THE SOIL AND SURFACE WATERS. ALL APPROVED STORAGE CONTAINERS ARE TO BE PROTECTED FROM THE WEATHER. SPILLS MUST BE CLEANED UP IMMEDIATELY AND DISPOSED OF IN A PROPER MANNER. SPILLS MAY NOT BE WASHED INTO THE DRAINAGE SYSTEM, NOR BE ALLOWED TO SETTLE OR INFILTRATE INTO SOIL.
- EXCESS OR WASTE CONCRETE MAY NOT BE WASHED INTO THE PUBLIC WAY OR ANY OTHER DRAINAGE SYSTEM. PROVISIONS SHALL BE MADE TO RETAIN CONCRETE WASTES ON SITE UNTIL THEY CAN BE DISPOSED OF AS SOLID WASTES.
- TRASH AND CONSTRUCTION SOLID WASTES SHALL BE DEPOSITED INTO A COVERED RECEPTACLE TO PREVENT CONTAMINATION OF RAINWATER AND DISPERSAL BY WIND.
- SEDIMENTS AND OTHER MATERIALS MAY NOT BE TRACKED FROM THE SITE BY VEHICULAR TRAFFIC. THE CONSTRUCTION ENTRANCE ROADWAYS MUST BE STABILIZED SO AS TO INHIBIT SEDIMENTS FROM BEING DEPOSITED INTO THE PUBLIC WAY. ACCIDENTAL DEPOSITS SHALL BE SWEEPED UP IMMEDIATELY AND MAY NOT BE WASHED DOWN BY RAIN OR OTHER MEANS.
- STORM WATER POLLUTION PREVENTION DEVICES AND /OR PRACTICES SHALL BE MODIFIED AS NEEDED AS THE PROJECT PROGRESSES TO ENSURE EFFECTIVENESS.

ADDITIONAL ENVIRONMENTAL REQUIREMENTS

AT LEAST 75% OF CONSTRUCTION WASTE MATERIALS ARE TO BE RECYCLED.

SITE ACCESSIBILITY NOTES

- SITE FULLY COMPLIES WITH T-24 ACCESSIBILITY STANDARDS; AND SUBJECT TO FIELD VERIFICATION
- ALL FLOOR AND GROUND SURFACES SHALL BE STABLE, FIRM AND SLIP RESISTANT TYPE. CBC 11B-302.1.
- FOR WORK SURFACES REQUIRED TO BE ACCESSIBLE, TOE CLEARANCE SHALL BE PROVIDED THAT IS 30 INCHES IN WIDTH AND 9 INCHES IN HEIGHT ABOVE FINISH FLOOR OR GROUND FOR A DEPTH OF 19 INCHES MINIMUM. CBC 11B-206.2.8.
- COMMON USE CIRCULATION PATHS WITHIN EMPLOYEE WORK AREAS SHALL COMPLY WITH 11B-402 ACCESSIBLE ROUTES. CBC 11B-206.2.8.
- MINIMUM MANEUVERING CLEARANCES AT DOORS AND GATES SHALL COMPLY WITH 11B-404.2.4. MANEUVERING CLEARANCES SHALL EXTEND THE FULL WIDTH OF THE DOORWAY AND THE REQUIRED LATCH SIDE OR HINGE SIDE CLEARANCE. CBC 11B-404.2.4.
- SWINGING DOORS AND GATES SHALL HAVE MANEUVERING CLEARANCES COMPLYING WITH TABLE 11B-404.2.4.1.
- EXISTING BUILDINGS THAT HAVE BEEN REMODELED TO PROVIDE SPECIFIC TOILET ROOMS FOR PUBLIC USE THAT COMPLY WITH THESE STANDARDS SHALL HAVE THE LOCATION OF AND DIRECTIONS TO THESE ROOMS POSTED IN OR NEAR THE BUILDING LOBBY OR ENTRANCE ON A SIGN COMPLYING WITH 11B-703.5. CBC 11B-216.8.
- WORK SURFACES SHALL COMPLY WITH 11B-902.2 AND 11B-902.3.

CONSTRUCTION SAFEGUARDS NOTES

- CONSTRUCTION EQUIPMENT AND MATERIALS SHALL BE STORED AND PLACED SO AS TO NOT ENDANGER THE PUBLIC, THE WORKERS OR ADJOINING PROPERTY FOR THE DURATION OF THE CONSTRUCTION PROJECT. CBC 3301.2.
- REQUIRED EXITS, EXISTING STRUCTURAL ELEMENTS, FIRE PROTECTION DEVICES AND SANITARY SAFEGUARDS SHALL BE MAINTAINED AT ALL TIMES DURING ALTERATIONS, REPAIRS OR ADDITIONS TO ANY BUILDING OR STRUCTURE. CBC 3302.1.
- SANITARY FACILITIES SHALL BE PROVIDED DURING CONSTRUCTION, REMODELING OR DEMOLITION ACTIVITIES IN ACCORDANCE WITH CALIFORNIA PLUMBING CODE. CBC 3305.1.
- ADJOINING PUBLIC AND PRIVATE PROPERTY SHALL BE PROTECTED FROM DAMAGE DURING CONSTRUCTION, REMODELING OR DEMOLITION WORK. PROTECTION SHALL BE PROVIDED FOR FOOTINGS, FOUNDATIONS, PARTY WALLS, CHIMNEYS, SKYLIGHTS AND ROOFS. CBC 307.1.
- STRUCTURES UNDER CONSTRUCTION, ALTERATION OR DEMOLITION SHALL BE PROVIDED WITH NO FEWER THAN ONE APPROVED PORTABLE FIRE EXTINGUISHER IN ACCORDANCE WITH SECTION 906 AND SIZED FOR ORDINARY HAZARD. CBC 309.1.

I CERTIFY THAT THE PRIMARY PATH OF TRAVEL TO THE SPECIFIC AREA OF ALTERATION, STRUCTURAL REPAIR OR ADDITION FROM THE PUBLIC WAY OR ACCESSIBLE PARKING SPACE AS INDICATED ON THE PLANS DOES NOT INCLUDE STEPS OR A SLOPE EXCEEDING 1:20 EXCEPT WHERE ACCESS IS PROVIDED BY A RAMP WITH 1:12 MAXIMUM SLOPE, ACCESSIBLE ELEVATOR OR OTHERWISE GRANTED BY AN UNREASONABLE HARDSHIP EXEMPTION. I UNDERSTAND THAT IF THE PRIMARY PATH OF TRAVEL IS FOUND NOT TO BE AS INDICATED, SIGNIFICANT DELAYS MAY RESULT.

FURTHERMORE, I CERTIFY THAT THESE PLANS WERE PREPARED UNDER MY DIRECT SUPERVISION AND THAT THE AREA OF SPECIFIC ALTERATION, STRUCTURAL REPAIR OR ADDITION, INCLUDING PRIMARY ENTRANCE TO THE EXISTING BUILDING AND, WHEN APPLICABLE, SANITATION FACILITIES, DRINKING FOUNTAINS, SIGNS AND PUBLIC TELEPHONES SERVING THE AREA COMPLIES WITH CURRENT CA TITLE 24 ACCESSIBILITY REQUIREMENTS.

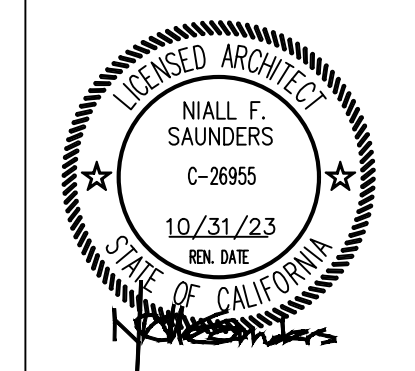
Niall Saunders
 SIGNATURE OF THE ARCHITECT: NIALL SAUNDERS DATE: 7-6-2021
 LICENSE NUMBER: C-26955 EXPIRATION DATE: 10/31/2021

PARKING ANALYSIS

NOTE: IF ANY ACCESSIBLE COMPONENT IS NOT IN COMPLIANCE PER FIELD VERIFICATION BY THE CITY BUILDING INSPECTOR THEN IT WILL BE REQUIRED TO BE BROUGHT INTO FULL COMPLIANCE WITH CURRENT ACCESSIBILITY STANDARDS. CBC 107.2.5.

PARKING STUDY:	S.F.	RATIO	PARKING SPACES
625 W. LA CADENA			
EXPANDED ANIMAL CLINIC	4819 S.F.	3/1000	14.45
TOTAL			15 (15 PROVIDED)

SWA
 Saunders + Wiant
 Architects, Inc.
 MEDICAL COMMERCIAL RESIDENTIAL
 2700 WEST COAST HIGHWAY
 SUITE 200 NEWPORT BEACH,
 CALIFORNIA 92663
 P: 949.721.0730
 F: 949.721.0747
 WWW.ARCHITECTSOCI.COM



Vet Villa Animal Hospital
 Veterinary Office Expansion Project
 2140 HUNTINGTON DR. SOUTH PASADENA, CA 91030

SITE PLAN

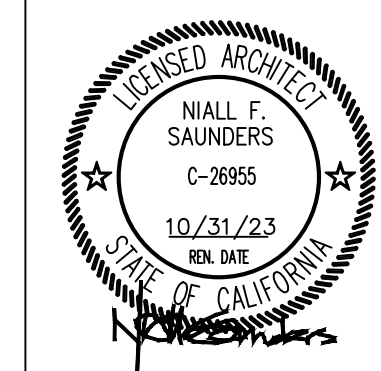
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 DATE: 7/6/2021
 REVISIONS:

PERMIT NO.

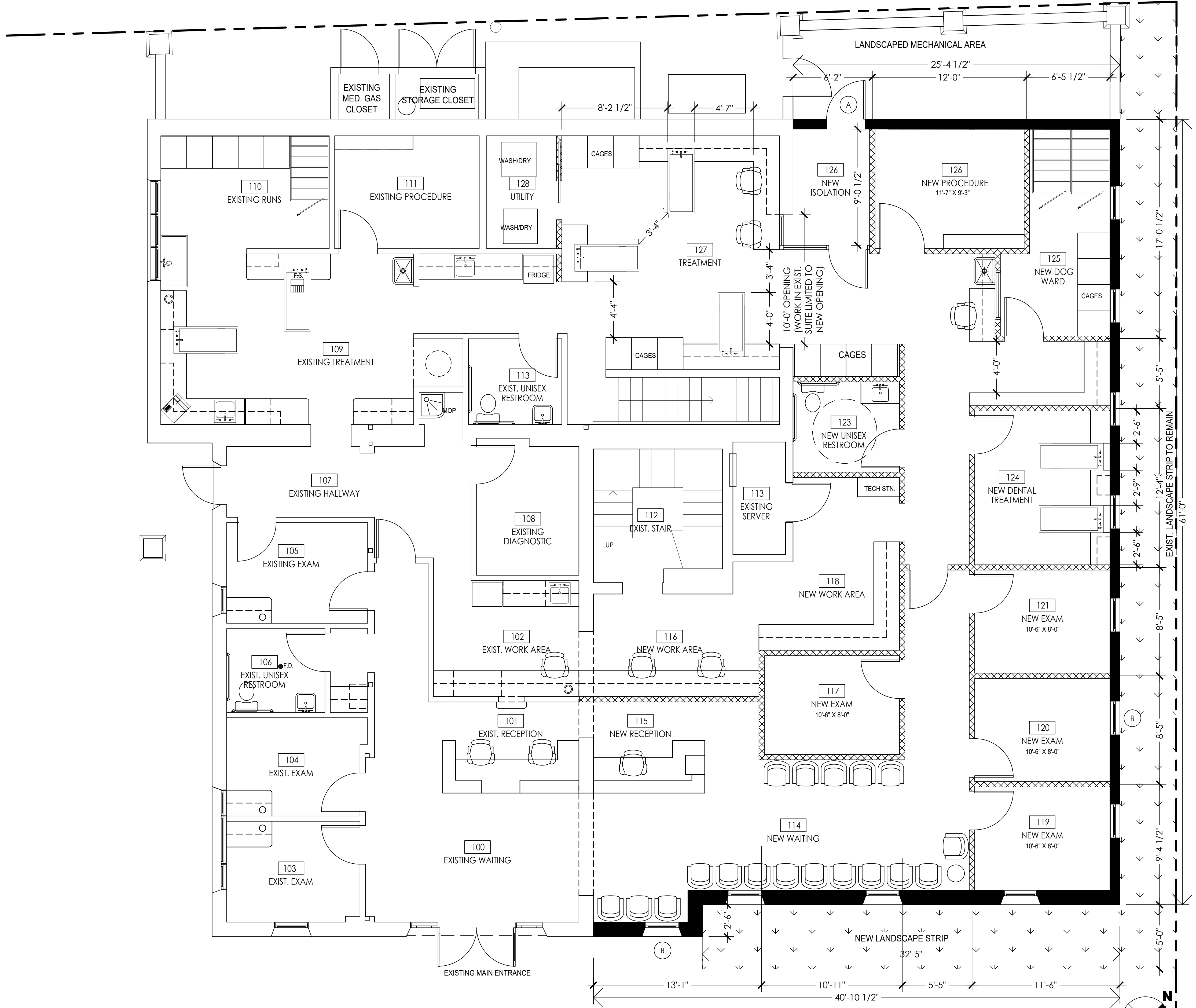
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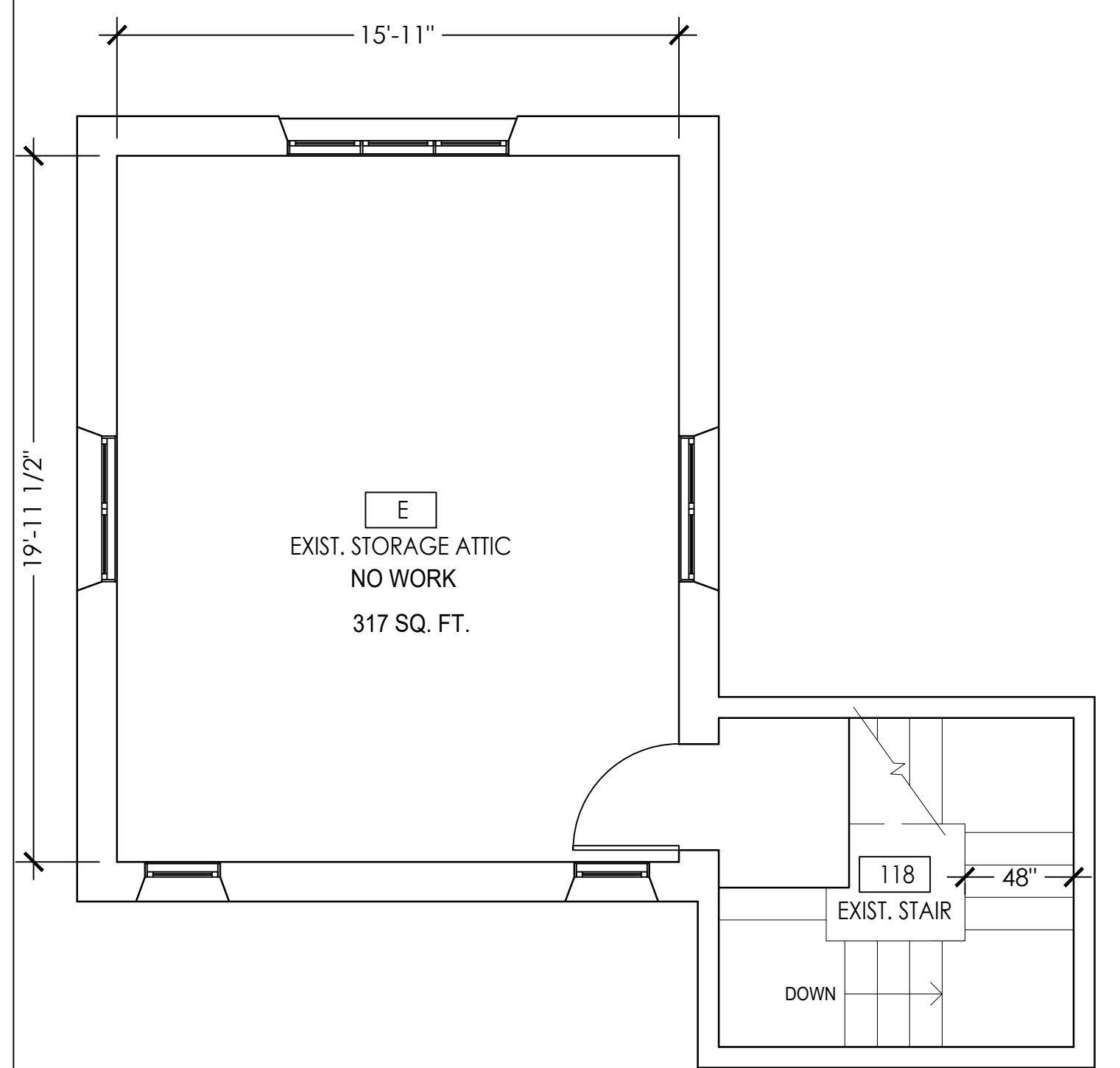
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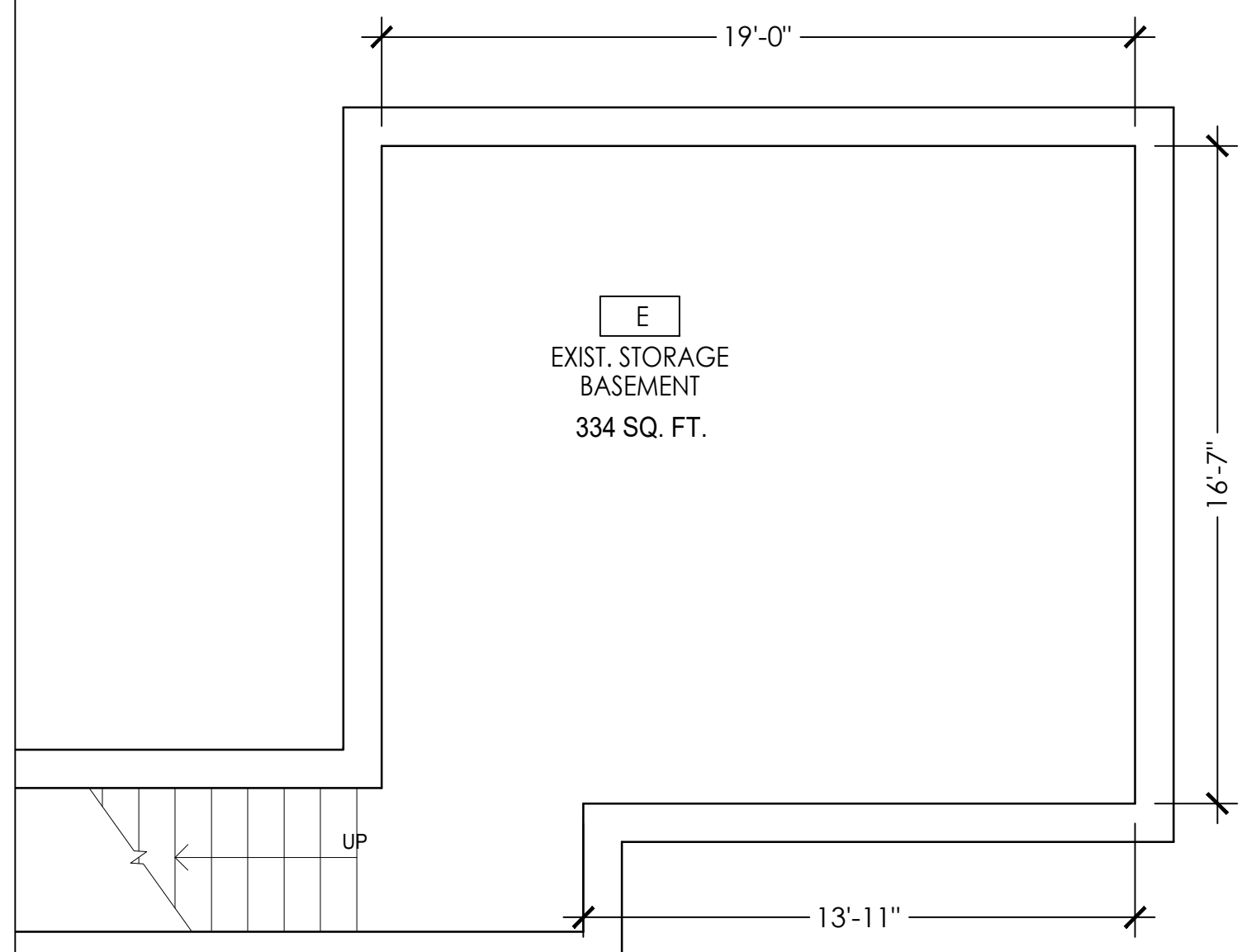
Vet Villa Animal Hospital
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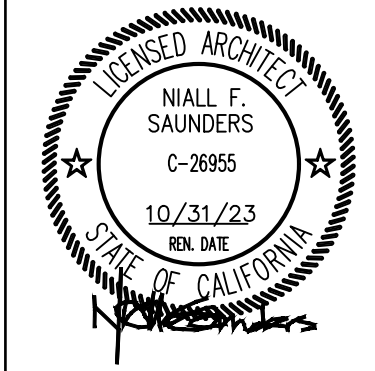
FLOOR PLAN
 1/4" = 1'-0"



ATTIC LEVEL PLAN



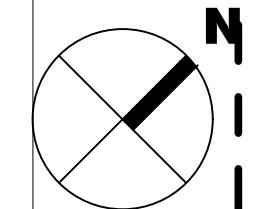
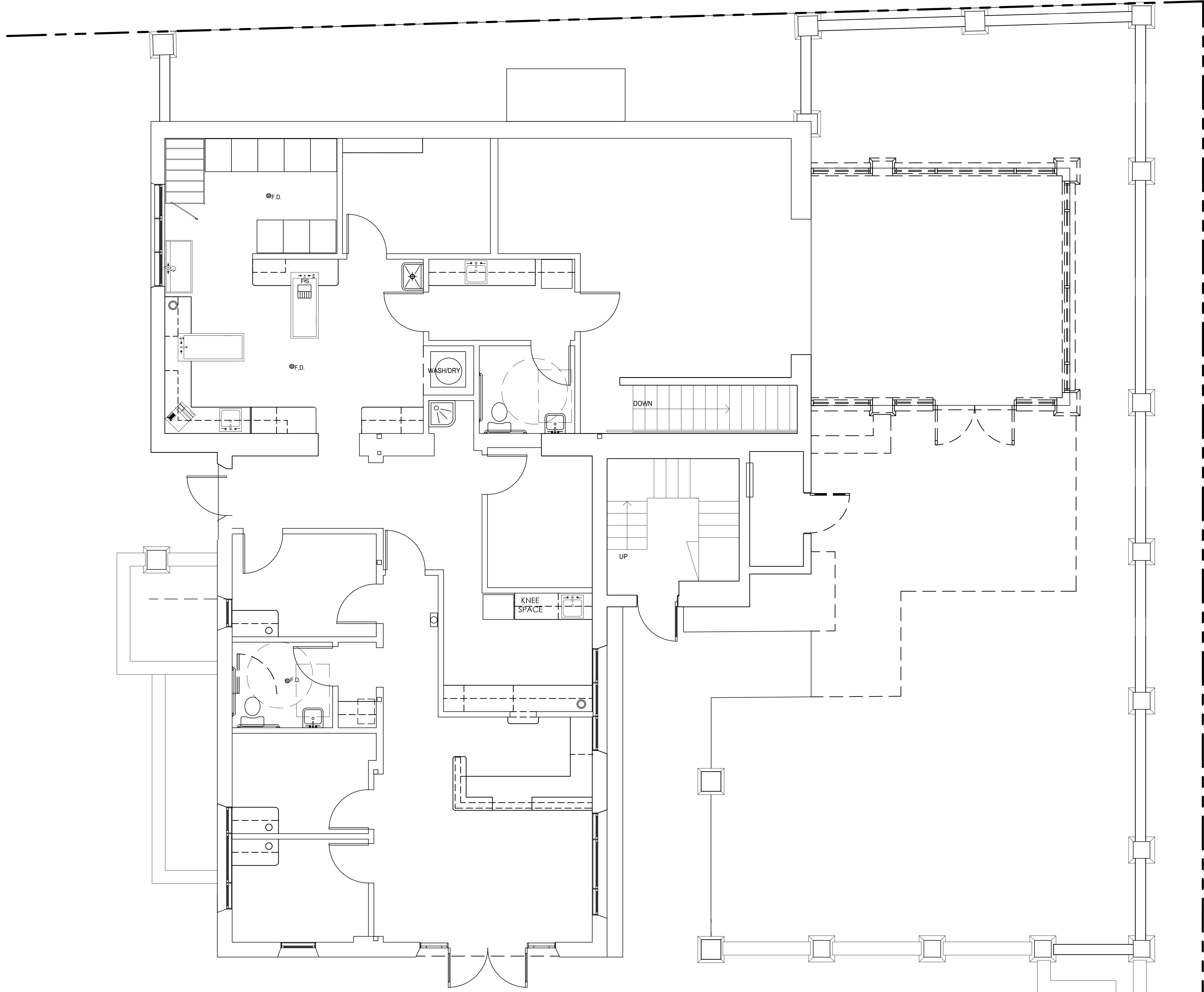
BASEMENT & ATTIC PLANS
 1/4" = 1'-0"



DEMOLITION PLAN KEYNOTES

1. CARRY OUT ALL DEMOLITION WORK INDICATED IN ACCORDANCE WITH STATE AND LOCAL CODES. CARRY OUT WORK DURING TIMES PERMITTED BY AND THE LOCAL AUTHORITIES AND INFORM ADJACENT PROPERTIES OF TIMES SCHEDULED FOR NOISY WORK OR ANY INTERRUPTION IN UTILITIES OR SERVICES. KEEP NUISANCE TO A MINIMUM.
2. FIXTURES, FURNISHINGS AND FINISHES SHOWN ARE TO BE REMOVED ONLY AS EXPRESSLY NOTED HEREIN.
3. REMOVE ALL DEBRIS FROM SITE AS WORK PROCEEDS AND DISPOSE OF PER CODE AND THE LOCAL CODE REQUIREMENTS. INFORM ARCHITECT IMMEDIATELY IF ANY HAZARDOUS MATERIALS ARE FOUND TO BE PRESENT.
4. PATCH AND REPAIR ANY DAMAGE THAT OCCURS TO THE EXISTING BUILDING SHELL, OR FINISHES NOT INDICATED TO BE DEMOLISHED.
5. ONLY INTERIOR PARTITIONS STRICTLY NOTED WITHIN EXISTING SUITE TO BE REMOVED. ONLY NON-LOAD BEARING WALLS TO BE DEMOLISHED.
6. REMOVE ANY EXISTING INTERIOR FLOORING AND PREP CONC. SLAB FOR NEW FINISH AS NOTED BELOW.
7. PATCH AND REPAIR ANY PITS OR HOLES IN FLOOR SLAB AS REQUIRED. CORE EXISTING CONC. FLOOR SLAB ONLY WHERE NEEDED TO ACCOMMODATE NEW UTILITIES (COORD. W/ FLOOR BEAMS).
8. REMOVE ONLY EXISTING DOORS AND FRAMES, AS SPECIFICALLY INDICATED.
9. REMOVE EXIST. CEILING, LIGHTING, SOFFITS, ETC. IN THIS AREA ONLY. ALL OTHER AREAS ARE EXISTING TO REMAIN
10. REMOVE EXISTING TOILET ROOM FIXTURES AND SPECIALTIES. CAP WATER / SEWER AND LEAVE READY FOR CONNECTION OR RELOCATION OF NEW FIXTURES.
11. REMOVE ALL ELECTRICAL OUTLETS IN WALLS TO BE REMOVED. REMOVE ANY SURFACE RUN CONDUIT OR ABANDONED ELECTRICAL OUTLETS. DO NOT REMOVE TELEPHONE AND CABLE TV UTILITY SERVING SITE.
12. MAIN ELECTRICAL PANEL AND BREAKERS TO REMAIN (SEE ELECTRICAL PLANS). DO NOT ADJUST UNTIL VERIFIED. ARRANGE FOR TEMPORARY POWER UNTIL NEW WORK IS COMPLETED AND READY FOR IMMEDIATE CONNECTION.
13. REMOVE ALL EXISTING INTERIOR AND EXTERIOR SIGNS, INCLUDING WINDOW SIGNS.
14. ENSURE EXISTING CEILING MOUNTED HVAC DUCTWORK IS ADJUSTED OR MOVED. DO NOT REMOVE ANY HVAC EQUIPMENT UNLESS EXPRESSLY NOTED.
15. EXISTING EXTERIOR BLDG. SHELL FEATURES TO BE PROTECTED IN PLACE.
16. PROTECT ALL EXISTING REMAINING WALLS (REPAIR), STRUCTURAL POSTS AND BEAMS.
17. INFORM ARCHITECT IMMEDIATELY OF ANY UN-RESOLVABLE CONFLICTS.
18. ADJUST EXIST. HVAC FENCE ENCLOSURE AS REQUIRED FOR NEW STORAGE BUILDING CONSTRUCTION.
19. EXISTING EXTERIOR ENTRY DOORS TO REMAIN.
20. REPAIR AND PATCH EXIST. EXTERIOR WALLS WHERE NECESSARY
21. PROCEED CAUTIOUSLY W/ DEMO THIS LOCATION TO VERIFY IF STRUCTURE POST EXISTS. INFORM ARCHITECT AT ONCE.
22. PROTECT EXISTING STAIRS DOWN TO BASEMENT. BASEMENT IS BELOW PORTION OF EXISTING OFFICE TO BE DEMOLISHED. CEILING OF BASEMENT TO REMAIN - REVIEW WITH ARCHITECT / ENGINEER AS DEMO PROCEEDS.

NOTE: INTERIOR PARTITION LAYOUT MAY NOT BE AS EXISTING ON SITE. PROCEED WITH CAUTION DURING DEMOLITION.



DEMOLITION PLAN
1/4" = 1'-0"

WALL LEGEND

- WALLS/HARDSCAPE/FIXTURES TO BE DEMOLISHED
- ===== EXISTING WALLS/HARDSCAPE/FIXTURES TO REMAIN

A
1.2



VIEW 1



VIEW 2



VIEW 3



VIEW 4



VIEW 5



VIEW 6

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NEW
EXTERIOR
IMAGES

PROJECT NO. 2020
DATE: 7/6/2021
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A-3.0



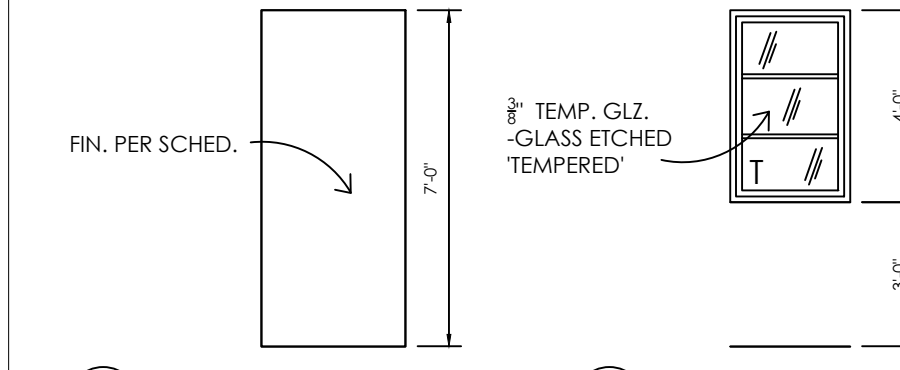
DOOR SCHEDULE - READ WITH DOOR NOTES/DETAIL 2 A7.0

NO	ROOM	RATING	TYPE OPENING					FRAME		HARDWARE		VISION PNL. / GLAZED	REMARKS
			WIDTH	HEIGHT	THICKNESS	MATERIAL	FINISH	MATERIAL	FINISH	HARDWARE SET	OPERATION		
A	126 - ISOLATION	NON RATED	3'-0"	7'-0"	1-3/4"	HM	P	HM	P	I	EXIT	NO	NEW DOOR - NOT AN EXIT

WINDOW SCHEDULE

NO	ROOM	OPENING			FRAME					HARDWARE			REMARKS
		WIDTH	HEIGHT	SILL HT.	MATERIAL	FINISH	HEAD DETAIL	JAMB DETAIL	SILL DETAIL	OPERATION	FINISH	SCREEN	
B	ALL LOCATIONS	4'-0"	2'-6"	3'-0"	ALUM	VINYL				YES	-	YES	ALL WINDOWS TO BE MARVIN ESSENTIAL CASEMENT IN CONFIG. SHOWN. PERFORMANCE RATING FOR ALL NEW WINDOWS: U FACTOR: 0.36 SHGC: 0.25 VT: 0.42 GLAZING AT ALL LOCATIONS SHALL BE OF SAFETY GLAZING MATERIAL IN ACCORDANCE WITH CBC SECTION 2406 AND SHALL CONFORM TO THICKNESSES AS SPECIFIED IN CBC 2403 (CBC TABLE 2403.2.1) PROVIDE CLEAR TEMP. GLASS W/ ETCH-MARKING "TEMPERED" (TYP. ALL LOC'NS.) PROVIDE 3M FROSTED FILM FINISH TO INSIDE FACE IN LOC'NS. APPROVED BY OWNER (IF APPLICABLE)

DOOR TYPE



- HARDWARE TYPES (OR EQUAL):**
 HINGES: HAGER AB800 4.5X4.5 NRP SEC STUD
 I.C. RIM CYLINDER: SCHLAGE 20-057
 DEADLOCK: SCHLAGE L9460L
 EXIT LOCK: SCHLAGE L9080 L/OST 17A
 SURFACE CLOSER: LCN 1480
 PERIMETER SEALS: ZERO 188 HEAD AND JAMBS
 SWEEP: ZERO 38A
 DOME STOP: GLYNN JOHNSON FB13 / 14R / FB17
 SILENCERS: GLYNN JOHNSON GJ64
- HARDWARE SETS**
 1. 3 PR X 4" BUTT HINGES
 LEVER CYL. LOCK
 WEATHERSTRIPPING, THRESHOLD & STOPS

- DOOR MATERIALS**
 P = PAINTED
 (E) = EXISTING
 ALUM = ALUMINUM W/ PRE-FINISH
 VP = VISION PANEL

COLOR/MATERIALS BOARD

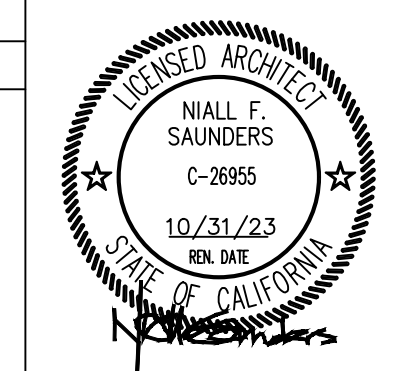
STUCCO:
 LA HABRA STUCCO
 SANTA BARBARA FINISH
 COLOR: X-55 FRENCH VANILLA
 *MATCH EXISTING

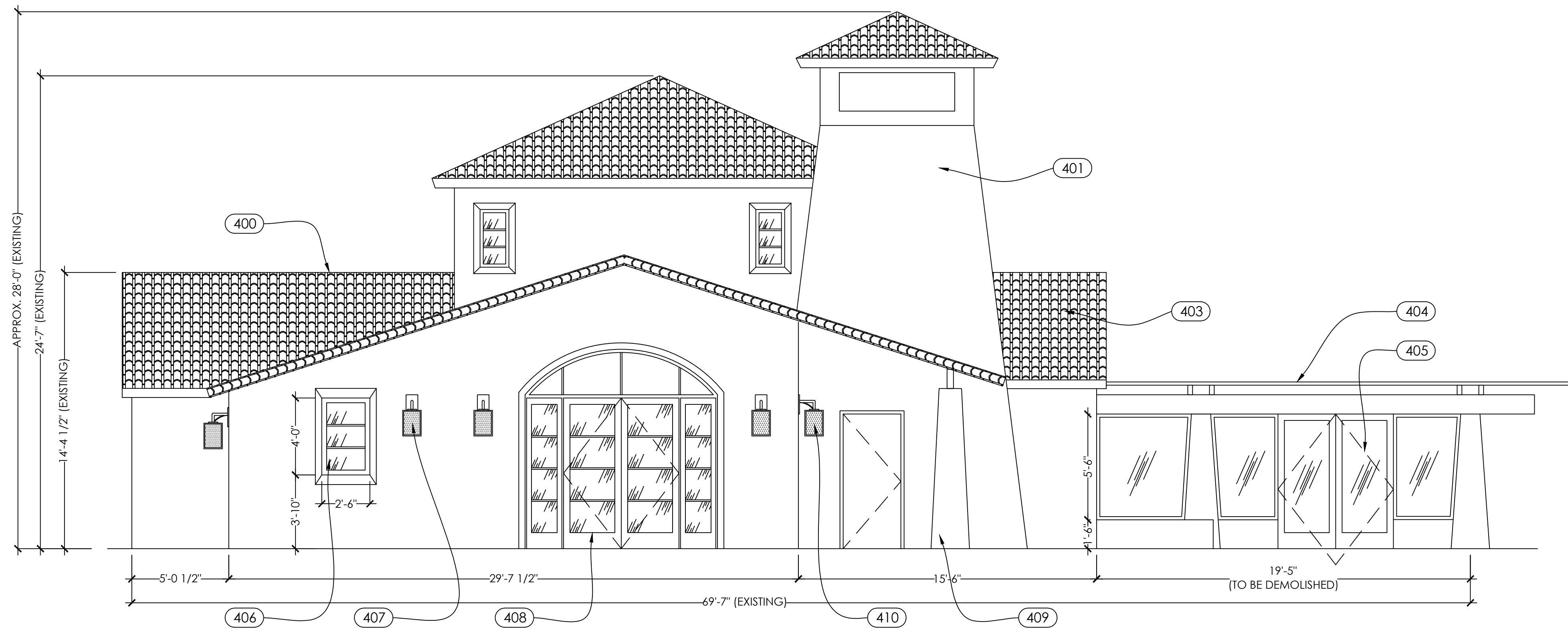
WINDOWS:
 3 PANE MARVIN CASEMENT WINDOW
 COLOR: MATCH W/ EXISTING

FLATWORK:
 SMOOTH FLOAT CONCRETE
 COLOR: GREY

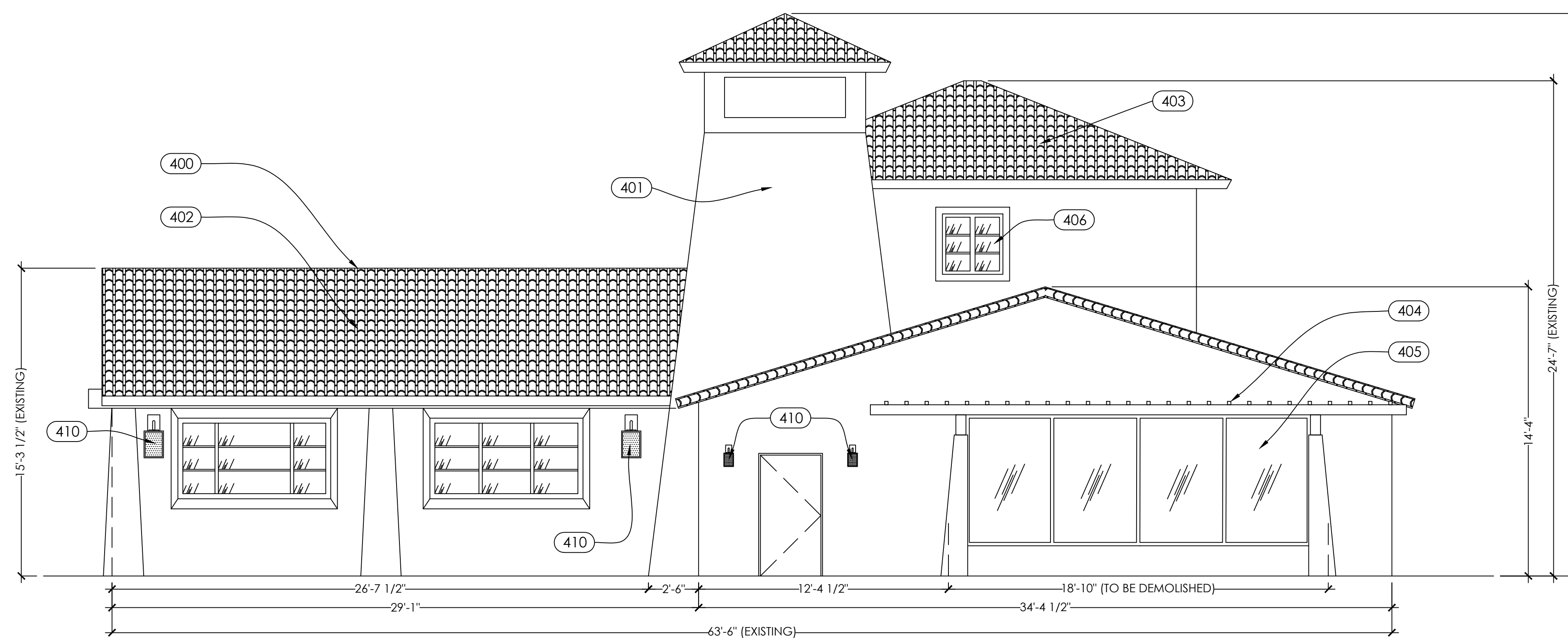
ROOFING:
 2-PIECE CLAY TILE W/ RANDOM MORTAR PACKING & BOOSTER TILES @ EAVES & RIDGES
 *MATCH EXISTING

*ALL WINDOWS TO HAVE 6" DEP W/ 2" FLARE STUCCO JAMB/HEADER TO MATCH EXIST. ALL STUCCO TREATMENTS TO MATCH EXISTING.





EXISTING SOUTH ELEVATION (HUNTINGTON DR.) 1/4" = 1'-0" **A**
3.2

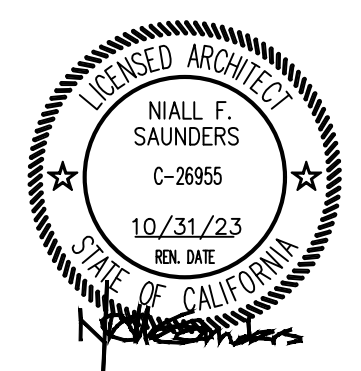


EXISTING EAST ELEVATION (GARFIELD AVE.) 1/4" = 1'-0" **B**
3.2

DRAWING KEYNOTES (EXISTING)
NOTE: NOT ALL KEYNOTES PERTAIN TO THIS PLAN

KEYNOTE	DESCRIPTION
400	PROVIDE THE FOLLOWING ITEMS:
400	EXISTING ROOF RIDGE
401	EXISTING STRUCTURE TO BE PROTECTED IN PLACE
402	ROOFING TO BE REMOVED - PROTECT IN PLACE ADJACENT ROOFING, ENSURE SEAMLESS CONNECTION TO NEW ROOF
403	EXISTING TILE ROOF - PATCH/REPAIR AS NECESSARY.
404	ENCLOSED TRELLIS ROOM - TO BE REMOVED
405	STOREFRONT GLAZING SYSTEM (WINDOW AND DOORS) - TO BE REMOVED
406	EXISTING GLAZING, TYP. - PROTECT IN PLACE
407	EXISTING EXTERIOR LIGHTING FIXTURES - TO REMAIN
408	STOREFRONT GLAZING SYSTEM - TO REMAIN, PROTECT IN PLACE
409	EXISTING POST/COLUMN - CLADDING TO BE REMOVED
410	EXTERIOR LIGHTING FIXTURES - TO BE REMOVED
411	EXISTING ELECTRICAL ROOM (SWITCH-GEAR, METERS, AND PANEL) - NO WORK
412	EXISTING 1-HR FIRE RATED MED. GAS CLOSET AND STORAGE - NO WORK
413	EXISTING TRASH ENCLOSURE
414	EXISTING EXTERIOR DOOR TO REMAIN
415	EXTERIOR DOOR TO BE REMOVED
416	EXISTING PLANTER/DOG WALK AREA - NO WORK

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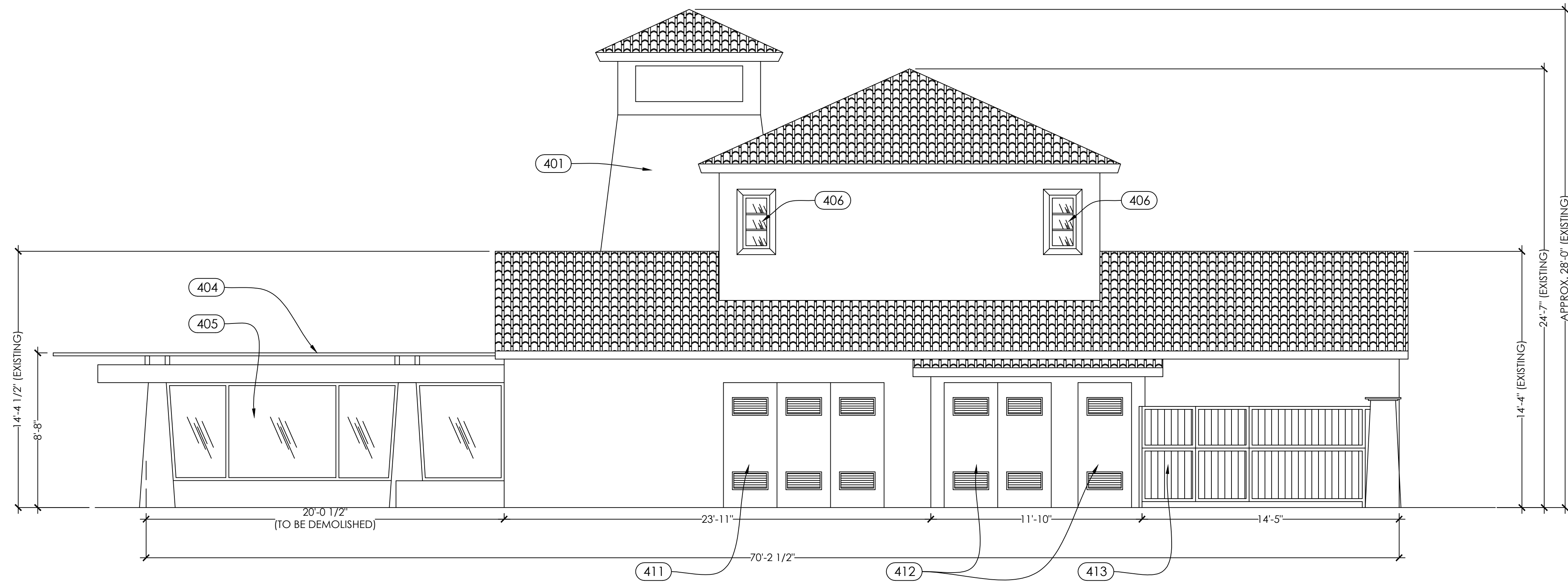
EXTERIOR ELEVATIONS (EXIST)

PROJECT NO. 2020
DATE: 7/6/2021
REVISIONS:

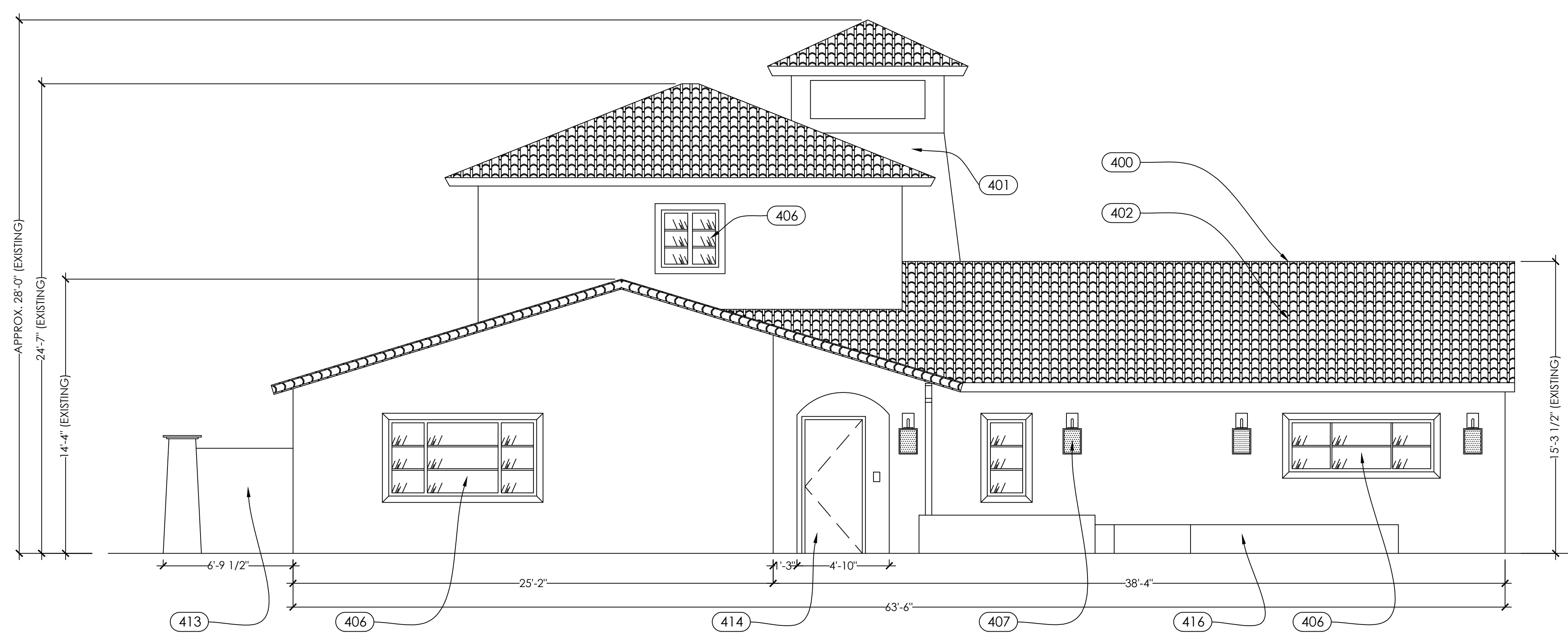
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A-3.2



EXISTING NORTH ELEVATION (ALLEYWAY) 1/4" = 1'-0" A
3.3



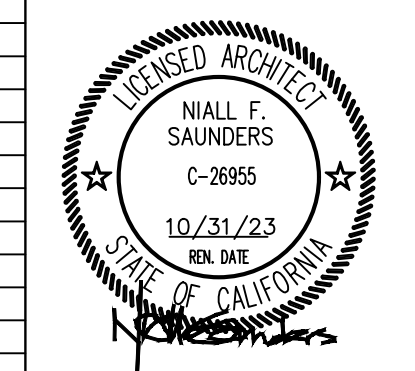
EXISTING WEST ELEVATION (PARKING) 1/4" = 1'-0" B
3.3

DRAWING KEYNOTES (EXISTING)
NOTE: NOT ALL KEYNOTES PERTAIN TO THIS PLAN

KEYNOTE	DESCRIPTION
400	PROVIDE THE FOLLOWING ITEMS:
400	EXISTING ROOF RIDGE
401	EXISTING STRUCTURE TO BE PROTECTED IN PLACE
402	ROOFING TO BE REMOVED - PROTECT IN PLACE ADJACENT ROOFING, ENSURE SEAMLESS CONNECTION TO NEW ROOF
403	EXISTING TILE ROOF - PATCH/REPAIR AS NECESSARY.
404	ENCLOSED TRELLIS ROOM - TO BE REMOVED
405	STOREFRONT GLAZING SYSTEM (WINDOW AND DOORS) - TO BE REMOVED
406	EXISTING GLAZING, TYP. - PROTECT IN PLACE
407	EXISTING EXTERIOR LIGHTING FIXTURES - TO REMAIN
408	STOREFRONT GLAZING SYSTEM - TO REMAIN, PROTECT IN PLACE
409	EXISTING POST/COLUMN - CLADDING TO BE REMOVED
410	EXISTING LIGHTING FIXTURES - TO BE REMOVED
411	EXISTING ELECTRICAL ROOM (SWITCH-GEAR, METERS, AND PANEL) - NO WORK
412	EXISTING 1-HR FIRE RATED MED. GAS CLOSET AND STORAGE - NO WORK
413	EXISTING TRASH ENCLOSURE
414	EXISTING EXTERIOR DOOR TO REMAIN
415	EXTERIOR DOOR TO BE REMOVED
416	EXISTING PLANTER/DOG WALK AREA - NO WORK

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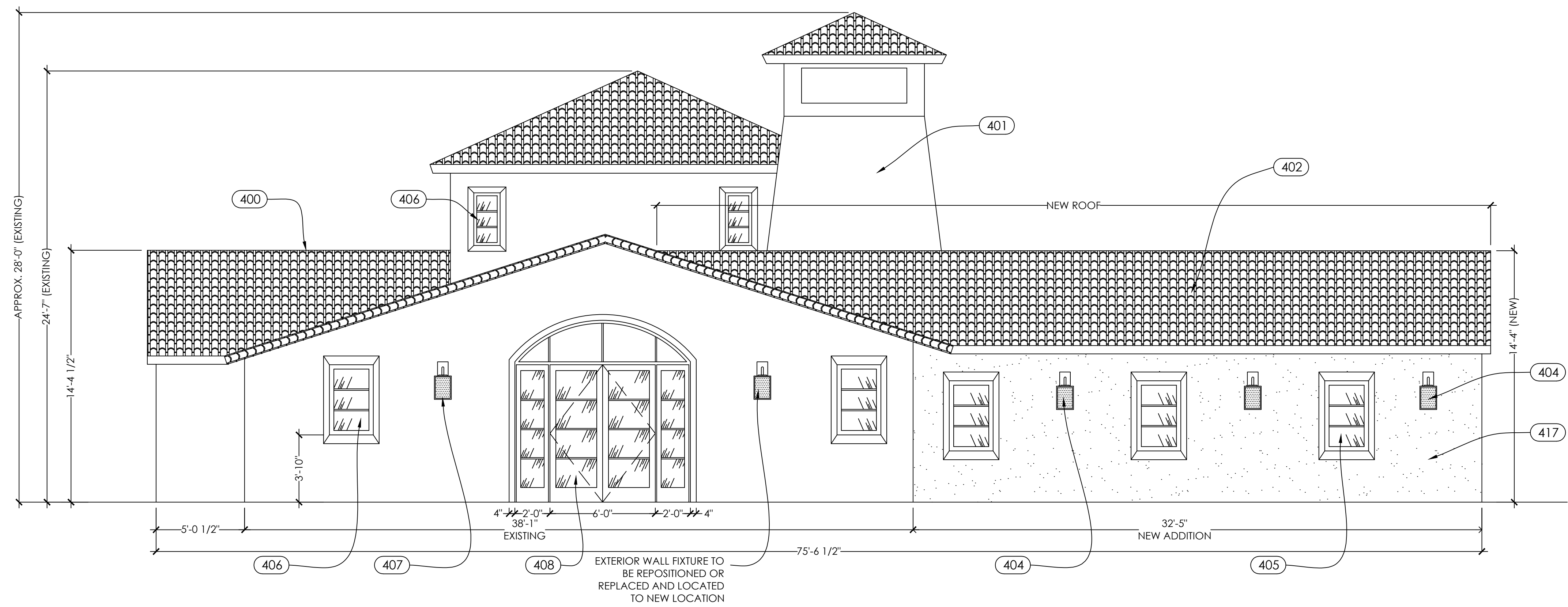
EXTERIOR ELEVATIONS (EXIST)

PROJECT NO. 2020
DATE: 7/6/2021
REVISIONS:

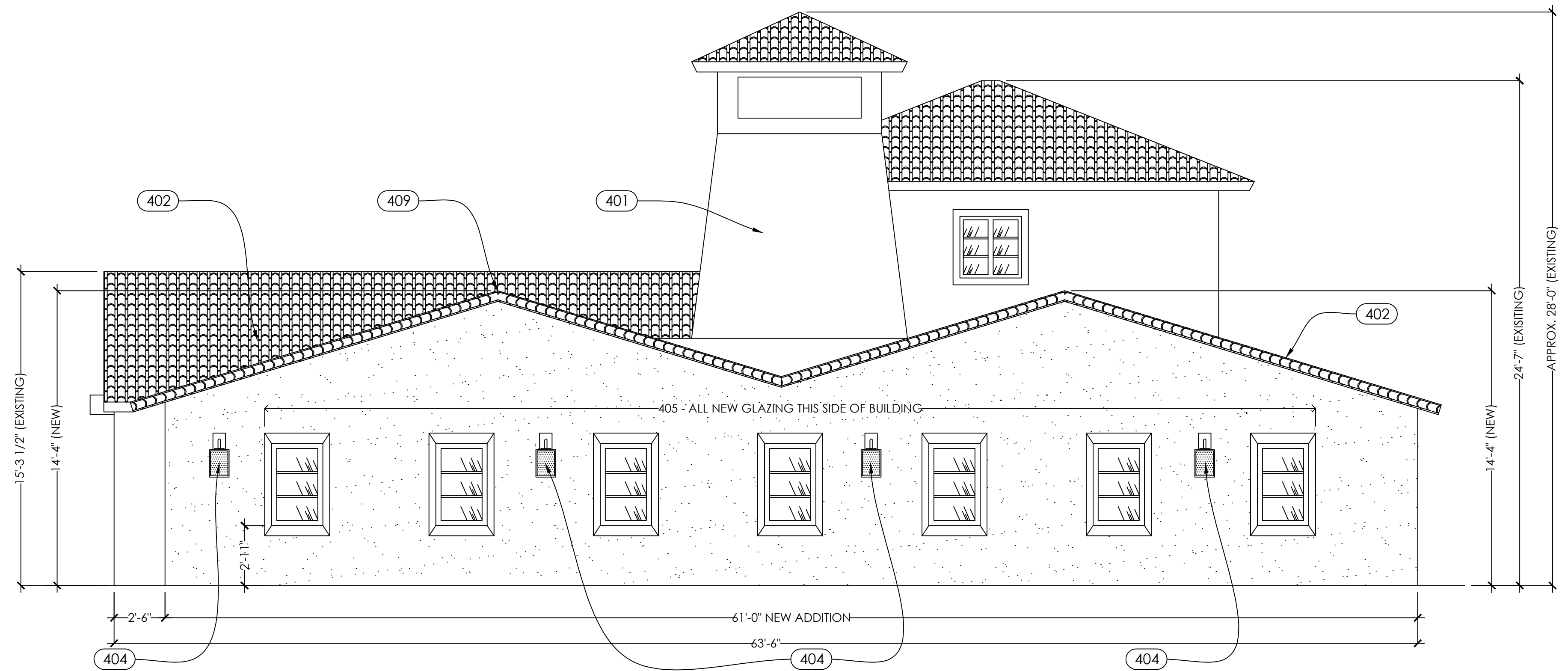
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A-3.3



REMODELED SOUTH ELEVATION
(HUNTINGTON DR.) 1/4" = 1'-0" A
3.4



REMODELED EAST ELEVATION
(GARFIELD AVE.) 1/4" = 1'-0" B
3.4

DRAWING KEYNOTES
NOTE: NOT ALL KEYNOTES PERTAIN TO THIS PLAN

KEYNOTE	DESCRIPTION
400	PROVIDE THE FOLLOWING ITEMS:
400	EXISTING ROOF RIDGE
401	EXISTING STRUCTURE TO BE PROTECTED IN PLACE
402	NEW TILE ROOF - ENSURE SEAMLESS CONNECTION TO NEW ROOF
403	EXISTING TILE ROOF - PATCH/REPAIR AS NECESSARY.
404	NEW EXTERIOR LIGHTING FIXTURES - TO MATCH EXISTING EXTERIOR FIXTURES; VERIFY W/ OWNER
405	NEW EXTERIOR GLAZING
406	EXISTING GLAZING, TYP. - PROTECT IN PLACE
407	EXISTING EXTERIOR LIGHTING FIXTURES - TO REMAIN
408	STOREFRONT GLAZING SYSTEM - TO REMAIN, PROTECT IN PLACE
409	NEW ROOF RIDGE LINE
410	NEW EXTERIOR DOOR - SEE FLOOR PLAN AND DOOR SCHEDULE FOR MATERIALS, HARDWARE AND SIZE.
411	EXISTING ELECTRICAL ROOM (SWITCH-GEAR, METERS, AND PANEL) - NO WORK
412	EXISTING 1-HR FIRE RATED MED. GAS CLOSET AND STORAGE - NO WORK
413	EXISTING TRASH ENCLOSURE
414	EXISTING EXTERIOR DOOR TO REMAIN
415	EXISTING DOOR TO BE REMOVED
416	EXISTING PLANTER/DOG WALK AREA - NO WORK
417	NEW EXTERIOR BUILDING SURFACE - MATCH EXISTING AND BLEND SEAMLESS TO EXISTING

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REGISTERED ARCHITECT
NALL F. SAUNDERS
C-26955
10/31/23
68.848
STATE OF CALIFORNIA

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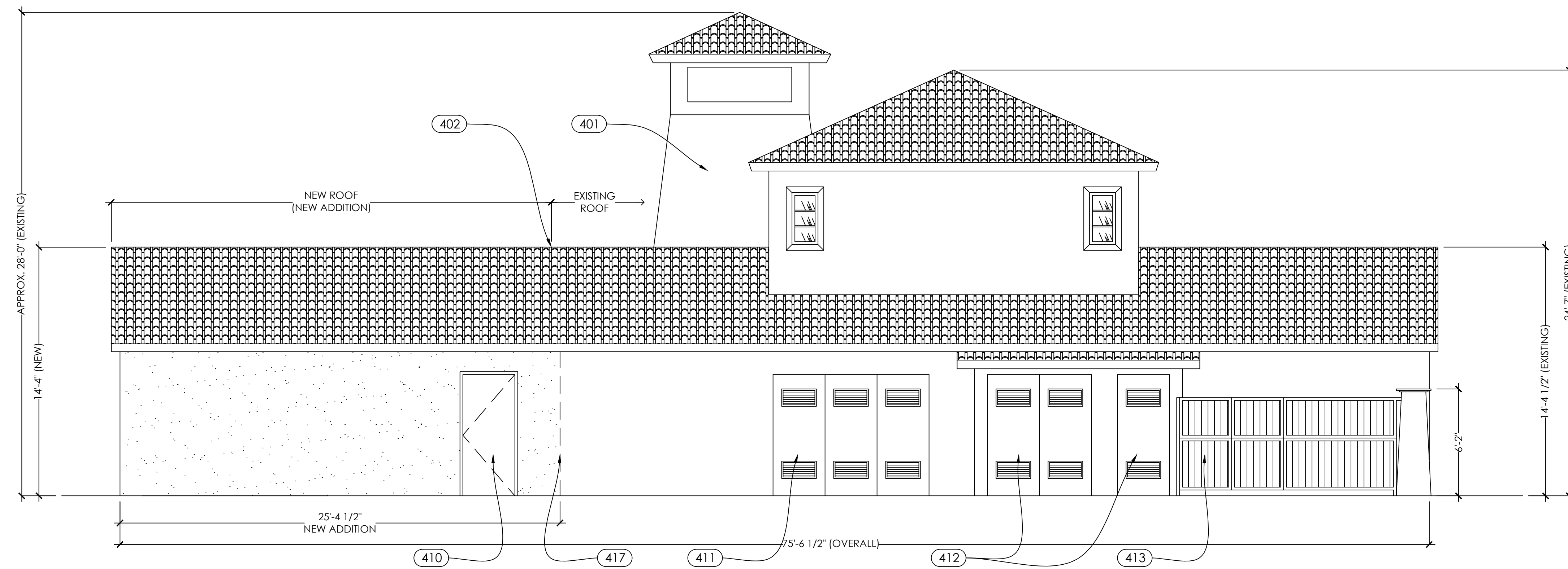
EXTERIOR ELEVATIONS (PROPOSED)

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REVISIONS:

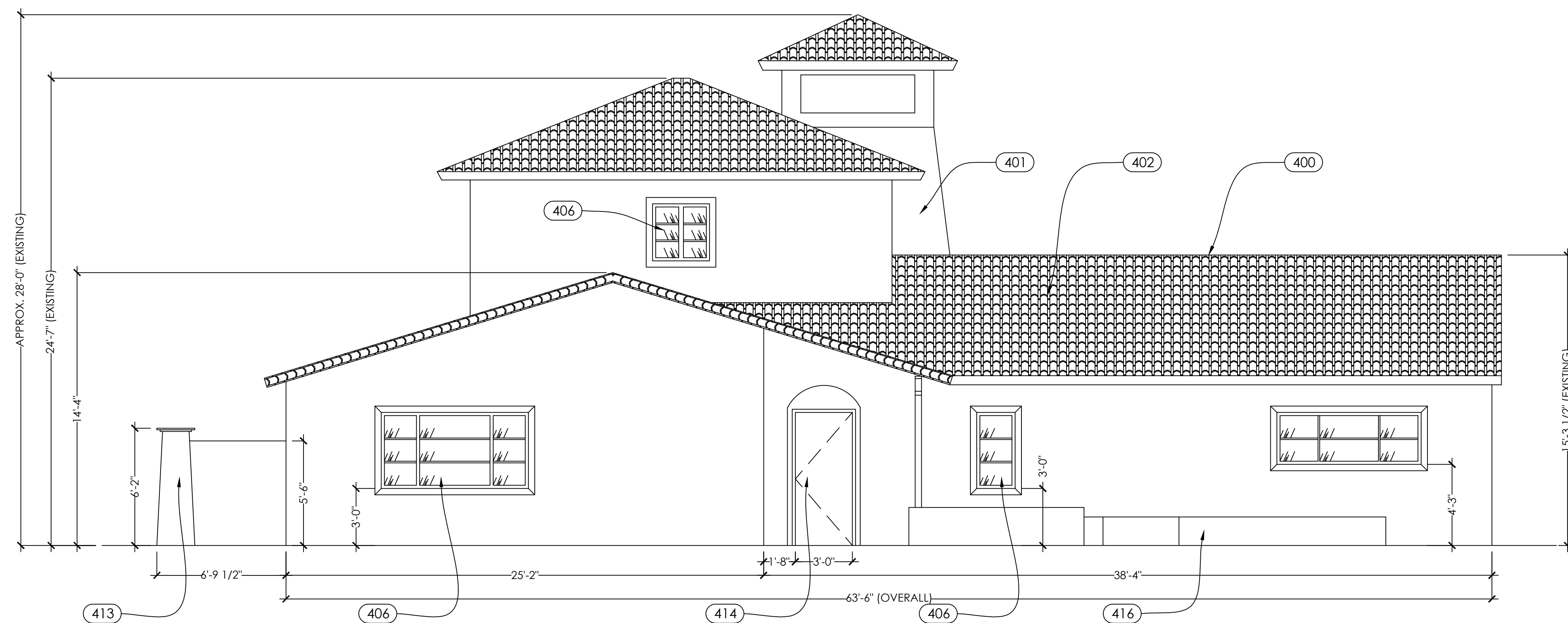
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A-3.4



REMODELED NORTH ELEVATION (ALLEYWAY) 1/4" = 1'-0" A 3.5



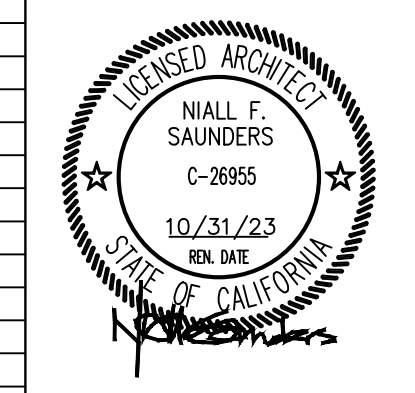
REMODELED WEST ELEVATION (NO WORK VISIBLE THIS SIDE) 1/4" = 1'-0" B 3.5

DRAWING KEYNOTES

NOTE: NOT ALL KEYNOTES PERTAIN TO THIS PLAN

KEYNOTE	DESCRIPTION
400	PROVIDE THE FOLLOWING ITEMS:
400	EXISTING ROOF RIDGE
401	EXISTING STRUCTURE TO BE PROTECTED IN PLACE
402	NEW TILE ROOF - ENSURE SEAMLESS CONNECTION TO NEW ROOF
403	EXISTING TILE ROOF - PATCH/REPAIR AS NECESSARY.
404	NEW EXTERIOR LIGHTING FIXTURES - TO MATCH EXISTING EXTERIOR FIXTURES; VERIFY W/ OWNER
405	NEW EXTERIOR GLAZING
406	EXISTING GLAZING, TYP. - PROTECT IN PLACE
407	EXISTING EXTERIOR LIGHTING FIXTURES - TO REMAIN
408	STOREFRONT GLAZING SYSTEM - TO REMAIN, PROTECT IN PLACE
409	NEW ROOF RIDGE LINE
410	NEW EXTERIOR DOOR - SEE FLOOR PLAN AND DOOR SCHEDULE FOR MATERIALS, HARDWARE AND SIZE.
411	EXISTING ELECTRICAL ROOM (SWITCH-GEAR, METERS, AND PANEL) - NO WORK
412	EXISTING 1-HR FIRE RATED MED. GAS CLOSET AND STORAGE - NO WORK
413	EXISTING TRASH ENCLOSURE
414	EXISTING EXTERIOR DOOR TO REMAIN
415	EXTERIOR DOOR TO BE REMOVED
416	EXISTING PLANTER/DOG WALK AREA - NO WORK
417	NEW EXTERIOR BUILDING SURFACE - MATCH EXISTING AND BLEND SEAMLESS TO EXISTING

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